

**Commissioners:**

Julie Kimble  
Michelle Kruzal  
Tammy  
McGehee  
Michelle Pribyl  
Karen  
Schaffhausen  
Erik Bjorum  
Emily Leutgeb



**Planning Commission Agenda**

Wednesday, June 2, 2021  
6: 30pm

**Address:**  
2660 Civic Center Dr.  
Roseville, MN 55113

**Phone:**  
651-792-7080

**Website:**  
[www.cityofroseville.com/pc](http://www.cityofroseville.com/pc)

Following guidance from state health officials, Planning Commission Members will participate in upcoming meetings electronically pursuant to Minn. Stat. § 13D.021.

Members of the public who wish speak during public comment or an agenda item during this meeting can do so virtually by registering at [www.cityofroseville.com/attendmeeting](http://www.cityofroseville.com/attendmeeting)

1. Call To Order
2. Roll Call
3. Approval Of Agenda
4. Review Of Minutes

Documents:

[MAY 5, 2021 MINUTES.PDF](#)  
[MAY 13, 2021 MINUTES.PDF](#)

5. Communications And Recognitions

5.A. From The Public:

Public comment pertaining to general land use issues not on this agenda.

5.B. From The Commission Or Staff:

Information about assorted business not already on this agenda.

6. Public Hearing

6.A. Consider A Request For A Conditional Use To Allow An Outdoor Pet Exercise Area In Conjunction With A Dog Daycare At 2216 County Road D (Tower Glen) (PF21-005)

Documents:

[6A REPORT AND ATTACHMENTS.PDF](#)

6.B. Consider A Request For A Conditional Use To Allow A Drive-Through For A Proposed Panda Express At 2030 Twin Lakes Parkway (PF21-004)

Documents:

[6B REPORT AND ATTACHMENTS.PDF](#)

6.C. Request For Preliminary Approval Of A Major Plat To Subdivide The Development Site Into 11 Lots For Single-Family, Detached Homes At 2395 County Road B And 2224 Eustis Street (PF21-003)

Documents:

[6C REPORT AND ATTACHMENTS.PDF](#)  
[6C BENCH HANDOUT.PDF](#)

7. Adjourn



**Planning Commission Regular Meeting  
City Council Chambers, 2660 Civic Center Drive  
Draft Minutes – Wednesday, May 5, 2021 – 6:30 p.m.**

*Pursuant to Minn. Stat. 13.D.021, Planning Commission members, City Staff, and members of the public participated in this meeting electronically due to the COVID-19 pandemic.*

- 1 **1. Call to Order**  
2 Chair Kimble called to order the regular meeting of the Planning Commission meeting at  
3 approximately 6:30 p.m. and reviewed the role and purpose of the Planning Commission.  
4
- 5 **2. Roll Call**  
6 At the request of Chair Kimble, City Planner Thomas Paschke called the Roll.  
7  
8 **Members Present:** Chair Kimble; Vice Chair Michell Pribyl, and Commissioners  
9 Michelle Kruzel, Tammy McGehee, Karen Schaffhausen (arrived  
10 late), Erik Bjorum and Emily Leutgeb.  
11  
12 **Members Absent:** None  
13  
14 **Staff Present:** City Planner Thomas Paschke, Community Development Director  
15 Janice Gundlach, Senior Planner Bryan Lloyd and Community  
16 Development Department Assistant Staci Johnson.  
17
- 18 **3. Approve Agenda**  
19  
20 **MOTION**  
21 **Member Kruzel moved, seconded by Member Pribyl, to approve the agenda as**  
22 **presented.**  
23  
24 **Ayes: 6**  
25 **Nays: 0**  
26 **Motion carried.**  
27
- 28 **4. Review of Minutes**  
29  
30 **a. April 7, 2021 Planning Commission Regular Meeting**  
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32 **MOTION**  
33 **Member Pribyl moved, seconded by Member McGehee, to approve the April 7,**  
34 **2021 meeting minutes.**  
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36 **Ayes: 6**  
37 **Nays: 0**  
38 **Motion carried.**

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**5. Communications and Recognitions:**

**a. From the Public:** *Public comment pertaining to general land use issues not on this agenda, including the 2040 Comprehensive Plan Update.*

None.

**b. From the Commission or Staff:** *Information about assorted business not already on this agenda, including a brief update on the 2040 Comprehensive Plan Update process.*

Member McGehee indicated she attended two webinars put on by the DNR on shoreland ordinances and variances that were very interesting.

**6. Other Business**

**a. Review and Provide Feedback on Zoning Code Update**

Mr. Jeff Miller and Ms. Rita Trapp, Hoisington Koegler Group, Inc., made a presentation on the Zoning Code update.

Member Schaffhausen joined the meeting at 6:54 p.m.

Member McGehee indicated when the Commission was going through the housing types, particularly at the tri-plex and quad, it really did not seem that they fit into the neighborhood styles of Roseville. She explained Roseville does have a variety of neighborhoods and some very clearly have a smaller lot and smaller house and there are distinct neighborhoods that have larger lots. She thought to try to make a one size, fits all, as she got to the final recommendations, she was not sure this is what most residents were looking for. She wondered whether the recommendation to divide the LDR, the LU into two formats and how much flexibility does the Planning Commission have to change the equivalent of LDR-1 to move it from four to six instead of from four to eight and then make the MDR go from six to eight or something like that. She did not know if the Commission has the authority to talk about things like that. She was also not sure what was meant my non-traditional in the presentation she went through. She thought in general, if looking at the style of most of the residential areas, if the City wants to increase density, the cottage style development seems to fit better given the sort of almost uniform story or story and a half in a lot of the neighborhoods.

Mr. Miller clarified the non-traditional plan for the Commission.

Community Development Director Janice Gundlach indicated regarding the density change, it was her understanding that the comp plan has been adopted and if the City were to change that the City would have to go back to the Met Council and propose an amendment to the comp plan.

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Mr. Miller continued with the presentation on recommending renaming and consolidating of Districts.

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Member McGehee explained in the LDR that is still in the recommendation, to increase density in that area. It seemed more suitable to use cottage homes there and then have the variety of tri-plex and quads and twin homes in the LMDR from the standpoint of existing low-density housing.

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Chair Kimble asked if Commissioner McGehee was now thinking of just the type versus the density. She thought the question about changing the density is not without a comp plan change.

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Member McGehee indicated that was correct. She was speaking to the kind and also speaking to the density because, at least from what she is seeing and hears, she thought there is little appetite for much increased density in the residential areas, but people seem to be perfectly happy to have some areas of twinhomes and certainly to increase the density in the City with some of the things the Council has done to put some housing in the MU Districts.

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Mr. Miller continued with their recommendation of allowed housing types in each District. He noted the recommendation is trying to line up the changes to the District with what the comp plan shows. He also reviewed the recommended residential densities.

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Member Schaffhausen indicated she imagined fast forward to when it is time to do the community meetings, one of the questions might be, if the City does this LMDR and the size of the lots are changed and make them smaller, how would that start to affect some of the conversations with regard to variance and outbuildings and their size and what that looks like. She imagined this will be like dominoes where this starts to hang up and this is the first line and then it kind of follows through and the City will need to follow up to find out how the other things fit into that. She asked if that is coming yet where the City will have to go and fix those other options as well or will the City need to take those as a one off as they pop up, based on what is left to be developed in Roseville.

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Senior Planner Bryan Lloyd explained that current regulations do not provide for things like storage buildings and detached garages, according to lot size. A larger detached garage or storage shed has more to do with the footprint size of the house, up to a certain maximum size. That is the same for people that have very small lots and for those who have very large lots. Beyond that there are less direct constraints that get to the improvement area and impervious coverage and meeting setback requirements, etc. There may be a reason to look at those provisions moving forward, especially if lot sizes are changing but those accessory structures are not pegged to lot sizes.

132 Member Schaffhausen explained she would like to have the opportunity to look at it  
133 because of the size of some of the houses. The quads are neat, but the question  
134 becomes all of a sudden if it is all the need for a shed. Part of it is her lack of  
135 knowledge who is responsible for maintenance if someone has a tri-plex and if that  
136 property would get a shed if there would be an option. She noted she was thinking  
137 ahead and possible implications.

138  
139 Mr. Paschke explained his answer would be that currently those types of things are  
140 allowed in the Districts the City has right now so not a whole lot is changing when  
141 looking at if there can be smaller lots and those type of things in a development or for  
142 that matter, even a four plex, six plex or eight plex, the code would allow an  
143 accessory dwelling, but it is really capped by the amount of coverage on the lot. He  
144 thought that may need to be tweaked but he did not know that these changes will  
145 impact how the City has enforced or allowed accessory sheds and or a garage or  
146 accessory building in any of those residential districts.

147  
148 Member McGehee indicated she looked over all of the comments from citizens and  
149 has yet to see anyone asking for more density. In fact, that is the thing the City might  
150 most likely bump up against is people not wanting anymore density, particularly in  
151 the residential neighborhoods. She explained she was a little bit reluctant and did not  
152 know where the density mandate is coming from given the number of apartments that  
153 the City has been putting in. The other thing, she did not see where the City is going  
154 to put in equity in this. As the City begins to increase the density, one of the things  
155 that come up a lot in hearings and meetings is the sense of community and she  
156 thought there was a good sense of community and people really value that and also  
157 value their neighborhoods. She thought the City should think about moving forward.  
158 If the City is going to increase density, how does the City increase that density in a  
159 way that brings people in and makes them a part of the community that exists rather  
160 than overwhelming the existing community or makes it so dense that it is not a  
161 community or a neighborhood. She thought the City has done that in a couple of  
162 places and she thought it was important when thinking about this.

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164 Member Pribyl explained she was thinking about the neighborhood she used to live in  
165 where there was a pretty broad range of single family, duplexes and apartment  
166 buildings and it felt very compatible. There were not any buildings that were really  
167 out of scale. Personally, she did not have concerns about things like putting duplexes  
168 or twinhomes in a low-density neighborhood if they are designed well and in scale  
169 with the neighborhood. Her questions relates to the building height. It felt a little bit  
170 random by going in five feet increments for the different districts. She knew those  
171 were related to the taller heights already allowed in the different districts but thinking  
172 about the number of stories in a building that relates to and also thinking about the  
173 City where there is not just LDR. The Districts will be next to each other, so she was  
174 thinking about the comments raised at some of the community meetings. Is there a  
175 step down or a range of heights allowed in the different Districts and the concerns of  
176 the neighborhoods next to the ones that allow taller heights. She wondered if there  
177 could be some sort of buffer so a low-density area with shorter buildings are not right  
178 next to taller buildings.

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Mr. Miller thought it was an interesting idea to think about having a lower building height adjacent to residential. He noted he would have to think about how to actually implement that. As far as the building heights recommended, he was open to those being different but were trying to get them to match up with the scale that was identified in the comp plan.

Member Leutgeb thought that the comparison of reality to current zoning on lot size was interesting and she thought it was pretty clear that the more common current lot size of around nine thousand, there was a big discrepancy, percentage wise of what exists and what is written in code. She wondered what the consultants have seen elsewhere as codes have been rewritten.

Mr. Miller explained he looked a little bit at neighboring cities to Roseville but did not do a comprehensive look at it, but he thought seventy-five was perhaps typical for a lot of width. He noted every community is different and every community develops differently.

Member Bjorum explained he wanted to weigh in on the density. He saw a lot of comments from the meetings were regarding moving from renting to owning and he thought how that relates to some of the density discussed, he thought allowing for those duplexes and tri-plexes in some of these neighborhoods actually help that issue and make a more inclusive argument for that. He really appreciates the density the City is providing because it allows for some of those construction types that would allow for more of an economic threshold for people in them and it allows for those people to become more a part of the community and the neighborhood that those units are developed in. He thought that really sets that precedent that allows for those types of construction to be included in their neighborhoods and allows more people into those opportunities.

Member Leutgeb indicated that by adding some of this density the City is acknowledging that family structures and multi-generational families have different density requirements in housing. She did see that this is a clear alignment with the equity initiative by offering more types and more density in housing selections.

Member Kruzal thought the big thing is the change to make the housing more inclusive for all levels of income and equity across the board. She hoped this will touch on that and enhance that.

Chair Kimble observed and agreed that the housing density does help with the equity and also to aligning the nine thousand square foot minimum also makes it more affordable. She thought the idea of mimicking what is in the comp plan in the zoning was great because when developers come in, it just makes everything tight and clear. She thought it seemed like the primary type of unit that seems to be in some conflict with the LDR is the idea of the cottage housing because that is eight units on that acre but yet at the same time she thought when she looks at that particular kind of product she thinks a lot about seniors and a lot about building community around those kind

226 of cottage developments and she thought there is a real opportunity for it. She  
227 wondered if it is really practical to get eight units of the cottage type of home onto  
228 one acre. She wondered if the cottage homes should be allowed in the LDR. She  
229 thought it seemed like there are pros and cons for it.

230  
231 Mr. Miller explained the fifty-five hundred number is in the higher districts but the  
232 attached housing types in the LMDR and MDR allow a minimum of thirty-six  
233 hundred square feet per unit, which is much smaller. The fifty-five hundred is a  
234 generous number in that it is really just being set by that maximum density of eight  
235 units per acre. He thought if they were looking for an ideal number for cottage  
236 courtyard housing, it is not fifty-five hundred and would probably be something  
237 smaller, but they are really sticking with the density that has already been adopted in  
238 the comp plan.

239  
240 Chair Kimble thought another things the City should be focusing on is the height of  
241 the buildings in terms of how many stories and if it is making sense.

242  
243 Mr. Miller agreed that 35 feet is really more like two stories or 2.5 stories. They are  
244 open to those numbers being something different. He thought they were basically  
245 saying that this reflects a difference in scale. Going higher as the density goes up and  
246 it provides one height per district. He asked if there were any suggestions for  
247 changing the building heights in the zoning districts.

248  
249 Member Pribyl asked for clarification on the measurements for heights on pitched  
250 roofs and if it included half of the height of the slope on pitched roofs and on flat roof  
251 buildings if that includes the parapet or if it is the top roof sheathing.

252  
253 Mr. Paschke explained the Code would say that as it relates to a gabled or hipped  
254 roof, that it is to the midpoint of the roof truss. It does go up a little on the roof truss  
255 system. On a flat roof, it is to the top of the roof system, not the part of the parapet,  
256 that might go up further than that. In the LMDR, thirty-five feet is really a two and a  
257 half story home. Three stories might be attainable out of the forty foot, maybe four  
258 but it would be a very tight four-story building.

259  
260 Member Bjorum explained his concern with the five-foot increments is that it does  
261 not really align with construction or architecture. A story is not going to be added  
262 between LMDR and MDR if the difference is thirty-five feet or forty feet. He  
263 thought it would make more sense to do them in ten-foot increments and LMDR  
264 should really start at thirty feet or thirty-five feet maybe works but then MDR should  
265 jump ten feet which is more relatable to a construction type of story in a building.

266  
267 Chair Kimble agreed.

268  
269 Member Pribyl thought it might be helpful to look ahead to when this goes forward to  
270 a public hearing to have examples of buildings that are those heights for comparison.  
271 She also thought going back to the question of density and rental versus ownership,  
272 the idea of introducing duplexes and accessory dwelling units to her, allows more



273 people to be either renters or owners in some of the lower density neighborhoods.  
274 She thought was also an equity issue the City is addressing.

275  
276 Ms. Trapp continued the presentation on the Non-Residential District Structure  
277 recommendations. She noted this will be discussed further when brought back for  
278 Commission review. She also reviewed Section One remaining discussion topics  
279 including Consider Allowing Increased Density in MDR with CUP.

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281 Member McGehee indicated she would like to see the City's standard CUP things  
282 that they look at and how do they assess those. If they are going to start using  
283 Conditional Use Permits for a variety of things then they should look at that.

284  
285 Member Bjorum thought these seemed reasonable but thought it would be nice to  
286 have the City standard CUP information before the Commission fully agrees with this  
287 jump but on the surface it makes a lot of sense to him.

288  
289 Ms. Trapp reviewed the idea of considering increasing the Minimum Green Space  
290 requirement in MDR and HDR with the Commission.

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292 Member Schaffhausen thought about the number of times the Commission has gone  
293 through and had to pare down on the number of trees and she wondered where this  
294 fell into that same conversation where there is going to be trading going on.

295  
296 Mr. Paschke thought the key to the Landscape Ordinance is that it probably should be  
297 reviewed as it relates to the amount of landscaping that it is seeking on certain lots.  
298 He thought when the City has run into the majority of the problem, it has to do with  
299 the multi-family requirement of one tree per unit, which staff is aware of and will  
300 look into and will most likely have a proposal to change that as a part of this process,  
301 probably in phase 2 than in phase 1.

302  
303 Member Pribyl thought these were two different things potentially, on one hand,  
304 increasing the green space minimum overall would address some concerns that have  
305 been raised at other meetings about there being a lot of parking area and a lot of roof  
306 area, just increase in the green space on the site reduces the heat island effect,  
307 improves the appearance overall, even if the green space is not useable it is something  
308 that can potentially be looked at and enjoyed. She indicated usable outdoor space is a  
309 separate issue which is also important and could be a rooftop terrace or balcony but  
310 that is not addressing the other issues of overall aesthetics and incorporating nature  
311 and lowering heat island effects.

312  
313 Chair Kimble explained the other part to it is, is it a part of the overall green space  
314 and not additive, just making sure that they are not doubling up too much, would be  
315 important.

316  
317 Member McGehee indicated this something she has been interested in for a long time  
318 and she agreed with Mr. Paschke. She agreed with Member Pribyl that there are  
319 divisions of place that you can sit outside and maybe there are a couple of trees and a

320 picnic table versus a manicured flower bed by the front door. They are both green  
321 space but are very different. She thought both types of green space are important and  
322 if the City can make it all happen nicely would be great. She also thought the more  
323 density there is in an area the less green space there will be. She would like the City  
324 to think about the ratio as well.

325  
326 Member Bjorum thought the green outdoor space requirement really just needs a  
327 better definition. He wondered if the City is going to deal with increasing or  
328 maintaining the green space then they may need to just define better what actually  
329 they are going to consider increasing. Especially, since there seems to be two sets of  
330 things here being discussed for the green space and he thought just having a clearer  
331 definition of what they should increase. He also wondered if this should be put in  
332 phase one or phase two. It put in phase one, does that mean as part of all the updates  
333 to all of the densities for medium, high, and low, this would be piggy backed into that  
334 group of considerations they are doing right now so as all of this is being worked out  
335 this could be included because that would be his preferred method for handling it.  
336

337 Ms. Trapp indicated the phase one would be the elements they are working on right  
338 now, where changes are being made to lot size and densities and housing types. She  
339 reviewed the topic “Consider Increasing the CUP Threshold in the CMU Districts”  
340 with the Commission. She asked for concerns.

341  
342 Member Pribyl thought it made sense and could not think of a reason why the City  
343 would need a CUP for that if the idea were to encourage and allow multi-family in  
344 mixed use.

345 Member Bjorum thought it seemed unnecessarily restrictive.

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347  
348 Chair Kimble agreed.

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350 Ms. Trapp continued with the presentation on Establishing a BRT Overlay District,  
351 which is one of the elements of the comp plan.

352  
353 Member McGehee thought the City should enhance safety and walkability because in  
354 most place they are talking about significant amount of traffic at significant speeds.  
355 They might want to also consider talking to MnDOT because MnDOT has been  
356 thinking about doing something like a pedestrian, non-motorized crossing between  
357 Rosedale and HarMar, which would be pretty significant, and the City might be able  
358 to tie into something like that.

359  
360 Chair Kimble requested when this is looked at that the Commission have a visual of  
361 at least segments of this BRT Overlay District so the Commission can understand it  
362 because it is really great to talk about an enhanced pedestrian plan but how  
363 continuous is that along the overlay district because these are all great things but what  
364 does it really mean when looked at.

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366 Ms. Trapp indicated they can add that.

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Member Pribyl was also wondering if talking about safety and a pedestrian plan, it obviously goes beyond the individual parcels and goes beyond their scope. She wondered if the Public Works Commission and City Engineering could also be involved in some of that discussion as well.

Ms. Trapp agreed and thought the idea was, at least within individual sites that when redevelopment happens the City could think about it in terms of how the parking is laid out, how to make sure where the connections are, etc. The little enhancements overtime will knit together and there may be value in having people do that and really think about it.

Ms. Trapp reviewed the last topic of Implementation of the 10% Minimum Residential Requirement in the MU-2, MU-3, and MU-4 Districts with the Commission.

Member Pribyl indicated she could see a lot of problems with requiring ten percent on individual parcels or somehow trying to require ten percent throughout particular areas. She thought including it as a part of the purpose statement but not making it a hard requirement for each development makes a lot of sense.

Chair Kimble agreed and thought it was really hard to understand and would never work so then how would staff govern this if it were across that particular district. She thought the intent was good but in practicality in looking at it, it is really hard to understand how it could be done and enforced and is not really doable on a development by development, basis. She would be in agreement that pointing it back as a purpose statement might make more sense.

Member McGehee agreed with all of that, and it seemed that staff could make it easy to follow up on.

Member Bjorum asked if the City were to just include this in the purpose statement for these districts, the comprehensive plan still says the ten percent and he wondered if there is still a requirement for the developers in these districts to still have to meet that.

Ms. Trapp explained it would be something that staff would continually evaluate and think about every time a development would come through. She thought it would be something the Commission would have the opportunity to comment on. The Commission would be able to be more reflective of the location and the size of the development to say does it make sense or not that residential is a part of it, but it would be something that would be calculated over the entire things and something that staff would be looking at.

Ms. Trapp indicated her team will be putting together additional information about this in the coming months. She indicated she was glad that this is headed in a direction that made sense to the Commission and there are not big concerns.

414

Mr. Miller finished the presentation with the next steps for the Zoning Code update.

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416

Chair Kimble thanked Mr. Miller and Ms. Trapp and staff for all the work.

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419 **7. Adjourn**

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**MOTION**

421

**Member Pribyl, seconded by Member McGehee, to adjourn the meeting at 8:52**

422

**p.m.**

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**Ayes: 7**

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**Nays: 0**

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**Motion carried.**

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**Special Planning Commission Meeting  
City Council Chambers, 2660 Civic Center Drive  
Draft Minutes – Thursday, May 13, 2021 – 7:00 p.m.**

*Pursuant to Minn. Stat. 13.D.021, Planning Commission members, City Staff, and members of the public participated in this meeting electronically due to the COVID-19 pandemic.*

1 **1. Call to Order**

2 Chair Kimble called to order the regular meeting of the Planning Commission meeting at  
3 approximately 7:00 p.m. and reviewed the role and purpose of the Planning Commission.  
4

5 **2. Roll Call**

6 At the request of Chair Kimble, City Planner Thomas Paschke called the Roll.  
7

8 **Members Present:** Chair Kimble; Vice Chair Michell Pribyl, and Commissioners  
9 Michelle Kruzel, Tammy McGehee, Karen Schaffhausen, Erik  
10 Bjorum and Emily Leutgeb.  
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12 **Members Absent:** None  
13

14 **Staff Present:** City Planner Thomas Paschke, Community Development Director  
15 Janice Gundlach, Senior Planner Bryan Lloyd, Community  
16 Development Department Assistant Staci Johnson  
17

18 **3. Approve Agenda**

19 **MOTION**

20 **Member McGehee moved, seconded by Member Leutgeb, to approve the agenda as**  
21 **presented.**  
22

23 **Ayes: 7**

24 **Nays: 0**

25 **Motion carried.**  
26  
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28 **4. Communications and Recognitions:**

29  
30 **a. From the Public:** *Public comment pertaining to general land use issues not on this*  
31 *agenda, including the 2040 Comprehensive Plan Update.*  
32

33 None.  
34

35 **b. From the Commission or Staff:** *Information about assorted business not already on*  
36 *this agenda, including a brief update on the 2040 Comprehensive Plan Update*  
37 *process.*  
38

39 None.

40

41

42 **5. Other Business**

43

44 **a. Review and Provide Feedback on Zoning Code Update**

45 Community Development Director Janice Gundlach introduced Mr. Jeff Miller and  
46 Ms. Rita Trapp from HKGi who made a presentation on the Zoning Code Update and  
47 asked for feedback from the Commission.

48

49 Ms. Trapp started off the presentation with the update to density standards for mixed  
50 use districts. She asked for Commission input or questions.

51

52 Member Pribyl wondered about the distinction between Mixed Use 2A and 2B. She  
53 thought it looked like there was a little more density in 2B, but she noticed the  
54 housing types allowed are the same and later on the building height allowances seem  
55 to be reversed. 2B allows taller buildings than Mixed Use 3 does.

56

57 Ms. Trapp explained there are use differences between 2A and 2B once the  
58 consolidation is done. There is a difference in height, 2A is only 35 feet so it is the  
59 smaller height where 2B is being proposed to allow up to 55 feet or 65 feet with a  
60 CUP. There is a difference between the two districts in terms of scale and intensity.  
61 HKGi did recommend distinction between those two. 2A seems to be neighborhood  
62 business in its character and scale whereas 2B is more similar to more of a  
63 community business or where there is more intense use. That is why they did not  
64 consolidate all the way down to have just one mixed use area.

65

66 Member Pribyl was not sure what distinctions there were between 2B and 3.

67

68 Mr. Miller currently thought the Districts MU-1, MU-3 and MU-4 really relate to  
69 each other as far as neighborhood community and regional business and CMU is a  
70 different animal as far as zoning districts. It is their understanding with the Comp.  
71 Plan, with the intention of bringing those business districts clearly into play as mixed-  
72 use districts means they are more aligned with the CMU. He noted they are using the  
73 Comp. Plan as a guide. He indicated those can be looked at to see if there is any need  
74 to distinguish between MU-2B and MU-3.

75

76 Chair Kimble asked if there will be more description in the Zoning Code or  
77 introduction to these various zoning districts that differentiate or describe what was  
78 said.

79

80 Ms. Trapp indicated once in the zoning districts; it will become a little more obvious  
81 because there will be additional language. She noted this information will be used  
82 and they will make strike changes in the Code as it is today.

83

84 Member McGehee agreed with Member Pribyl and thought originally there were the  
85 four levels, as proposed by staff, to have those four levels within the SMU District

86 because that was their mixed-use district and there was a lot of discussion about  
87 having the intensity based on positions along either Cleveland Avenue or along  
88 County Road C and then moving back and then there was also the issue where these  
89 regions bumped against residential housing along Caris Drive. If they are going to go  
90 ahead and make a blanket, MU classification to allow housing and so on in all of  
91 these districts than it does not seem to her that it makes any sense to try to maintain  
92 this area and she agreed with Member Kimble that it is very confusing and it is not  
93 clear that these various differences that have been spelled out for Twin Lakes, there is  
94 only one parcel left to develop there so it seems it is just additionally confusing to try  
95 to run this.

96  
97 Member McGehee asked if MU-1 is neighborhood business, she is really not familiar  
98 with all of the sizes of buildings in that are in the small neighborhood businesses but  
99 if the idea is to have some limited housing among buildings in the neighborhood  
100 business then she did not see any benefit in adding a minimum to that. If someone  
101 has a small, private business that is run on the first floor and that person has the space  
102 to put one or two apartments up above, she thought that should be an available option  
103 and by adding these minimums they are not helping out small business. She also  
104 indicated she did not see anywhere in these business districts where the City is going  
105 to have cottage units or similar that are described there and how this is all going to fit  
106 together. She thought all of those housing types are nice to have in the City's  
107 toolbox, but she did not see in any of what is being discussed here, where it is  
108 showing anywhere from ten to thirty-six units, that a lot of those housing types are  
109 realistic in the City's business districts. She was not sure the minimums should be  
110 there.

111  
112 Ms. Trapp thanked Member McGehee for her comments. She noted she will have to  
113 go back to do some calculations, but she thought the change or the insertion of the  
114 community mixed uses/mixed use 2 is probably more confusing at this point because  
115 of the way the Commission is used to thinking about it and seeing it. She noted she  
116 was not so concerned about the confusion, necessarily for others because how the  
117 City Code is currently set up, and they are not talking about changing it significantly,  
118 each district, other than the use tables is handled kind of individually in how their  
119 standards are set forth.

120  
121 Ms. Trapp explained there were the four levels and staff did take a hard look at it and  
122 felt like they were not ready because it was for Twin Lake specifically and it was not  
123 really identified in the Comprehensive Plan. Again, they are trying to stay true to the  
124 Comprehensive Plan. They wanted to do more combination or consolidation in this.  
125 They did really feel like there was a distinction between 2A and 2B. She also noted  
126 that there were two new areas that were identified for community mixed use, one  
127 being HarMar and one being the shopping center on Hamline. Because of that there  
128 are new areas that are being looked at to use the form base standard. Information will  
129 be added about frontages to help guide that should redevelopment occur. That is one  
130 of the reasons why staff is proposing it the way they are, but more discussion can  
131 occur.

132

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179

Ms. Trapp indicated regarding the cottage homes, the reason why they are thinking about providing all of these is just more flexibility. It is true that the City may or may not see all of these different housing types, but the more choices provided the more flexibility there is to make something that work on the site or for the market.

Chair Kimble asked since the housing types are being named, what if there is a new type or a type is missed, what are the implications then.

Ms. Trapp explained there are two ways of addressing that, if the Commission or staff identifies that there is a new type of housing then they could go through proactively and make an adjustment to the code. If not, an applicant can request a zoning text amendment to add something in. She hoped these are broad enough that they capture most of what is seen in the future. She noted this is accurate for what they have now and if there are changes then the City will either go through this process again or someone will ask for something to be added.

Member McGehee explained something that will be coming up late is the build forward that was laid out for Twin Lakes as opposed to the traditional setback. There has been a significant amount of pushback in the community, displeasure with that building type, that build forward that does not make it feel like a suburb. The City has gotten several complaints about building along Larpenteur that went with that model. She thought moving forward, that model might be fine by itself but is not one that seems to be appreciated or desired in other parts of the community.

Chair Kimble asked for confirmation that the minimums really are confirming what is in the Comp. Plan so that there is consistency between both.

Ms. Trapp indicated that was correct.

Mr. Miller explained the housing types are also reflected in the Comp. Plan, those districts include medium or high density residential and the description of them.

Ms. Trapp continued her presentation on Scale and Intensity and how it relates to the Comprehensive Plan.

Member Pribyl thought in MU-2B, with it being HarMar, potentially considering stepping it down as it approaches the single-family neighborhood. Fifty-five feet would be quite objectionable to the neighbors immediately to the east and south but if it is further away it might be less objectionable.

Ms. Trapp noted she would make sure that is still on their radar to discuss.

Member McGehee asked that be looked at across the board because she felt everywhere this would bump up to residential the City needed to be cognizant of that.

Chair Kimble thought it would be really helpful to understand the scale of these districts and then of course adjacency is so important. She understood the CUP and



180 that some conditions can be created to help control, but it felt to her often that CUPs  
181 are almost in the reverse, almost a negative and almost a way to stop something so  
182 when a developer comes in with a project it is almost like they are “guilty until  
183 proven innocent”, kind of a reverse psychology and she was curious about that. She  
184 understood that it gives some controls, but does it sometimes give controls for wrong  
185 reasons.

186  
187 Ms. Trapp explained the reason why a CUP is generally the choice to use over  
188 rezoning in cities.

189  
190 Ms. Gundlach thought Chair Kimble was correct that not many of the buildings in  
191 Roseville are above 65 feet. Regarding the CUP, she thought the trick will be when  
192 they actually get the text amendment language in front of the Commission and have  
193 an opportunity to identify what are the conditions under which they would allow the  
194 increased height. They also have to remember that there are some general health,  
195 safety and welfare conditions that apply to all conditional uses. In addition to sort of  
196 the very project specific type of conditions and the problem with that being open to a  
197 lot of interpretation is a conditional use requires a public hearing with notification to  
198 the neighborhood and a lot of time the testimony that comes forward during those  
199 processes can impact the interpretation of those general health, safety, and welfare  
200 standards. If the Commission is not comfortable with that, it just needs to be  
201 recognized and discussed as a part of the text amendment language discussion.

202  
203 Ms. Gundlach also wanted the Commission to be very cognizant of the scope and  
204 timeline and the cost of this overall project. She indicated a few things have come up  
205 in the last meeting, a couple of things have come up in this meeting where there is  
206 interest in looking at certain things, but those things were not identified in the original  
207 scope and go beyond the mandatory updates that the City is talking about doing in  
208 order to comply with the City’s Comp. Plan and then they also were not noted as a  
209 part of the section two, which is the option updates. She thought examining the  
210 Conditional Use process was something that came up at the last meeting and then  
211 there was discussion briefly about sort of the build to designs which was also not  
212 identified in the scope and that is not to say those items cannot be looked at but it is  
213 going outside of the scope of the work that HKGi was hired to do, it is going to  
214 prevent them from meeting their timeline and it is going to prevent them from  
215 meeting their budget so that may be something that is set aside momentarily while  
216 staff seeks additional authorization to broaden the scope of the work. She did not  
217 want HKGi to be caught up in additional work as this item proceeds forward.

218  
219 Member McGehee explained she was cognizant of both issues but on the other hand  
220 she thought the issue of the Conditional Use and the issue of the build to actually  
221 came up as a part of this discussion because it would appear that they had different  
222 things for different districts and it seemed to her that what was growing out of this  
223 desire for consolidation was to actually pick one set of guidelines that would fit  
224 everything and she did not think that was anything that the Commission brought up.  
225 But if this is going to be a meaningful process to actually work a little bit on the City  
226 Code and Comprehensive Plan and planning this community going forward she

227 thought both the build forward and the Conditional Use Permit are important issues to  
228 discuss, and she hoped that HKGi can set those things aside and staff can bring it to  
229 the Council and see that the Commission is actually authorized to do this kind of  
230 work because she thought it was important. As far as the Conditional Use she saw it  
231 as a valuable way for example, in the case of HarMar or the case of any of these  
232 things that abut residential areas, the Conditional Use Permit would allow on a certain  
233 size parcel to get the build back and to get things pulled away or made somehow  
234 compatible with neighboring communities or neighboring business. She thought it  
235 provides flexibility for staff and for the Council and to some extent, for the developer  
236 to come up with things that work well all around.

237  
238 Member Schaffhausen in regard to scope, she explained one of the reasons why she is  
239 not asking a lot of questions is that not only is this the first time of her running  
240 through something like this but in addition to that, it seems like the main focus of this  
241 project is to align the Zoning with the Comp. Plan and she would look to staff in their  
242 help and guidance on what fits and what does not. It is not that she is not up for  
243 discussion, but it is just one of those where as far as the point and focus of this  
244 meeting is how do they want to proceed with this. She is equally cognizant of what  
245 staff is saying and respect that and appreciate the opportunity to have this discussion  
246 and personally she is in need of guidance on what is in scope and what is out of scope  
247 so that they can figure out what is relevant so that the Commission does not end up in  
248 a four-hour meeting but rather a two-hour meeting. Otherwise, they can go down a  
249 rabbit hole on all of this to a detail.

250  
251 Ms. Gundlach explained HKGi has tried really hard to put the language in red in both  
252 the print materials and presentation to highlight the high-level summary changes that  
253 staff thinks needs to occur to align with the Comprehensive Plan. She noted she did  
254 not want the Commissioners to feel that the other concerns are not important because  
255 she did agree that they are meaningful if they are going to do this discussion. That is  
256 why staff has asked the consultants to take notes on this and have this document for  
257 staff to go back to the Council with later on to show them the things that came out of  
258 this process that they were not able to address with the timeline, scope and budget and  
259 they can start thinking about the timeline, scope, and budget for those other items.

260  
261 Mr. Paschke explained the discussion about building forward and any of those design  
262 standards is definitely something that would be separate and take a lot of time and it  
263 would be helpful for staff to know if there are other Commissioners that share the  
264 same thoughts and opinions that Commission McGehee has on building forward  
265 design and/or any of the other design standards that the City has currently in Code.

266  
267 Ms. Trapp continued with the presentation regarding intensity standards in the Zoning  
268 Code.

269  
270 Mr. Miller reviewed the intent of the required updates of the Zoning Map and the next  
271 steps to take.

272

273 Chair Kimble explained regarding the next two meeting and the draft text  
274 amendments, trying to understand how extensive those are, and she wondered if there  
275 was any ability if these were going to be extensive to get them a little sooner than the  
276 Friday before the meeting.

277  
278 Mr. Miller indicated that could be discussed.

279  
280 Ms. Gundlach explained staff will try their best to get the changes out as soon as  
281 possible but it is really hard to try to turn this stuff around in less than a month.

282  
283 Member Leutgeb asked regarding community outreach, as a renter in Roseville, she  
284 wanted to make sure that the community outreach is not only including property  
285 owners but also tenants, both residential and business tenants because she thought  
286 these zoning changes regardless of whether they are required to align with the Comp.  
287 Plan or optional updates, may affect somebodies desire to continue leasing properties  
288 in the City.

289  
290 Ms. Gundlach indicated staff definitely notified property owners and renters of all  
291 residential property and one the business property, the property owner was notified.

292  
293 Mr. Lloyd did not think staff notified commercial tenants for this item.

294  
295 Ms. Gundlach indicated staff can look at including those people in the next  
296 notification.

297  
298 The Commission and staff discussed notification areas.

299  
300 **6. Adjourn**

301  
302 **MOTION**  
303 **Member Pribyl, seconded by Member Kruzel, to adjourn the meeting at 8:33**  
304 **p.m.**

305  
306 **Ayes: 7**  
307 **Nays: 0**  
308 **Motion carried.**

309

Prepared By



Department Approval

*Jamie Gundlach*

Agenda Section

**Public Hearings**

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**Item Description:** Consider a Request by Dogtopia (Standish Partners), in partnership with Clear Choice Property Management, LLC, for a Conditional Use to allow an outdoor pet exercise area in conjunction with a dog day care at 2216 County Road D (Tower Glen) **(PF21-005)**

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**1 APPLICATION INFORMATION**

2 Applicant:	Dogtopia
3 Property Owner:	Clear Choice Property Management, LLC
4 Location:	2216 County Road D
5 Application Submission:	04/27/21; deemed complete 05/06/21
6 City Action Deadline:	06/25/21
7 Planning File History:	NA
8 Zoning:	Community Business – Corridor Mixed Use

9 **LEVEL OF DISCRETION IN DECISION MAKING:** Action taken on a conditional use proposal is  
10 **quasi-judicial**; the City’s role is to determine the facts associated with the request, and apply  
11 those facts to the legal standards contained in State Statute and City Code.

**12 BACKGROUND**

13 Dogtopia is leasing approximately 4,000 sq. ft. of space located in the northeastern most unit  
14 (closest to County Road D and the railroad tracks) of the Tower Glen Shopping Center. Within  
15 the Dogtopia facility there will be dog daycare, spa, grooming and overnight boarding  
16 services, as well as a retail component. The proposal also includes a small 1,000 sq. ft.  
17 screened outdoor exercise area, which is the sole purpose of the requested conditional use  
18 request. The daycare, space, grooming, boarding and retail components are all permitted  
19 uses based upon underlying zoning. Documents depicting details regarding the proposed  
20 outdoor screened exercise enclosure are provided as Attachment C.

21 Zoning Code, sections §1009.02.C and §1009.02.D.12, set the criteria for reviewing general and  
22 specific conditional use requests. The Planning Division’s review of these criteria can be found  
23 in the below Conditional Use Analysis section.

**24 CONDITIONAL USE ANALYSIS**

25 **REVIEW OF GENERAL CONDITIONAL USE CRITERIA:** §1009.02.C of the Zoning Code establishes  
26 general standards and criteria for all conditional uses, which the Planning Commission and City  
27 Council must determine compliance with those stated findings.

28 The general code standards of §1009.02.C are as follows:

29 *The proposed use is not in conflict with the Comprehensive Plan.* While a dog daycare with  
30 outdoor exercise area does not appreciably advance the goals of the Comprehensive Plan aside  
31 from facilitating continued investment in a property, Planning Division staff believes it does not  
32 conflict with the Comprehensive Plan. More specifically, the General and Commercial Area  
33 Goals and Policies sections of the Comprehensive Plan include a number of policies related to  
34 reinvestment, redevelopment, and quality development. The proposed Dogtopia, and its outdoor  
35 exercise area, is one component of reinvestment in the Tower Glen Shopping Center, which  
36 would align with the related goals and polices of the Comprehensive Plan.

37 *The proposed use is not in conflict with a Regulating Map or other adopted plan.* The proposed  
38 use is not in conflict with such plans because none apply to the property.

39 *The proposed use is not in conflict with any City Code requirements.* Because this is a  
40 reinvestment into an existing tenant space that has otherwise been vacant for some time, there are  
41 few City Code items that apply to the reuse, short of obtaining necessary building permits. It is  
42 also worth noting, an animal indoor boarding/kennel/daycare, as is proposed, is a permitted use.  
43 Therefore, Planning Division staff have determined the applicant's proposal addresses all  
44 applicable requirements of the City Code as they pertain to the proposed use, tenant  
45 improvement, and CU; moreover, a CU approval can be rescinded if the approved use fails to  
46 comply with all applicable Code requirements or any conditions of the approval. As part of the  
47 building permit review process, Planning Division staff will conduct a complete Code  
48 compliance analysis, including zoning standards such as trash/recycling, signage, and any  
49 conditions of the CU.

50 *The proposed use will not create an excessive burden on parks, streets, and other public*  
51 *facilities.* Planning Division staff does not anticipate the proposal to intensify any practical  
52 impacts on parks, streets, or public infrastructure.

53 *The proposed use will not be injurious to the surrounding neighborhood, will not negatively*  
54 *impact traffic or property values, and will not otherwise harm the public health, safety, and*  
55 *general welfare.* Planning Division staff had concluded this proposed CU in support of an  
56 outdoor 1,000 square foot exercise area for dogs will not be injurious to the surrounding  
57 neighborhood, will not negatively impact traffic or property values, and will not otherwise harm  
58 the public health, safety, and general welfare. In fact, reuse of the vacant space will improve  
59 upon the public health, safety and general welfare of the immediate area due to disinvestment  
60 that tends to occur at vacant retail developments.

61 REVIEW OF SPECIFIC CONDITIONAL USE CRITERIA: §1009.02.D.1 of the Zoning Code establishes  
62 additional standards and criteria that are specific to Animal Boarding, Animal Day Care, Kennel  
63 facilities:

64 1. *Animal Boarding, Animal Day Care, Kennel: If outside exercise runs or other outdoor*  
65 *activities are contemplated, the following standards shall be met:*

66 a. *Outdoor dog runs or exercise areas shall be located at least 100 feet from a residentially*  
67 *zoned property or property in residential use or shall have, at the time of application for*  
68 *conditional use approval, the written support of all owners of such properties within 100*  
69 *feet.*

70 The nearest residential property to Tower Glen, and specifically the proposed Dogtopia  
71 space, is just over 1,200 feet. Therefore, no written support letter from residents is  
72 necessary or required.

73 b. *Any portion of an outdoor kennel facing an adjacent property shall be screened from*  
74 *view by a solid fence, hedge or similar plant material.*

75 The proposed outdoor exercise area will lie adjacent to the Dogtopia tenant space, the  
76 parking lot, and County Road D. The proposed plan is for a solid vinyl fence to be  
77 installed on three sides of the exercise area providing a full screen from the parking lot,  
78 adjacent tenants, and County Road D. Planning Division staff finds this acceptable,  
79 however will work with the applicant on the final fence design and height.

80 **PLANNING DIVISION RECOMMENDATION**

81 The Planning Division recommends approval of the CU to allow up to a 1,000 sq. ft. outdoor  
82 exercise area in the area that lies between the building and parking lot for Dogtopia, based on the  
83 submitted plans and subject to the following condition:

84 a. Planning Division staff work with the applicant on the final screen fence design and height.

85 **SUGGESTED PLANNING COMMISSION ACTION**

86 **By motion, recommend approval of a CONDITIONAL USE** for 2216 County Road D, allowing  
87 an outdoor exercise area, up to 1,000 sq. ft., associated with a dog day care and kennel by  
88 Dogtopia, based on the comments, findings, and the condition provided as part of this RPCA  
89 dated June 2, 2021.

90 **ALTERNATIVE ACTIONS**

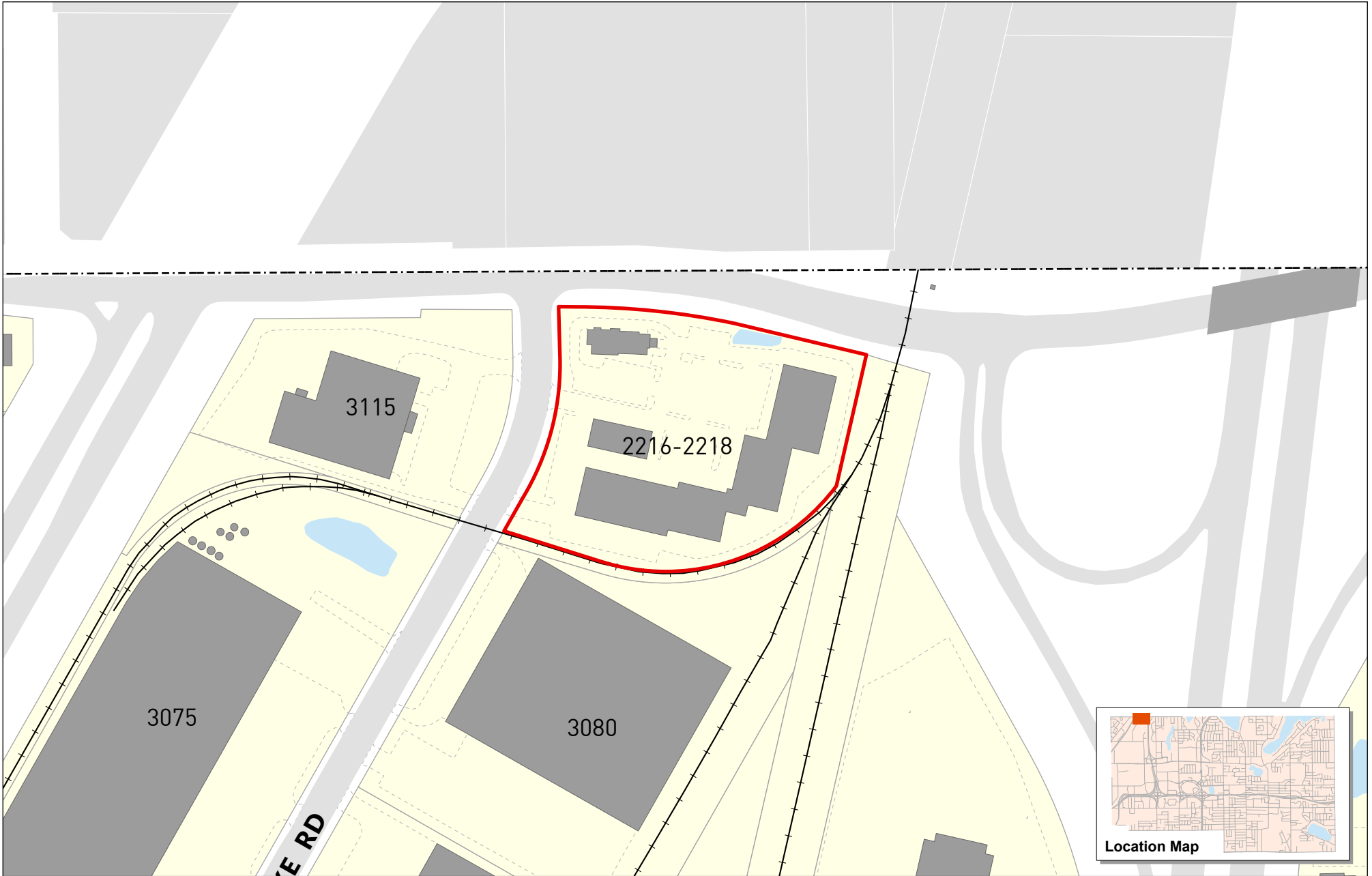
91 a. Pass a motion to table the item for future action. An action to table must be tied to the need  
92 for clarity, analysis, and/or information necessary to make a recommendation on the request.

93 b. Pass a motion recommending denial of the proposal. A motion to deny must include findings  
94 of fact germane to the request.

**Report prepared by: Thomas Paschke, City Planner, 651-792-7074 | [thomas.paschke@cityofroseville.com](mailto:thomas.paschke@cityofroseville.com)**

Attachments: A. Location Map B. Aerial photo  
C. Narrative/plans

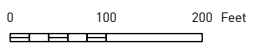
# Attachment A: Planning File 21-005



Site Location

**Data Sources**  
\* Ramsey County GIS Base Map [5/4/2021]  
For further information regarding the contents of this map contact:  
City of Roseville, Community Development Department,  
2660 Civic Center Drive, Roseville MN

**Disclaimer**  
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# Attachment B: Planning File 21-005



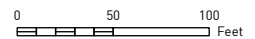
### Disclaimer

#### Data Sources

- \* Ramsey County GIS Base Map (5/4/2021)
- \* Aerial Data: Surdex (4/2020)

For further information regarding the contents of this map contact:  
City of Roseville, Community Development Department,  
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## Conditional Use Application

### 7. Additional Required Information:

#### a. Legal Description & PIN

Plat or Section / Township / Range 5 / 29 / 23 Legal: Beg At Intersection Of Se Line Of Long Lake Rd & Nly Line Of An 18 Ft Wide R/w For Mtr Cos Lead Track Th Ne & Nly Along Sd Rd Line to N Line Of Nw  $\frac{1}{4}$  Of Ne  $\frac{1}{4}$  Th E Thereon To Nw Line Of Mtr 100 Ft Wide R/w Th Sw Thereon to Nw Line of Sd 18 Ft Wide R/w Th Sw & W Thereon to Beg Subj To Hwy In Sec 5 Tn 29 Rn 23 (Parcel Number: 052923120002)

#### b. Proposed Plans:

Attached is a scaled site plan indicating the location of the outdoor play area. Also included is an example of the type of fence proposed and fencing specifications. We have also provided a photo of another Dogtopia play area as an example.

#### c. Narrative:

1. Standish Partners, Inc. DBA Dogtopia is requesting a conditional use permit to allow a fully fenced outdoor dog play area. The play area will be connected to our new Dogtopia retail space at Tower Glen Shopping Center and will be approximately 1,000 SF. Dogtopia facilities are state-of-the-art retail stores providing dog daycare, spa, grooming and overnight boarding services for the safety and convenience of residents and working professionals who commute into and through the Roseville area. There are currently over 150 Dogtopia stores nationwide and soon to be three in the Twin Cities area. Our proposed use supports the comprehensive plan by providing a service that is in short supply in the Roseville area. We also will be providing employment for a staff of approximately 15 and developing a retail space

that has been vacant for more than a year. The store will be locally and family owned.

2. The proposed use is not in conflict with any Regulating Maps or other adopted plans.
3. The proposed use is not in conflict with any City Code requirements.
4. The proposed use will not create an excessive burden on parks, streets and other public facilities.
5. Dogtopia facilities are good retail neighbors with a commitment to safety that is unsurpassed. Our health and safety is guided by a Dogtopia staff biologist, dog behaviorist and veterinarian. We will employ certified canine coaches who are extensively trained. Odor control is of paramount importance. Each playroom is on a separate HVAC system and has an outside air intake. Steril-Aire UVC emitters are installed and walls extend to the deck to avoid smell and sound transfer. There is great care and additional expense invested to soundproof the facility. Our use will not in any way negatively affect the surrounding neighborhood, negatively impact traffic or property values or otherwise harm public safety and general welfare.

Sarpino's Pizzeria  
Takeout • Delivery

Dogtopia

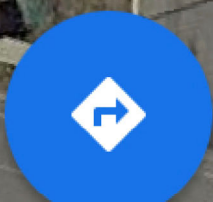
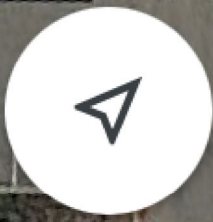
Proposed  
Outdoor  
Space

New Hong Kong Wok  
Takeout • Delivery

Caribou Coffee  
Takeout

Marathon Gas

Long Lake Rd





FOR  
LEASE

612.419.7577  
M

Prepared By

Agenda Section



**Public Hearings**

Department Approval

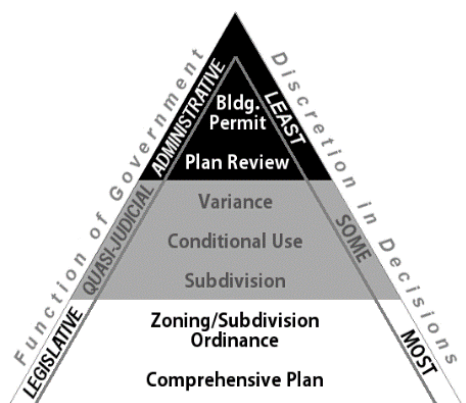
*Jamie Gundlach*

**Item Description:** Consider a Request by Heights Venture Architects, LLP, in conjunction with Wal-Mart Real Estate Business Trust (property owner), for a Conditional Use to allow a Drive-Through for a proposed Panda Express at 2030 Twin Lakes Parkway **(PF21-004)**

**1 APPLICATION INFORMATION**

2 Applicant:	Heights Venture Architects (Panda Express)
3 Location:	2030 Twin Lakes Parkway
4 Application Submission:	04/01/21; deemed complete 05/11/21
5 City Action Deadline:	July 3, 2021
6 Planning File History:	NA
7 Zoning:	Community Mixed-Use-4

8 **LEVEL OF DISCRETION IN DECISION MAKING:** Action taken  
9 on a conditional use proposal is **quasi-judicial**; the City’s role  
10 is to determine the facts associated with the request, and apply  
11 those facts to the legal standards contained in State Statute and  
12 City Code.



**13 BACKGROUND**

14 On October 26, 2020, the City Council approved a Zoning text  
15 amendment to include drive-throughs as a conditional  
16 accessory use in the Community Mixed-Use-4 district. This  
17 action was taken at the request of the applicant. Since that date,  
18 the Planning Division has been working with Heights Venture  
19 on the proposed Panda Express project.

20 As a component of the project, the applicant requested four variances which are to be considered  
21 by the Variance Board prior to the Planning Commission meeting. These variances seek relief  
22 from the following sections of the Zoning Code:

- 23 • §1005.07.E.2.a.i.B. *At least 50% of the lineal Build To Area shall be occupied by the front*  
24 *facade of the building.*
- 25 • §1005.07.E.2.a.i.C. *Within 30 feet of a block corner, the ground story facade shall be built*  
26 *within 10 feet of the corner.*
- 27 • §1005.07.E.2.a.ii.B. *Undeveloped and open space created in front of a building shall be*  
28 *designed as a semi-public space, outdoor seating, or other semi-public uses.*

- 29 • §1009.02.D.12.f. *A 10-foot buffer area with screen planting and/or an opaque wall or fence*  
30 *between 6 and 8 feet in height shall be required between the drive-through lane and any*  
31 *property line adjoining a public street or residentially zoned property or property in*  
32 *residential use and approved by the Community Development Department.*

33 Planning Division staff have included a number of development documents, mostly for reference  
34 purposes (Attachment C). The site plan is germane to the drive-through discussion as it details  
35 access, vehicle site circulation, and drive-through stacking. This report, and the associated  
36 documents, also assumes the four requested variances were approved by the Variance Board. If  
37 these variances are not supported, the Planning Commission may need to consider tabling action  
38 on the proposed Conditional Use if or until the applicant can provide a revised site plan based  
39 upon the action taken by the Variance Board.

40 As a component of the proposed redevelopment and related drive-through facility, and in order  
41 to comply with Public Works Department's policies regarding when traffic studies are required,  
42 a traffic study (Attachment D) was conducted to review existing operations, evaluate potential  
43 traffic impacts of the proposed redevelopment, review site access considerations, and  
44 recommend improvements to ensure safe and efficient operations.

45 The Planning Division emphasizes the requested conditional use (CU) is only related to the  
46 proposed drive-through. The CU does not apply to any other aspects of the proposed  
47 redevelopment project, which are governed by other sections of the Zoning Code. At the time of  
48 building permit submittal, Planning Division staff reviews more detailed plans to determine full  
49 compliance with all applicable zoning standards.

50 The Zoning Code §1009.02.C and §1009.02.D.12 set the criteria for reviewing general and  
51 specific conditional use requests. The Planning Division's review of these criteria can be found  
52 in the below Conditional Use Analysis section.

### 53 **CONDITIONAL USE ANALYSIS**

54 **REVIEW OF GENERAL CONDITIONAL USE CRITERIA:** §1009.02.C of the Zoning Code establishes  
55 general standards and criteria for all conditional uses, which the Planning Commission and City  
56 Council must determine compliance with those stated findings.

57 The general code standards of §1009.02.C are as follows:

- 58 **a.** *The proposed use is not in conflict with the Comprehensive Plan.* While a drive-through  
59 facility doesn't appreciably advance the goals of the Comprehensive Plan aside from  
60 facilitating continued investment in a property, Planning Division staff believes it does not  
61 conflict with the Comprehensive Plan. More specifically, the General and Commercial Area  
62 Goals and Policies sections of the Comprehensive Plan include a number of policies related  
63 to reinvestment, redevelopment, quality development, and scale. The proposed drive-through  
64 is one component of a larger investment, which would align with the related goals and  
65 polices of the Comprehensive Plan.
- 66 **b.** *The proposed use is not in conflict with a Regulating Map or other adopted plan.* The subject  
67 property is located within the Twin Lakes Redevelopment Area, an area which obtains most  
68 of its required development standards from a regulating plan, and specifically those  
69 applicable standards in §1005.07. In the case of the subject property it is required to comply  
70 with specific building placement and site design standards under the Urban Frontage  
71 (1005.07.E.2) and/or the Design Standards listed in §1005.02.A. In October of 2020, the  
72 City Council amended the Use Table (1005-5) to support drive-through lanes as an accessory

73 in the CMU-3, and CMU-4 districts. Planning Division staff has determined the proposed  
74 site design (including the drive-through lane) and building placement generally complies  
75 with the Urban Frontage and other applicable Regulating Plan standards and is not in conflict  
76 with said regulating plan.

77 c. *The proposed use is not in conflict with any City Code requirements.* Assuming the four  
78 requested variances were granted, Planning Division staff has determined the remainder of  
79 the project addresses all applicable requirements of the City Code as they pertain to the  
80 proposed drive-through CU. Moreover, a CU approval can be rescinded if the approved use  
81 fails to comply with all applicable Code requirements or any conditions of the approval. As  
82 part of the building permit review process, Planning Division staff will conduct a complete  
83 Code compliance analysis, including zoning standards such as landscaping, trash/recycling  
84 enclosures, vehicle parking, materials, etc.

85 d. *The proposed use will not create an excessive burden on parks, streets, and other public*  
86 *facilities.* Staff does not anticipate the proposal to intensify any practical impacts on parks,  
87 streets, or public infrastructure. A traffic study (Attachment D) completed for the subject  
88 development of a 2,300 sq. ft. drive-through restaurant indicates an expected generation of  
89 117 midday peak hour, 75 p.m. peak hour, and 1,083 daily trips. Also, according to the  
90 *Drive-Through Queue Generation Study* completed by Counting Cars in 2012, maximum  
91 queues at a fast food restaurant range between five (5) and 13 vehicles, with an 85th  
92 percentile queue of 12 vehicles. Based on the site plan provided, there is room for 12 vehicles  
93 to queue within the drive-through lane before impacting the internal parking lot circulation.  
94 Therefore, it is expected that the proposed drive-through will accommodate the majority of  
95 the expected drive-through queues at the site. Lastly, access to the site is via a right-in/right-  
96 out access 250 feet east of Cleveland Avenue on Twin Lakes Parkway. The site can be  
97 accessed from the Wal-Mart parking lot as well. Once the development is completed in 2023,  
98 the study intersections are expected to operate at acceptable overall LOS C or better during  
99 the midday and p.m. peak hours; queuing associated with the I-35W off ramps would be  
100 similar to the existing condition. There is not expected to be any queueing issues  
101 entering/exiting the public streets as a result of the additional trips to/from the proposed  
102 development.

103 e. *The proposed use will not be injurious to the surrounding neighborhood, will not negatively*  
104 *impact traffic or property values, and will not otherwise harm the public health, safety, and*  
105 *general welfare.* Generally, this proposed drive-through and restaurant will not be injurious  
106 to the surrounding neighborhood; negatively impact traffic or property values; and will not  
107 otherwise harm the public health, safety, and general welfare. Specifically, the Planning  
108 Division anticipates an approved drive-through for this site to increase vehicle trips on the  
109 adjacent roadways, however, not significantly, or consistently, or otherwise unmanageable  
110 under the proposed site access, drive-through, and circulation plan.

111 REVIEW OF SPECIFIC CONDITIONAL USE CRITERIA: §1009.02.D.12 of the Zoning Code establishes  
112 additional standards and criteria that are specific to drive-through facilities:

113 a. *Drive-through lanes and service windows shall be located to the side or rear of buildings*  
114 *and shall not be located between the principal structure and a public street except when the*  
115 *parcel and/or structure lies adjacent to more than one public street and the placement is*  
116 *approved by the Community Development Department (Ord. 1443, 6-17-2013).* The basis  
117 for this criterion is to limit vehicular impacts on adjacent roadways and more specifically the

118 pedestrian realm. The proposed drive-through lane is designed on the periphery of the lot,  
119 between the building and public street. The Planning Division has worked with the applicant  
120 to design the site in a manner that is not intrusive to pedestrians and vehicles given the drive-  
121 through location. It's worth recognizing the drive-through lane and building pad lie 10 feet  
122 above street grade and the sidewalks along Cleveland Avenue and Twin Lakes Parkway  
123 adjacent to the property. The topographic grade separation, coupled with the proposed  
124 landscaping and screen fencing proposed, will properly mitigate negative impacts this  
125 criterion exists to mitigate from.

126 **b.** *Points of vehicular ingress and egress shall be located at least 60 feet from the street right-*  
127 *of-way lines of the nearest intersection.* In review of this requirement, the proposed access to  
128 Panda Express is from the parking lot area shared with Walmart, which utilizes the site  
129 access from Twin Lakes Parkway. This interior site access lies in excess of 60 feet from any  
130 public street intersection and the ingress/egress to the overall site from Twin lakes Parkway.

131 **c.** *The applicant shall submit a circulation plan that demonstrates that the use will not interfere*  
132 *with or reduce the safety of pedestrian and bicyclist movements. Site design shall*  
133 *accommodate a logical and safe vehicle and pedestrian circulation pattern. Queuing lane*  
134 *space shall be provided, sufficient to accommodate demand, without interfering with primary*  
135 *driving, entrance, exit, pedestrian walkways, or parking facilities on site. The circulation*  
136 *plan shall be made a condition of approval and shall survive any and all users of the drive-*  
137 *through and may need to be amended from time to time to ensure continued compliance with*  
138 *this condition. Said amendments to the circulation plan will require an amendment to the*  
139 *conditional use.* The submitted vehicular and pedestrian circulation plan has been reviewed  
140 by Planning Division and Engineering staff and deemed acceptable. Given the elevation  
141 change from Cleveland Avenue and Twin Lakes Parkway, pedestrian access can only be  
142 achieved from Twin Lakes Parkway at the site access and from an existing sidewalk access  
143 midpoint between County Road C and Twin Lakes Parkway. The submitted circulation plan  
144 separates vehicle operations from the pedestrian circulation route. Pavement markings are  
145 proposed to delineate the pedestrian route across the drive-thru queue to promote driver  
146 awareness.

147 **d.** *Speaker box sounds from the drive-through lane shall not be loud enough to constitute a*  
148 *nuisance on an abutting residentially zoned property or property in residential use.*  
149 *notwithstanding this requirement, such speaker boxes shall not be located less than 100 feet*  
150 *from an existing residentially zoned property or property in residential use.* The nearest  
151 residential use to the subject site lies approximately a ½ mile away and is the Isaac apartment  
152 complex on Fairview Avenue. Additionally, the area around the development site includes  
153 lots of vehicular background noise generated from Cleveland Avenue and Interstate 35W.  
154 Sound from the speaker box is not expected to be audible at any residentially used property,  
155 or the hotels that lie to the north; approximately 480 feet (Home2 Suites) and 650 feet  
156 (Hampton Inn).

157 **e.** *Drive-through canopies and other structures, where present, shall be constructed from the*  
158 *same materials as the primary building and with a similar level of architectural quality and*  
159 *detailing.* The proposed drive-through does not provide a separate vehicle canopy, however  
160 it does include canopy features overhanging portions of the building and the drive-through  
161 (Attachment C). These overhangs are integrated into the design of the building to address  
162 vertical and horizontal articulation.



163 f. *A 10-foot buffer area with screen planting and/or an opaque wall or fence between 6 and 8*  
 164 *feet in height shall be required between the drive-through lane and any property line*  
 165 *adjoining a public street or residentially zoned property or property in residential use and*  
 166 *approved by the Community Development Department (Ord. 1443, 6-17-2013). Given the*  
 167 *distance of separation of the drive-through lane to the north and west property lines, there is*  
 168 *more than 10 feet to provide appropriate landscaping. This area also includes existing*  
 169 *maturing trees that will be preserved and designed around. Regarding the screen fence*  
 170 *requirement, this criterion is anticipated to receive a variance to install 4-foot tall versus 6-*  
 171 *foot tall drive-through lane screening sections augmented by landscaping, with the*  
 172 *justification of the variance being the topographic grade separation between the street and*  
 173 *drive-through lane. The proposed landscape plan, which includes these features, satisfies this*  
 174 *standard.*

175 **PLANNING DIVISION RECOMMENDATION**

176 The Planning Division recommends approval of the CU to allow Panda Express a drive-through  
 177 at 2030 Twin Lakes Parkway, based on the submitted site and development plans, subject to the  
 178 following condition:

179 a. The site, building, landscaping, and drive-through lane be constructed substantially  
 180 consistent with the plans submitted May 10, 2021 and provided as a component of the report  
 181 packet.

182 **SUGGESTED PLANNING COMMISSION ACTION**

183 **By motion, recommend approval of a CONDITIONAL USE** for 2030 Twin Lakes Parkway,  
 184 allowing a drive-through on the subject property based on the comments, findings, and the  
 185 conditions stated above of this report.

186 **ALTERNATIVE ACTIONS**

187 a. Pass a motion to table the item for future action. An action to table must be tied to the need  
 188 for clarity, analysis, and/or information necessary to make a recommendation on the request.  
 189 b. Pass a motion recommending denial of the proposal. A motion to deny must include findings  
 190 of fact germane to the request.

**Report prepared by: Thomas Paschke, City Planner, 651-792-7074 | [thomas.paschke@cityofroseville.com](mailto:thomas.paschke@cityofroseville.com)**

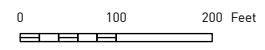
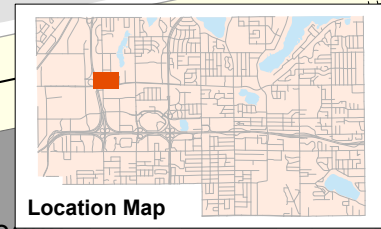
Attachments:      A. Location Map                                  B. Aerial photo  
                              C. Narrative/plans                                 D. Traffic study  
                              E. Interoffice memorandum

# Attachment A: Planning File 21-004



**Data Sources**  
 \* Ramsey County GIS Base Map [5/4/2021]  
 For further information regarding the contents of this map contact:  
 City of Roseville, Community Development Department,  
 2660 Civic Center Drive, Roseville MN

**Disclaimer**  
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# Attachment B: Planning File 21-004



Disclaimer



05-02-21

Community Development  
2660 Civil Center Drive  
Roseville, MN 55113

RE: VARIANCE AND CONDITIONAL USE NARRATIVE

The subject site is Lot 2, Block 1 of the Twin Lakes 2<sup>nd</sup> Addition, having an address of 2030 Twin Lakes Parkway and being approximately 56,725 sqft (1.30ac). The subject site is governed by the CMU-4 Zoning district and, at the October 6<sup>th</sup>, 2020 Planning Commission Hearing, the "Table of Uses" 1005.07-F was modified to change "Restaurant, Fast food w/ Drive-thru" from "NP" to "C" use. The following narrative describes the project and demonstrates compliance with applicable sections of Title 10 – Zoning, as well as the specific CMU-4 Zoning designation, including 1009.02.D.12 (a – f) Conditional Uses.

In Chapter 1005, the zoning requirements for commercial and mixed-use districts are described. The proposed building design complies with Section 1005.02.A Design Standards. The building is oriented to the corner of Twin Lakes Blvd and Cleveland Ave N. The proximity of the building is regulated by the existing 10' elevation change between the street sidewalk and the existing grade of the subject property. There is an existing 5' retaining wall along the subject property's entire frontage along Cleveland Ave, that supports a steep, vegetated slope up to the level graded area of the property, intended to hold a structure.

Operationally, the site is designed to deliberately separate vehicular drive-thru use and pedestrian use, with only one area of crossing paths. The vehicular use is on the opposite side of the building to the pedestrian use and is contained within the drive-thru lane and surrounding landscaping. The corner orientation of the building, the requirement to separate the vehicular paths from the pedestrian paths, the inability to connect to Cleveland Ave due to the existing topographical challenge, and the existing and proposed vegetative screening along Twin Lakes Parkway all support the location of the drive-thru lane and window facing Twin Lakes Parkway. The vehicular entrance to the drive-thru is 200' from the Twin Lakes Parkway access point on the North side of the site (Conditional Use 1009.02.D.12.a & b). The entrance to the drive-thru lane is accessed from the Panda Express parking lot and provides space for 12 vehicles (Conditional Use 1009.02.D.12.c – refer to Circulation plan at end of Narrative). The drive-thru lane and drive-thru window face North – facing Twin Lakes Parkway.

There are existing and well-developed evergreen and deciduous trees within an average of 20' wide landscape buffer between the drive-thru lane and the North property line. This landscape buffer is twice the required width and will contain the existing trees and shrubs as well as new screening shrubs. The existing trees extend approximately 15' above the proposed pavement of the drive-thru, are planted along the entire length of the Twin Lakes Parkway frontage and provide ample screening from the street (Conditional Use 1009.02.D.12.f). Regardless, we propose to include additional ground level vegetative screening of shrubs around the perimeter of the drive-thru lane to provide another layer of screening and headlight shielding along Twin Lakes Parkway and Cleveland Ave. Additionally, at the

request of Planning Staff, we have included a 4' high opaque fence that matches the material vocabulary of the building at the areas around the Drive-thru lane that have the highest opportunity for vehicular headlights to spill out onto Twin Lakes Parkway to the North and Cleveland Ave to the West.

The proposed impervious lot coverage is at 71.4% which is under the defined maximum of 85%, which is very well landscaped considering the property area includes the shared access road pavement. To touch on a final point in the Conditional Use requirements for Drive-thru facilities, the site is zoned CMU-4, a non-residential use, but does allow for Hotel use, however the location of the speaker box is more than 100' from any residential use or existing hotel use, or potentially any other future business or structure (Conditional Use 1009.02.D.12.d)

The pedestrian entrances are oriented toward both Cleveland Ave (Design Standards 1005.02.A.B) as well as the parking lot serving the building to the South. The Portals of Interaction for vehicles and pedestrians are separated to avoid conflict between the two groups, to clearly identify where pedestrians should interact and approach the building, and to promote a safe and enjoyable guest experience. The pedestrian entrances are articulated and identified by a cast stone portal flanked by storefront windows with direct views into and out of the dining room. The canopy over the drive-thru window extends from the metal band element wrapping around the building, where the depth grows and becomes a 4' deep canopy of the same material, color and style of the adjacent metal trim (Conditional Use 1009.02.D.12.e).

The building design is inspired by the modern vernacular, with clean rectilinear lines and a material palette consisting of subtle earth tones. The hard roughness of stone is contrasted with the sleek warmth of wood and are all bound together by conservative applications of pre-finished metal accents. The building is grounded with a continuous cast stone wainscot wrapping all sides of the building that expands at the Portals of Interaction to identify and accentuate the drive-thru window and pedestrian entrances. The facades facing Cleveland Ave and the building's parking field have storefront glass along 81% and 67% of the façade respectively providing an open and visual connection between outside and inside at the dining room. The entirety of the seating area has direct access to visible storefront glazing, natural light, and views.

A metal band wraps around the building and caps the top of the storefront, approximately the middle of the vertical façade. This metal band modulates its depth and function depending upon where it is located with respect to the plan. At the rear of the building, the band is compressed and subdued, but at the drive-thru window, the metal band extends out to 4' to become a cantilevered canopy for weather protection (Conditional Use 1009.02.D.12.e). At the storefront windows, the band becomes a subtle overhang to contain and finish the window elements.

At the top of the building, Fiberon boards are used to celebrate the warmth and texture of wood, but with the durability of a commercial material, providing both vertical and horizontal articulation, and complementing the warmth of natural wood used throughout the interior. All mechanical equipment on the roof is screened by a 4' to 5' high parapet wall bringing the overall building height to 24'. The remaining wall field is a tan and grey stucco that provides a neutral backdrop for the windows, metals, and wood elements. The building's material palette of Fiberon boards

replicating wood, Earth-tone stucco field, metal accent band, storefront glazing, and cast stone, is used on every façade and establishes a base, a middle, and framing elements. The building vocabulary creates a coherent composition, while maintaining and reinforcing the visual cues, such as the purposeful use of stone portals, and the pedestrian level storefront, to communicate the location of entries and dining rooms.

Specifically, to the CMU-4 Zoning regulations, the development generally complies with all portions of the CMU-4 zoning district, with the exception of the Urban Frontage Build-To Area requirement of section 1005.07.2.a.i defined by Regulating map Figure 1005-1. The southeast corner of Twin Lakes Parkway and Cleveland Avenue N. is defined as “Urban” for the hard corner, with “Flexible” frontage continuing East along Twin Lakes Parkway and south along Cleveland Ave. The proposed building placement complies with item D (and complies with Section 4 “Parking”) as the parking and drive lanes do not extend beyond (does not encroach upon) the build-to lines. However, per items C and B of Section 1005.07.2.a.i that defines the Build To area on the block corner within 10’ of the property line, the proposed development cannot meet this standard.

The existing site serves as an extension of the Wal-Mart parking lot being graded and leveled to align with the Wal-Mart building and Wal-Mart’s primary parking lot to the east. While the greater Wal-Mart development, including the proposed Panda outlot, maintains a level grade starting at the traffic circle on Twin Lakes Parkway, Twin Lakes Parkway slopes down and away from the traffic circle as it moves to connect with Cleveland Ave. This creates a **10’** grade change, over a distance of only 40’ to 50’ from the sidewalk along Cleveland Ave to the top of the proposed site. A 5’ high retaining wall was constructed as part of the Cleveland Ave road improvements across the entire Lot frontage along Cleveland Ave to accommodate the extreme differences between Cleveland Ave and the existing site.

*[1005.07.2.a.i.B (Variance Request) || 1005.07.2.a.i.C (Variance Request)  
Request for Variance to Items C and B of Section 1005.07.2.a.i that defines the Build To area on the block corner. The hardship for item C causes the non-compliance with item B. ]*

To comply with Item C, and by extension Item B, the proposed building would be over 10’ above the sidewalk level, and require another 10’ retaining wall that starts at the hard corner, and continues for nearly the entire extent of frontage along Cleveland Ave. Another 10’ high retaining wall would extend from the hard corner east along Twin Lakes Parkway all the way to the existing access drive connecting Twin Lakes to the Wal-Mart parking lot. The well-established evergreen and deciduous trees along Twin Lakes Parkway would be cut down, and new trees would be planted in front of the retaining wall. Given the topographical differences between building height and adjacent sidewalk being 10’ lower, the building and drive-thru window would not be screened at all by vegetation. The existing and new retaining walls would physically and psychologically cut off the building from the street below.

The goal and intent of providing an urban streetscape that is accessible and inviting to pedestrians could never be achieved, and strict adherence to the Urban Frontage requirement would negatively impact that intent, resulting in an obtrusive, oppressive, and unattractive element at the gateway that is Twin Lakes Parkway. The building is currently sited to the corner to respect the intent of the Urban Frontage, but also addresses existing and future pedestrian traffic patterns by providing

pedestrian access not from Cleveland Ave, but from the East, where pedestrians will be traveling from Wal-Mart, from the vicinity of the traffic circle, and from the nearby hotels. The I-35 corridor to the West of the site provides a barrier from which only vehicles will travel. Pedestrians will be coming from the East, from existing and future development of the Twin Lakes District. Due to the topography and the extreme grade change of the hard corner, we cannot provide a semi-public space, or transition space between public and private, and therefore request a variance to manage the existing topographical challenges of this site.

**Eric J. Abeln, AIA, NCARB**  
Partner

## **Heights Venture**

Architecture + Design

**Direct: 281.854.6119**

Cell: 312.507.1869

Office: 713.869.1103

E-mail: [eric.abeln@hva.cc](mailto:eric.abeln@hva.cc)

[www.heightsventure.com](http://www.heightsventure.com)

***REALIZING YOUR VISION...***



PANDA EXPRESS, INC.  
1683 Walnut Grove Ave.  
Rosemead, California  
91770  
Telephone: 626.799.9898  
Facsimile: 626.372.8288

All ideas, designs, arrangement and plans indicated or represented by this drawing are the property of Panda Express Inc. and were created for use on this specific project. None of these ideas, designs, arrangements or plans may be used by or disclosed to any person, firm, or corporation without the written permission of Panda Express Inc.

REVISIONS:

NO.	DESCRIPTION	DATE

ISSUE DATE:

VARIANCE/CUP	04.01.2021

DRAWN BY: JD

PANDA PROJECT #: S8-21-D7560  
ARCH PROJECT #: D7560  
19158






HOUSTON 1111 North Loop West, Suite 800 713 869 1103 V  
DALLAS 5741 Legacy Drive, Suite 320 Plano, Texas 75024 972 490 7292 V

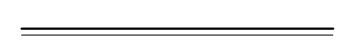

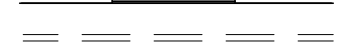
PANDA EXPRESS  
TRUE WARM & WELCOME  
2030 TWIN LAKE PARKWAY  
ROSEVILLE, MN 55113

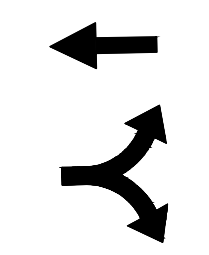
SITE PLAN

C03.0

LEGEND:

-  PROPOSED CONCRETE
-  PROPOSED SIDEWALK
-  PROPOSED FULL DEPTH ASPHALT

-  V612 CURB AND GUTTER (MNDOT) UNLESS NOTED OTHERWISE
-  V612 CURB AND GUTTER DEPRESSED (MNDOT)
-  EXISTING CURB & GUTTER



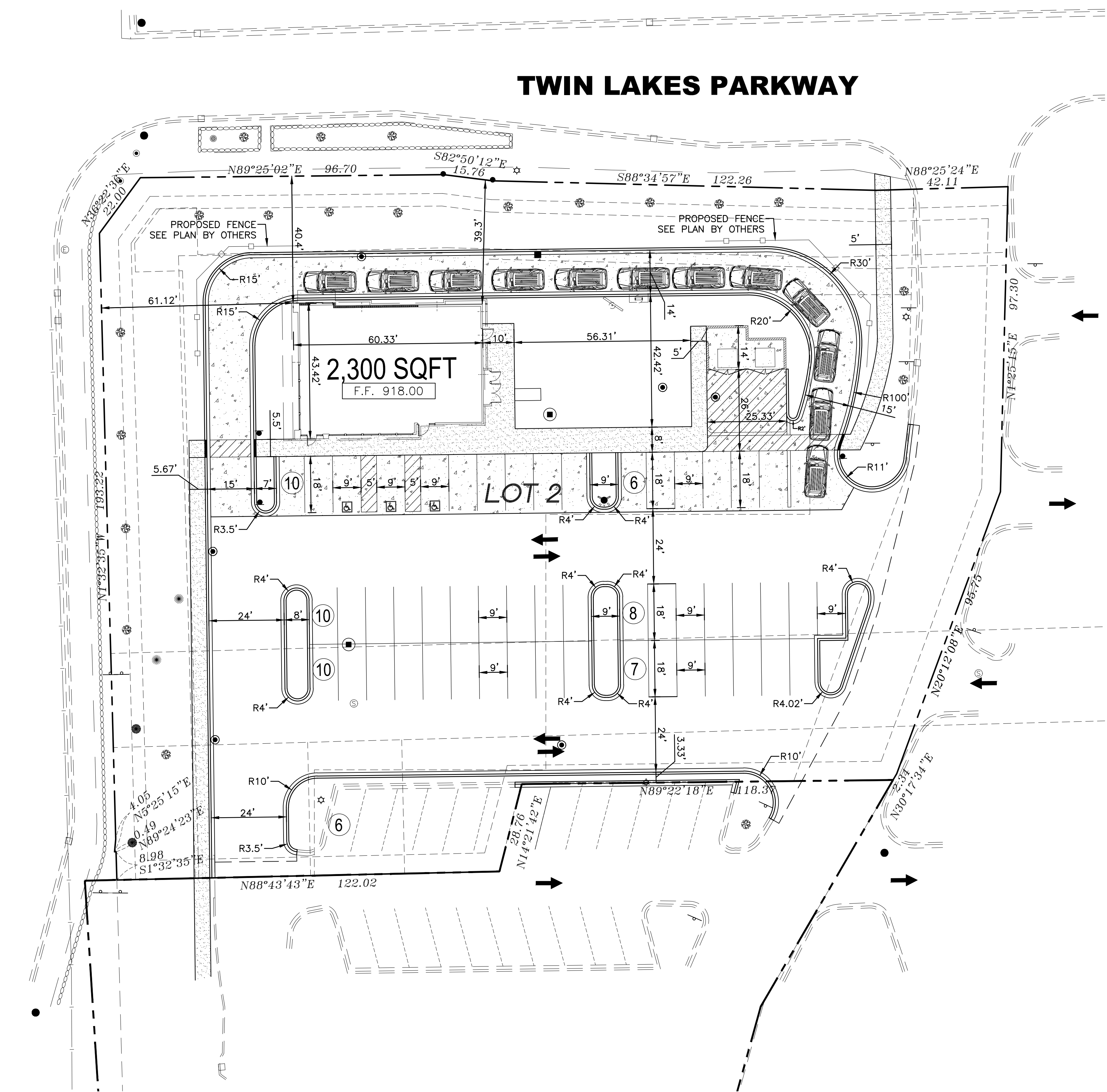
TRAFFIC FLOW DIRECTION/  
STRIPING WHERE NOTED

PARKING SUMMARY:

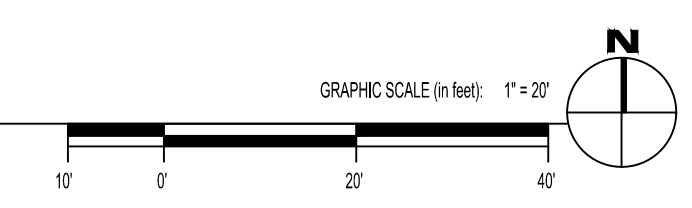
	PROP
REGULAR STALLS (8'X18')	54
HANDICAP STALLS (16'X18')	3
TOTAL	57

CLEVELAND AVENUE

TWIN LAKES PARKWAY



SITE PLAN



TO FILE A LOCATE REQUEST: 811 | 651-454-0002 | 1-800-252-1166  
EXCAVATORS ARE REQUIRED TO NOTIFY GSOC 48 HOURS IN ADVANCE, EXCLUDING WEEKENDS AND HOLIDAYS BEFORE BEGINNING EXCAVATION.  
GOPHER STATE ONE CALL IS AVAILABLE FOR EMERGENCY CALLS 24/7. EMERGENCY LOCATES ONLY: 1-866-640-3637





PANDA EXPRESS, INC.  
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Telephone: 626.799.9898  
Facsimile: 626.372.8288

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REVISIONS:


ISSUE DATE:

VARIANCE/CUP	04.01.2021

DRAWN BY: JD

PANDA PROJECT #: S8-21-D7560  
ARCH PROJECT #: D7560  
19158

rtm  
engineering consultants  
650 E. Algonquin Road, Suite 250  
Schaumburg, IL 60173 T: 857.756.4180  
www.rtmassociates.com

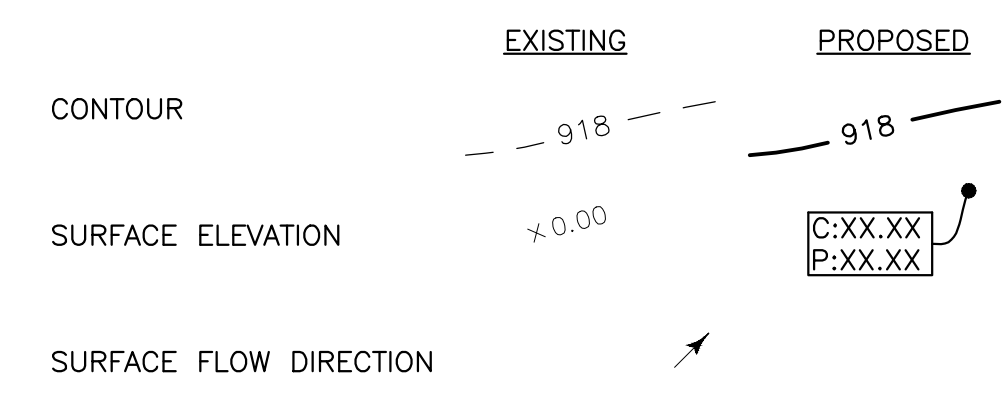
**Heights Venture**  
ARCHITECTURE • DESIGN

HOUSTON 1111 North Loop West, Suite 800 713 869 1103 V  
DALLAS 5741 Legacy Drive, Suite 320 Plano, Texas 75024 972 490 7292 V

**PANDA EXPRESS**  
TRUE WARM & WELCOME  
2030 TWIN LAKE PARKWAY  
ROSEVILLE, MN 55113

GRADING PLAN  
C05.0

**LEGEND:**



- SURFACE SPOT ELEVATION ABBREVIATIONS
- F.F. FINISHED FLOOR
  - P PAVING
  - R RIM
  - W WALK

**BENCHMARKS**

RAMSEY COUNTY BENCHMARK NO. 9141  
ELEVATION: 906.83 (NAVD 88 DATUM)

**BASIS OF ELEVATIONS**

PROJECT ELEVATIONS ARE NAVD88, BASED UPON GPS OBSERVATIONS FROM THE COUNTY OF RAMSEY BENCHMARK DATA.

**BASIS OF BEARING**

BEARINGS ARE BASED ON THE RAMSEY COUNTY COORDINATE SYSTEM.

**FLOODPLAIN STATEMENT**

THIS SITE IS WITHIN ZONE X - AREA OF MINIMAL FLOOD HAZARD AS DETERMINED BY THE FLOOD INSURANCE RATE MAP COMMUNITY NUMBER 2725 PANEL NO. 0020 SUFFIX G BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY, EFFECTIVE DATE JUNE 4, 2010.

**GENERAL NOTES**

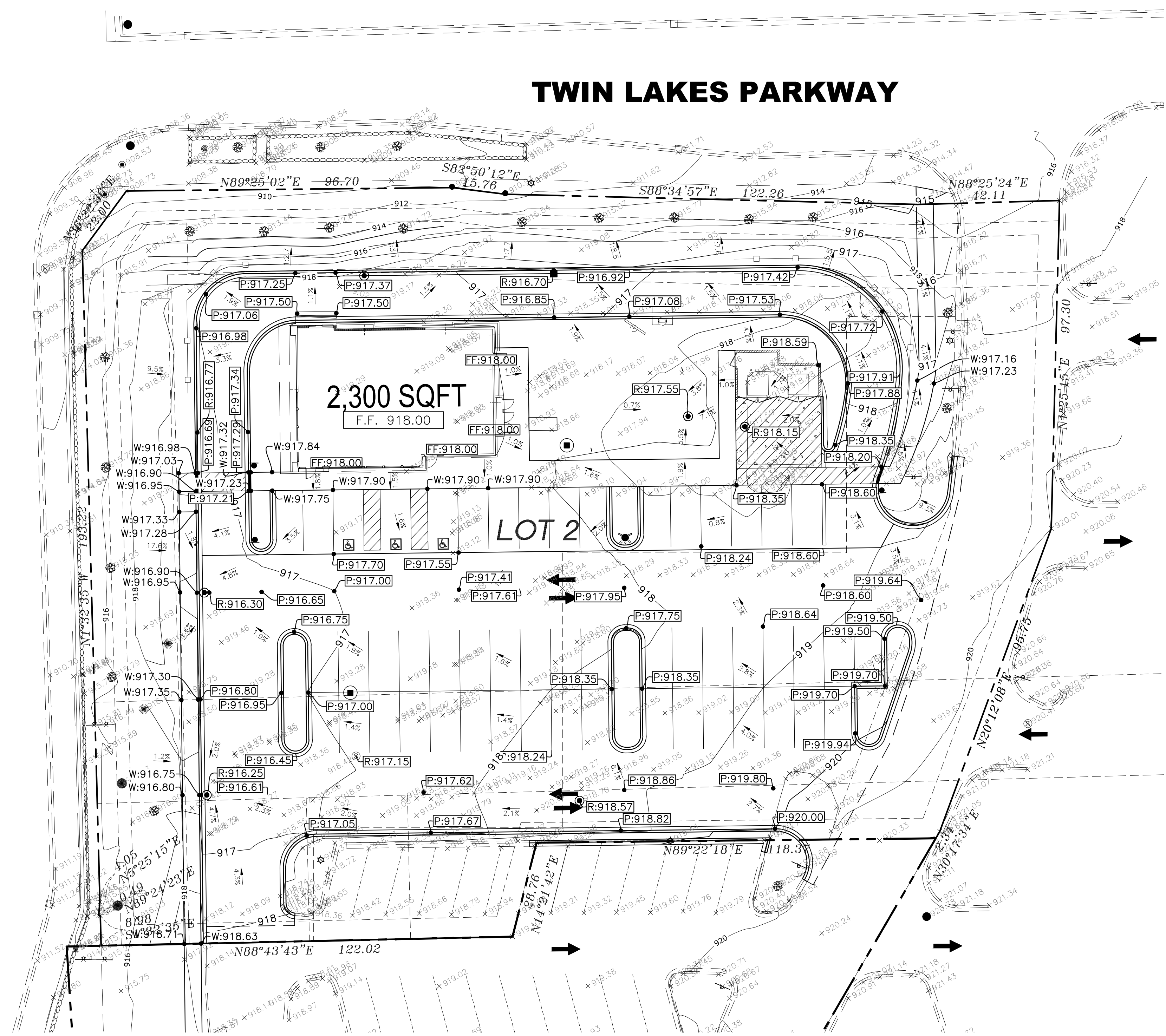
1. CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL INFORMATION FOR FINAL ACCEPTANCE OF WORK BY ALL FEDERAL, STATE, LOCAL, ETC. AGENCIES OR JURISDICTIONS HAVING APPROVAL AUTHORITY OVER WORK. THIS WORK INCLUDES BUT IS NOT LIMITED TO RECORD DRAWINGS, CERTIFICATIONS, INSPECTIONS AND/OR REPORTS.
2. SURVEYOR TO OBTAIN CAD FILE FROM ENGINEER AND VERIFY ALL HORIZONTAL CONTROL DIMENSIONING PRIOR TO CONSTRUCTION STAKING. SURVEYOR SHALL VERIFY ALL BENCHMARK, BASIS OF BEARINGS AND DATUM INFORMATION TO ENSURE IMPROVEMENTS WILL BE AT THE SAME HORIZONTAL AND VERTICAL LOCATIONS SHOWN ON THE DESIGN CONSTRUCTION DRAWINGS. PRIOR TO CONSTRUCTION STAKING ANY DISCREPANCY SHALL BE REPORTED TO OWNER AND ENGINEER PRIOR TO CONTINUATION OF ANY FURTHER STAKING OR CONSTRUCTION WORK.
3. CONTRACTOR SHALL COORDINATE WORK WITH UTILITY COMPANIES AND CITY PRIOR TO BEGINNING WORK AND IS RESPONSIBLE FOR ALL MATERIALS, LABOR, REPAIRS, ETC. TO COMPLETE WORK AND RESTORE AREA TO SAME STATE PRIOR TO STARTING WORK.
4. PROPOSED IMPROVEMENTS SHOWN ON THESE PLAN ARE BASED UPON THE PROPOSED IMPROVEMENTS BY OTHERS. PRIOR TO STAKING SURVEYOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL IMPROVEMENTS BY OTHERS AT THE LOCATIONS. VERIFICATION SHALL BE BY LOCATING THE IMPROVEMENTS IN THE FIELD AND/OR CONFIRMING THE HORIZONTAL AND VERTICAL CONTROL OF THE LATEST PLANS BY OTHERS MATCHES THE PROPOSED IMPROVEMENTS.
5. SURVEY PREPARED BY E.G. RUD & SONS, INC. FROM LINO, MN DATED AUGUST 27, 2020.

GSOC 811  
GOPHER STATE ONE CALL

TO FILE A LOCATE REQUEST: 811 (651-454-0002) | 1-800-252-1166  
EXCAVATORS ARE REQUIRED TO NOTIFY GSOC 48 HOURS IN ADVANCE, EXCLUDING WEEKENDS AND HOLIDAYS BEFORE BEGINNING EXCAVATION.

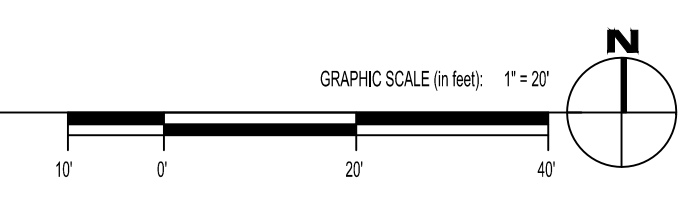
GOPHER STATE ONE CALL IS AVAILABLE FOR EMERGENCY CALLS 24/7.  
EMERGENCY LOCATES ONLY: 1-866-640-3637

**TWIN LAKES PARKWAY**



**CLEVELAND AVENUE**

**OVERALL GRADING PLAN**





PANDA EXPRESS INC.  
1683 Walnut Grove Ave.  
Rosemead, California  
91770  
Telephone: 626.799.9898  
Facsimile: 626.372.8288

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REVISIONS:

NO.	DESCRIPTION	DATE

ISSUE DATE:

VARIANCE/CUP	04.01.2021

DRAWN BY: LML

PANDA PROJECT #: S8-21-D7560  
PANDA STORE #: D7560  
ARCH PROJECT #: 19158



**Heights Venture**  
ARCHITECTURE • DESIGN  
HOUSTON 1111 North Loop West, Suite 800 Houston, Texas 77008  
DALLAS 517 Legacy Drive, Suite 240 Plano, Texas 75024  
713.869.1103 V 972.490.7292 V

**PANDA EXPRESS**  
TRUE WARM & WELCOME  
2030 TWIN LAKE PARKWAY  
ROSEVILLE, MN 55113

PLANTING PLAN

LP-1

TRUE WARM & WELCOME 2300 R5

PLANTING LEGEND

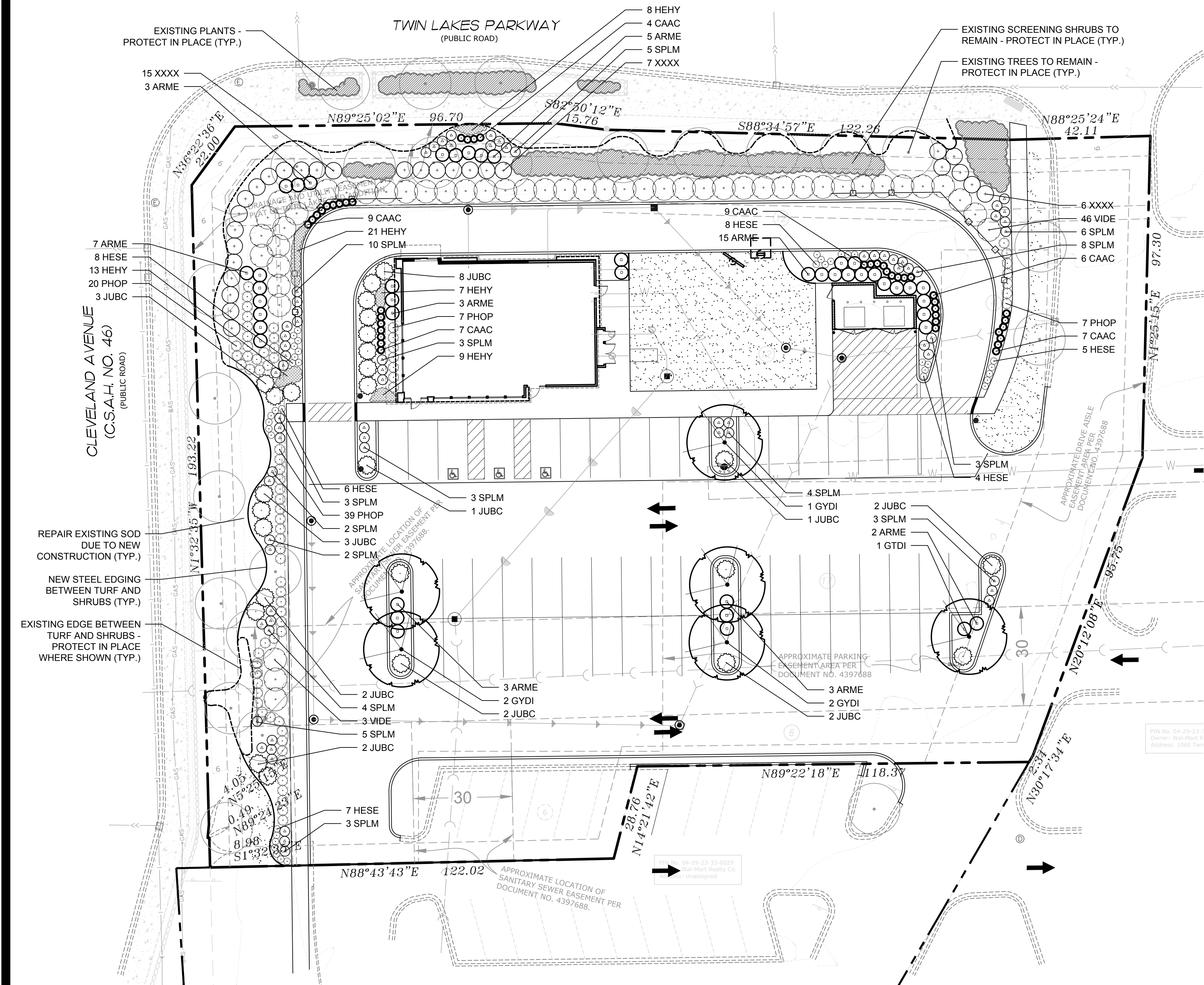
SYMBOL	BOTANIC NAME	COMMON NAME	MIN. SIZE	SPACING	QUANTITY	REMARKS
<b>TREES</b>						
GYDI	Gymnocladus dioica 'Espresso'	Espresso Coffee Tree	2.5" cal., 10'-12' h	Per plan	6	
NOTE: ALL TREES SHALL BE B&B. ROOTBALL SIZE AS APPROPRIATE FOR THE CALIPER SPECIFIED. SEE SPECIFICATIONS FOR PROPER ROOT QUALITY.						
<b>SHRUBS</b>						
ARME	Aronia melanocarpa	Black Chokeberry	#5 cont.	4' o.c.	43	
JUBC	Juniperus horizontalis 'Blue Chip'	Blue Chip Juniper	#5 cont.	5' o.c.	26	
PHOP	Physocarpus opulifolius 'SMPOTW'	Tiny Wine Ninebark	#5 cont.	3' o.c.	73	
SPLM	Spiraea japonica 'Monhub'	Limemound Spiraea	#5 cont.	3' o.c.	69	
VIDE	Viburnum dentatum	Arrowwood Viburnum	#5 cont.	6' o.c.	49	
XXXX	Unknown existing species	---	#5 cont.	5' o.c.	28	Match existing screening shrubs
<b>PERENNIALS AND ORNAMENTAL GRASSES</b>						
CAAC	Catagrostis acutiflora 'Karl Forster'	Karl Forster Reed Grass	#1 cont.	24" o.c.	42	
HEHY	Hemerocallis hybrids	Daylily	#1 cont.	18" o.c.	58	Peach colors
HESE	Helictotrichon sempervirens	Blue Avena	#1 cont.	24" o.c.	38	
<b>TURF</b>						
	Poa pratensis	Kentucky Bluegrass	Sod	---	---	

LANDSCAPE CALCULATIONS

<b>STREET TREES</b>	
FRONTAGE LENGTH - CLEVELAND AVE.:	229 LF
STREET TREES REQUIRED:	8 TREES (1 PER 30 LF OF FRONTAGE)
STREET TREES PROVIDED:	9 TREES (EXISTING)
<b>FRONTAGE LENGTH - TWIN LAKES PKWY.:</b>	
STREET TREES REQUIRED:	235 LF
STREET TREES PROVIDED:	8 TREES (1 PER 30 LF OF FRONTAGE)
STREET TREES PROVIDED:	9 TREES (EXISTING)
<b>SITE TREES</b>	
GREATER OF:	
TREES REQUIRED PER BUILDING GFA:	3 TREES (2,300 SF/1,000)
TREES REQUIRED PER PERIMETER LENGTH:	19 TREES (929 LF / 50)
<b>TOTAL TREES REQUIRED:</b>	19 TREES
<b>TREES PROVIDED:</b>	25 TREES (6 PROPOSED, 19 EXISTING)

GENERAL GRADING AND PLANTING NOTES

- BY SUBMITTING A PROPOSAL FOR THE LANDSCAPE PLANTING SCOPE OF WORK, THE CONTRACTOR CONFIRMS THAT HE HAS READ, AND WILL COMPLY WITH, THE ASSOCIATED NOTES, SPECIFICATIONS, AND DETAILS WITH THIS PROJECT.
- THE GENERAL CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL EXISTING VEGETATION (EXCEPT WHERE NOTED TO REMAIN).
- IN THE CONTEXT OF THESE PLANS, NOTES, AND SPECIFICATIONS, "FINISH GRADE" REFERS TO THE FINAL ELEVATION OF THE SOIL SURFACE (NOT TOP OF MULCH) AS INDICATED ON THE GRADING PLANS.
  - BEFORE STARTING WORK, THE LANDSCAPE CONTRACTOR SHALL VERIFY THAT THE ROUGH GRADES OF ALL LANDSCAPE AREAS ARE WITHIN +/-0.1' OF FINISH GRADE. SEE SPECIFICATIONS FOR MORE DETAILED INSTRUCTION ON TURF AREA AND PLANTING BED PREPARATION.
  - CONSTRUCT AND MAINTAIN FINISH GRADES AS SHOWN ON GRADING PLANS, AND CONSTRUCT AND MAINTAIN SLOPES AS RECOMMENDED BY THE GEOTECHNICAL REPORT. ALL LANDSCAPE AREAS SHALL HAVE POSITIVE DRAINAGE AWAY FROM STRUCTURES AT THE MINIMUM SLOPE SPECIFIED IN THE REPORT AND ON THE GRADING PLANS, AND AREAS OF POTENTIAL PONDING SHALL BE REGRADED TO BLEND IN WITH THE SURROUNDING GRADES AND ELIMINATE PONDING POTENTIAL.
  - THE LANDSCAPE CONTRACTOR SHALL DETERMINE WHETHER OR NOT THE EXPORT OF ANY SOIL WILL BE NEEDED, TAKING INTO ACCOUNT THE ROUGH GRADE PROVIDED, THE AMOUNT OF SOIL AMENDMENTS TO BE ADDED (BASED ON A SOIL TEST, PER SPECIFICATIONS), AND THE FINISH GRADES TO BE ESTABLISHED.
  - ENSURE THAT THE FINISH GRADE IN SHRUB AREAS IMMEDIATELY ADJACENT TO WALKS AND OTHER WALKING SURFACES, AFTER INSTALLING SOIL AMENDMENTS, IS 3" BELOW THE ADJACENT FINISH SURFACE, IN ORDER TO ALLOW FOR PROPER MULCH DEPTH. TAPER THE SOIL SURFACE TO MEET FINISH GRADE, AS SPECIFIED ON THE GRADING PLANS, AT APPROXIMATELY 18" AWAY FROM THE WALKS.
  - ENSURE THAT THE FINISH GRADE IN TURF AREAS IMMEDIATELY ADJACENT TO WALKS AND OTHER WALKING SURFACES, AFTER INSTALLING SOIL AMENDMENTS, IS 1" BELOW THE FINISH SURFACE OF THE WALKS. TAPER THE SOIL SURFACE TO MEET FINISH GRADE, AS SPECIFIED ON THE GRADING PLANS, AT APPROXIMATELY 18" AWAY FROM THE WALKS.
  - SHOULD ANY CONFLICTS AND/OR DISCREPANCIES ARISE BETWEEN THE GRADING PLANS, GEOTECHNICAL REPORT, THESE NOTES AND PLANS, AND ACTUAL CONDITIONS, THE CONTRACTOR SHALL IMMEDIATELY BRING SUCH ITEMS TO THE ATTENTION OF THE LANDSCAPE ARCHITECT, GENERAL CONTRACTOR, AND OWNER.
- ALL PLANT LOCATIONS ARE DIAGRAMMATIC. ACTUAL LOCATIONS SHALL BE VERIFIED WITH THE LANDSCAPE ARCHITECT OR DESIGNER PRIOR TO PLANTING. THE LANDSCAPE CONTRACTOR SHALL ENSURE THAT ALL REQUIREMENTS OF THE PERMITTING AUTHORITY ARE MET (I.E., MINIMUM PLANT QUANTITIES, PLANTING METHODS, TREE PROTECTION METHODS, ETC.).
  - THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR DETERMINING PLANT QUANTITIES; PLANT QUANTITIES SHOWN ON LEGENDS AND CALLOUTS ARE FOR GENERAL INFORMATION ONLY. IN THE EVENT OF A DISCREPANCY BETWEEN THE PLAN AND THE PLANT LEGEND, THE PLANT QUANTITY AS SHOWN ON THE PLAN (FOR INDIVIDUAL SYMBOLS) OR CALLOUT (FOR GROUNDCOVER PATTERNS) SHALL TAKE PRECEDENCE.
  - NO SUBSTITUTIONS OF PLANT MATERIALS SHALL BE ALLOWED WITHOUT THE WRITTEN PERMISSION OF THE LANDSCAPE ARCHITECT. IF SOME OF THE PLANTS ARE NOT AVAILABLE, THE LANDSCAPE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT IN WRITING VIA PROPER CHANNELS.
  - THE CONTRACTOR SHALL, AT A MINIMUM, PROVIDE REPRESENTATIVE PHOTOS OF ALL PLANTS PROPOSED FOR THE PROJECT. THE CONTRACTOR SHALL ALLOW THE LANDSCAPE ARCHITECT AND THE OWNER/OWNER'S REPRESENTATIVE TO INSPECT, AND APPROVE OR REJECT, ALL PLANTS DELIVERED TO THE JOBSITE. REFER TO SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS FOR SUBMITTALS.
- THE CONTRACTOR SHALL MAINTAIN THE LANDSCAPE IN A HEALTHY CONDITION FOR 90 DAYS AFTER ACCEPTANCE BY THE OWNER. REFER TO SPECIFICATIONS FOR CONDITIONS OF ACCEPTANCE FOR THE START OF THE MAINTENANCE PERIOD, AND FOR FINAL ACCEPTANCE AT THE END OF THE MAINTENANCE PERIOD.
- SEE SPECIFICATIONS AND DETAILS FOR FURTHER REQUIREMENTS.

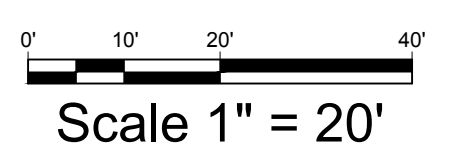


ROOT BARRIERS

THE CONTRACTOR SHALL INSTALL ROOT BARRIERS NEAR ALL NEWLY-PLANTED TREES THAT ARE LOCATED WITHIN FIVE (5) FEET OF PAVING OR CURBS. ROOT BARRIERS SHALL BE "CENTURY" OR "DEEP-ROOT" 24" DEEP PANELS (OR EQUAL). BARRIERS SHALL BE LOCATED IMMEDIATELY ADJACENT TO HARDSCAPE. INSTALL PANELS PER MANUFACTURER'S RECOMMENDATIONS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR USE ROOT BARRIERS OF A TYPE THAT COMPLETELY ENCIRCLE THE ROOTBALL.

MULCHES

AFTER ALL PLANTING IS COMPLETE, CONTRACTOR SHALL INSTALL 4" THICK LAYER OF RIVER COBBLE, 1"-2" DIA., OVER LANDSCAPE FABRIC (ANY APPROVED) IN ALL PLANTING AREAS (EXCEPT FOR TURF AND SEEDED AREAS). CONTRACTOR SHALL SUBMIT SAMPLES OF ALL MULCHES TO LANDSCAPE ARCHITECT AND OWNER FOR APPROVAL PRIOR TO CONSTRUCTION. ABSOLUTELY NO EXPOSED GROUND SHALL BE LEFT SHOWING ANYWHERE ON THE PROJECT AFTER MULCH HAS BEEN INSTALLED (SUBJECT TO THE CONDITIONS AND REQUIREMENTS OF THE "GENERAL GRADING AND PLANTING NOTES" AND SPECIFICATIONS).



I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED LANDSCAPE ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.  
SIGNATURE: Lawrence M. Lesser  
TYPED NAME: LAWRENCE M. LESSER  
DATE: 5/11/2021 LICENSE NUMBER: 56666



PANDA EXPRESS INC.  
1683 Walnut Grove Ave.  
Rosemead, California  
91770  
Telephone: 626.799.9898  
Facsimile: 626.372.8288

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REVISIONS:


ISSUE DATE:

VARIANCE/CUP	04.01.2021

DRAWN BY: LML

PANDA PROJECT #: S8-21-D7560  
PANDA STORE #: D7560  
ARCH PROJECT #: 19158

Scott Roselius, AIA  
MN Registration #44406

Not for Regulatory Approval,  
Permit, or Construction.



HOUSTON 1111 North Loop West, Suite 800 Houston, Texas 77008 713 869 1103 V  
DALLAS 5741 Legacy Drive, Suite 300 Plano, Texas 75024 972 490 7292 V

PANDA EXPRESS

TRUE WARM & WELCOME  
2030 TWIN LAKE PARKWAY  
ROSEVILLE, MN 55113

CIRCULATION PATH

CP-1

TWIN LAKES PARKWAY  
(PUBLIC ROAD)

Stop Sign

Pedestrian Xing

DT Exit Signage

CLEVELAND AVENUE  
(C.S.A.H. NO. 46)  
(PUBLIC ROAD)

Main Entrance

Secondary Entrance/Exit

Pedestrian Xing

Yield to Peds Signage

Shared Access Drive

Wal-Mart Parking Lot

Shared Access Drive

to ROW Sidewalk and Ramps

Drive-Thru Entry Path

Pedestrian Path

Drive-Thru Exit Path

Scale 1" = 20'



Drive-Thru Exit Signage



Stop Sign



Yield to Peds Sign



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91770  
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**Heights Venture**  
ARCHITECTURE DESIGN

HOUSTON 1111 North Loop West, Suite 800 Houston, Texas 77008 713 869 1103 V  
DALLAS 5741 Legacy Drive, Suite 320 Plano, Texas 75024 972 490 7292 V

**PANDA EXPRESS**  
TRUE WARM & WELCOME  
2030 TWIN LAKE PARKWAY  
ROSEVILLE, MN 55113

**A-203**

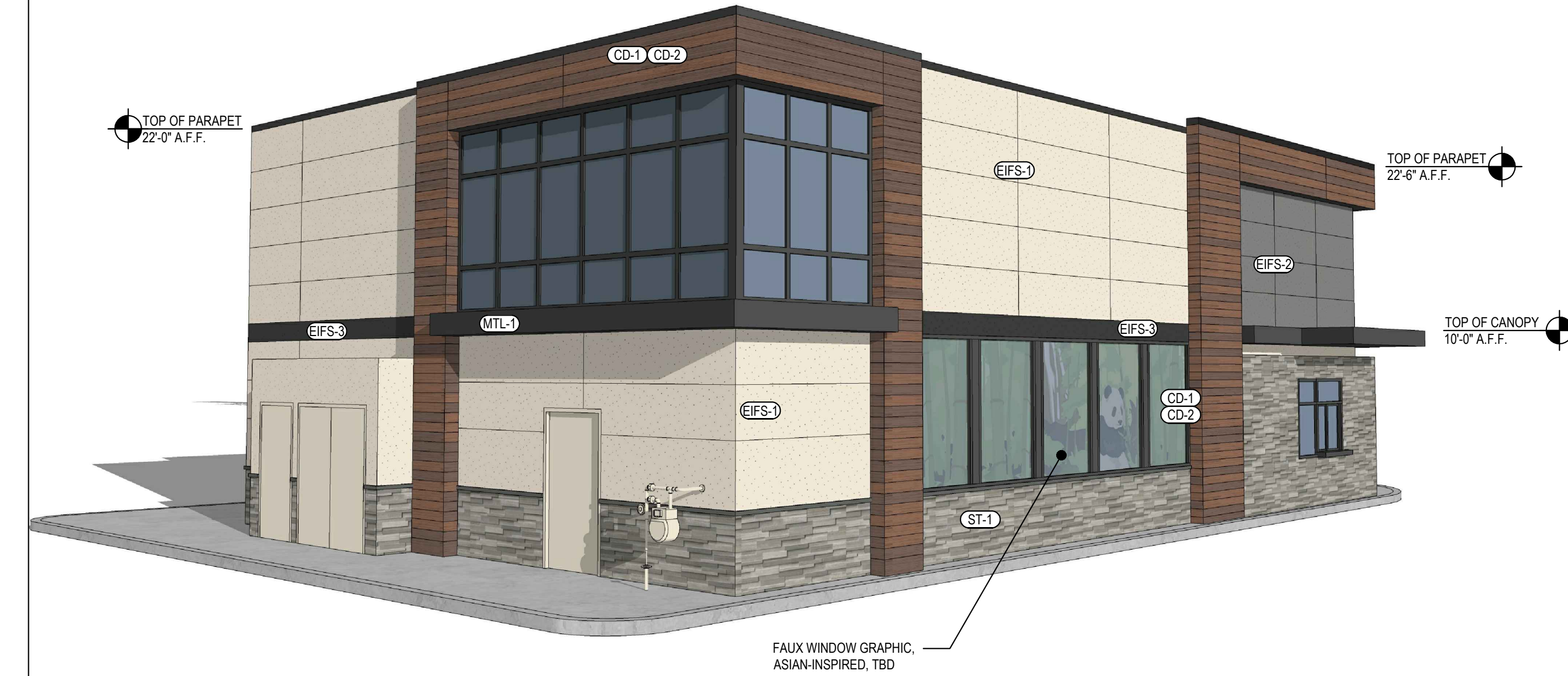
COLOR PERSPECTIVES



**SOUTHWEST CORNER** 2  
Scale= NTS **A-203**



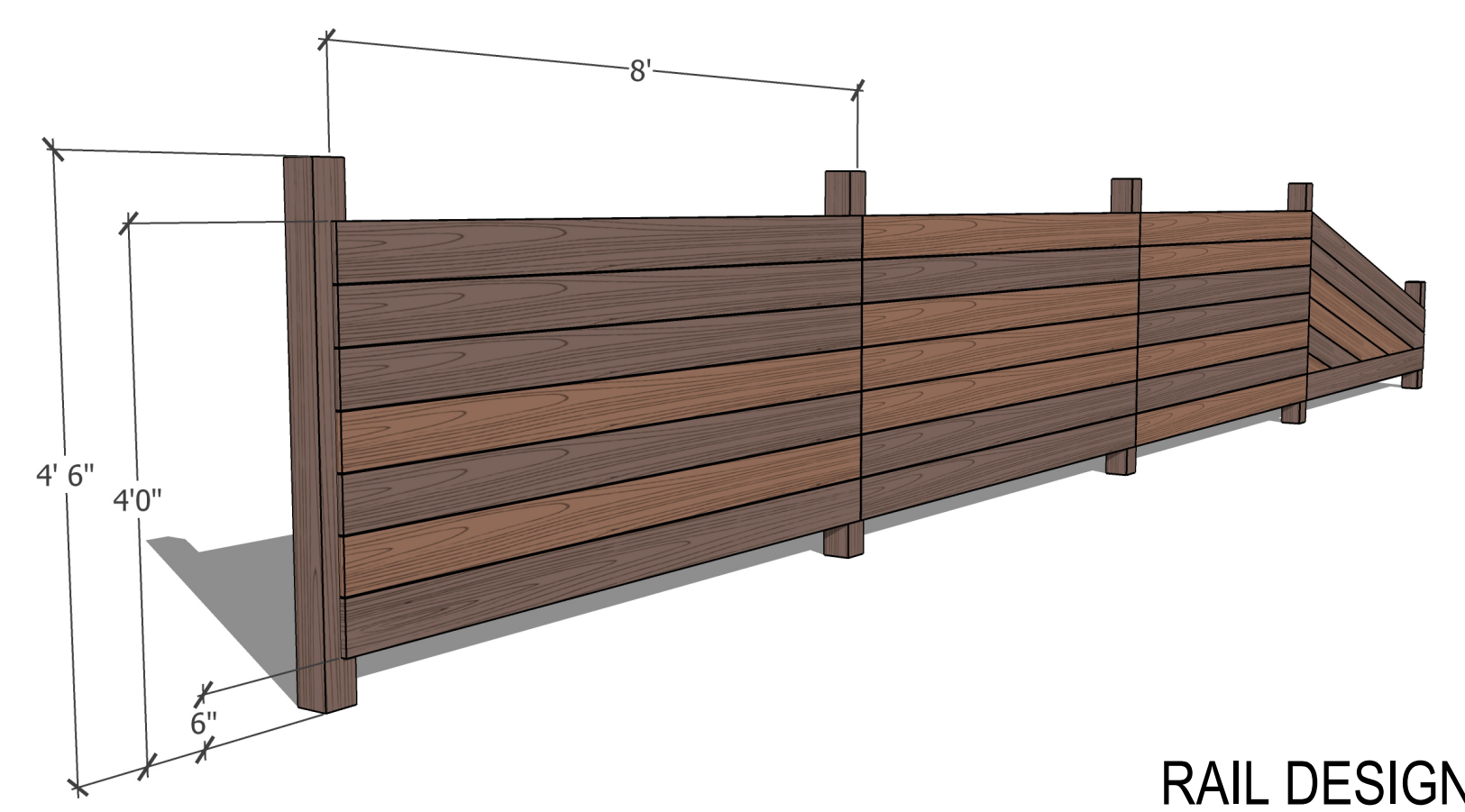
**SOUTHEAST CORNER** 4  
Scale= NTS **A-203**



**NORTHEAST CORNER** 1  
Scale= NTS **A-203**



**NORTHWEST CORNER** 3  
Scale= NTS **A-203**



**RAIL DESIGN**

**EXTERIOR FINISH SCHEDULE** INSTALLED AND FURNISHED BY: G.C. UNLESS NOTED OTHERWISE

NO	MANUFACTURER	MFG#	COLOR	FINISH	NOTES
(EIFS-1)	STO	STOTHERM ESSENCE SYSTEM	SW 6148 WOOL SKEIN	FINE	BUILDING BODY
(EIFS-2)	STO	STOTHERM ESSENCE SYSTEM	SW 7067 CITYSCAPE	FINE	BUILDING BODY
(EIFS-3)	STO	STOTHERM ESSENCE SYSTEM	SW 7069 IRON ORE	FINE	EIFS ACCENT BAND
(ST-1S)	CORONADO STONE PRODUCTS	900 SERIES	#2 GREY	-	STONE CAP (3/8" VERTICAL JOINT - MORTAR TO MATCH STONE CAP)
(ST-1)	CORONADO STONE PRODUCTS	INDUSTRIAL LEDGE	SHALE GREY	-	ENTRY PORTAL & WAINSCOT CONTACT: LISA KILGORE: 864-962-1221 PROVIDE 3/8" MORTAR JOINTS. MFG.: ARGOS, COLOR: PUTTY
(CD-1)	FIBERON	HORIZON	IPE	60% MATERIAL COVERAGE - RANDOM MIX (NON-GROOVE)	COMPOSITE DECKING - CONTACT: BILL ROSS @ 704-756-1980 EMAIL: Billr@fiberondecking.com
(CD-2)	FIBERON	HORIZON	TUDOR BROWN	40% MATERIAL COVERAGE - RANDOM MIX (NON-GROOVE)	COMPOSITE DECKING - CONTACT: BILL ROSS @ 704-756-1980 EMAIL: Billr@fiberondecking.com
(MTL-1)	EXCEPTIONAL METALS	-	"PANDA EXPRESS IRON ORE"	-	CAP FLASHING





## Draft Memorandum

SRF No. 14559

**To:** Jesse Freihammer, PE  
City Engineer/Assistant Public Works Director  
City of Roseville

**From:** Tom Sachi, PE, Associate  
Zach Toberna, EIT, Engineer

**Date:** May 17, 2021

**Subject:** Panda Express Traffic Study

### Introduction

SRF has completed a traffic study for the proposed Panda Express in the southeast quadrant of the Cleveland Avenue and Twin Lake Parkway intersection in the City of Roseville (see Figure 1: Project Location). The main objectives of the study are to review existing operations within the study area, identify any transportation impacts to the adjacent roadway network, and recommend any improvements to address any issues, if necessary. The following information provides the assumptions, analysis, and study findings offered for consideration.

### Existing Conditions

Existing conditions were reviewed to establish a baseline to identify any future impacts associated with the proposed development. Evaluation of the existing conditions includes a review of traffic volumes, roadway characteristics, and an intersection capacity analysis, which are summarized in the following sections.

#### Traffic Volumes

Historical vehicular and pedestrian/bicyclist turning movement counts taken during the midday and p.m. peak periods were utilized for the following intersections:

- Cleveland Avenue and Twin Lakes Parkway/I-35W (July 7, 2016, Collected by MnDOT)
- Twin Lakes Parkway and Mount Ridge Road (July 10, 2014, Collected by SRF)

The intersection turning movement counts were adjusted to account for existing conditions. Observations were completed to identify roadway characteristics (i.e. roadway geometry, posted speed limits, and traffic controls) within the study area. Cleveland Avenue is currently a four-lane roadway within the study area with a speed limit of 40 mph. Twin Lakes Parkway is a four-lane roadway between Cleveland Avenue and Mount Ridge Road, but changes a two-lane roadway east of Mount Ridge Road, and it has a speed limit of 30 mph within the study area.



H:\Projects\1400014559\TrafficStudy\Figures\Fig01\_Project Location.cdr



**Project Location**  
 Panda Express Traffic Study  
 City of Roseville

02114559  
 April 2021

Figure 1

The Cleveland Avenue and Twin Lakes Parkway/I-35W intersection is signalized, while the Twin Lakes Parkway and Mount Ridge Road intersection is a roundabout. Cleveland Avenue is classified as a minor arterial and Twin Lakes Parkway is classified as a local roadway. Existing geometrics, traffic controls, and traffic volumes in the study are shown in Figure 2.

### Intersection Operations Analysis

An intersection capacity analysis was completed for the weekday midday and p.m. peak hours to identify any areas of queuing or congestion concern under existing conditions. Note, the proposed development is not expected to be open during the a.m. peak hour. The study intersections were analyzed using Synchro/SimTraffic (Version 11). Capacity analysis results identify a Level of Service (LOS), which indicates the quality of traffic flow through an intersection. Intersections are given a ranking from LOS A through LOS F. The LOS results are based on average delay per vehicle, which correspond to the delay threshold values shown in Table 1. LOS A indicates the best traffic operation, with vehicles experiencing minimal delays. LOS F indicates an intersection where demand exceeds capacity, or a breakdown of traffic flow. Overall intersection LOS A through LOS D is generally considered acceptable in the Twin Cities.

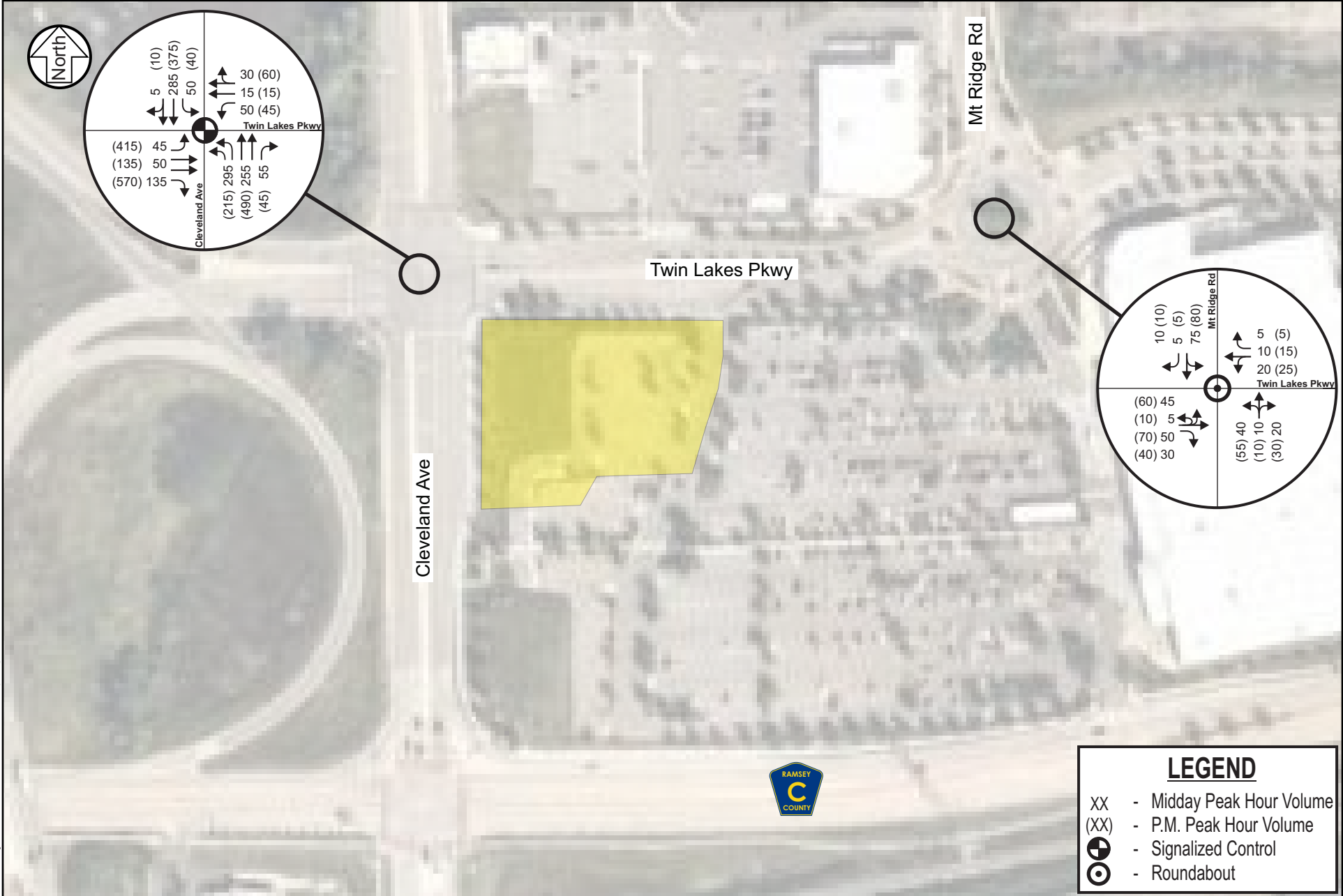
**Table 1. Level of Service Criteria for Signalized and Unsignalized Intersections**

LOS Designation	Signalized Intersection Average Delay/Vehicle (seconds)	Unsignalized Intersection Average Delay/Vehicle (seconds)
A	≤ 10	≤ 10
B	> 10 - 20	> 10 - 15
C	> 20 - 35	> 15 - 25
D	> 35 - 55	> 25 - 35
E	> 55 - 80	> 35 - 50
F	> 80	> 50

For side-street stop-controlled intersections, special emphasis is given to providing an estimate for the level of service of the side-street approach. Traffic operations at an unsignalized intersection with side-street stop control can be described in two ways. First, consideration is given to the overall intersection level of service. This takes into account the total number of vehicles entering the intersection and the capability of the intersection to support these volumes. Second, it is important to consider the delay on the minor approach. Since the mainline does not have to stop, the majority of delay is attributed to the side-street approaches. It is typical of intersections with higher mainline traffic volumes to experience high levels of delay (i.e. poor levels of service) on the side-street approaches, but an acceptable overall intersection level of service during peak hour conditions.

Results of the existing operations analysis shown in Table 2 indicate that all study intersections operate at an acceptable LOS C or better during the midday and p.m. peak hours with the existing traffic control and geometric layout. Note that average eastbound left-turn queues along the I-35W off ramps are approximately 400 feet and extend beyond the turn-lane storage and spill into the eastbound through lane during the p.m. peak hour. No other significant side-street delays or queuing issues were observed at the study intersections.

H:\Projects\1400014559\TrafficStudy\Figures\Fig02\_Existing Conditions.cdr



**Existing Conditions**  
 Panda Express Traffic Study  
 City of Roseville

02114559  
 April 2021

Figure 2



**Table 2. Existing Peak Hour Capacity Analysis**

Intersection	Weekday Midday Peak Hour		Weekday P.M. Peak Hour	
	LOS	Delay	LOS	Delay
	Cleveland Avenue and Twin Lakes Parkway/I-35W <sup>(1)</sup>	B	13 sec.	C
Twin Lakes Parkway and Mount Ridge Road <sup>(2)</sup>	A	4 sec.	A	4 sec.

(1) Indicates a signalized intersection, where the overall LOS is shown.

(2) Indicates a roundabout intersection with all-way yield control, where the overall LOS is shown.

## Proposed Development

The proposed development, shown in Figure 3, is located in the southeast quadrant of the Cleveland Avenue and Twin Lakes Parkway intersection and is anticipated to be fully operational by the year 2022. The proposed development includes a 2,300 SF fast-food restaurant with a drive-through lane. This outlot is expected to be occupied by a Panda Express restaurant. The primary access to the proposed development is located on Twin Lakes Parkway via a right-in/right-out located approximately 250 feet east of Cleveland Avenue. The development also is connected to the Wal-Mart parking lot and can access the roundabout at the Twin Lakes Parkway and Mount Ridge Road intersection.

## Year 2023 Conditions

To identify potential impacts associated with the proposed development, traffic forecasts for year 2023 conditions (i.e. one-year after opening) were developed. The year 2023 conditions take into account general area background growth and traffic generated by the proposed development. The following sections provide details on the background traffic forecasts, estimated trip generation, and the intersection capacity analysis for year 2023 conditions.

### Background Traffic Growth

Based on the *Twin Lakes Redevelopment Area Traffic Study Update* completed in 2015, there is expected to be a growth rate of one-half percent within the study area.

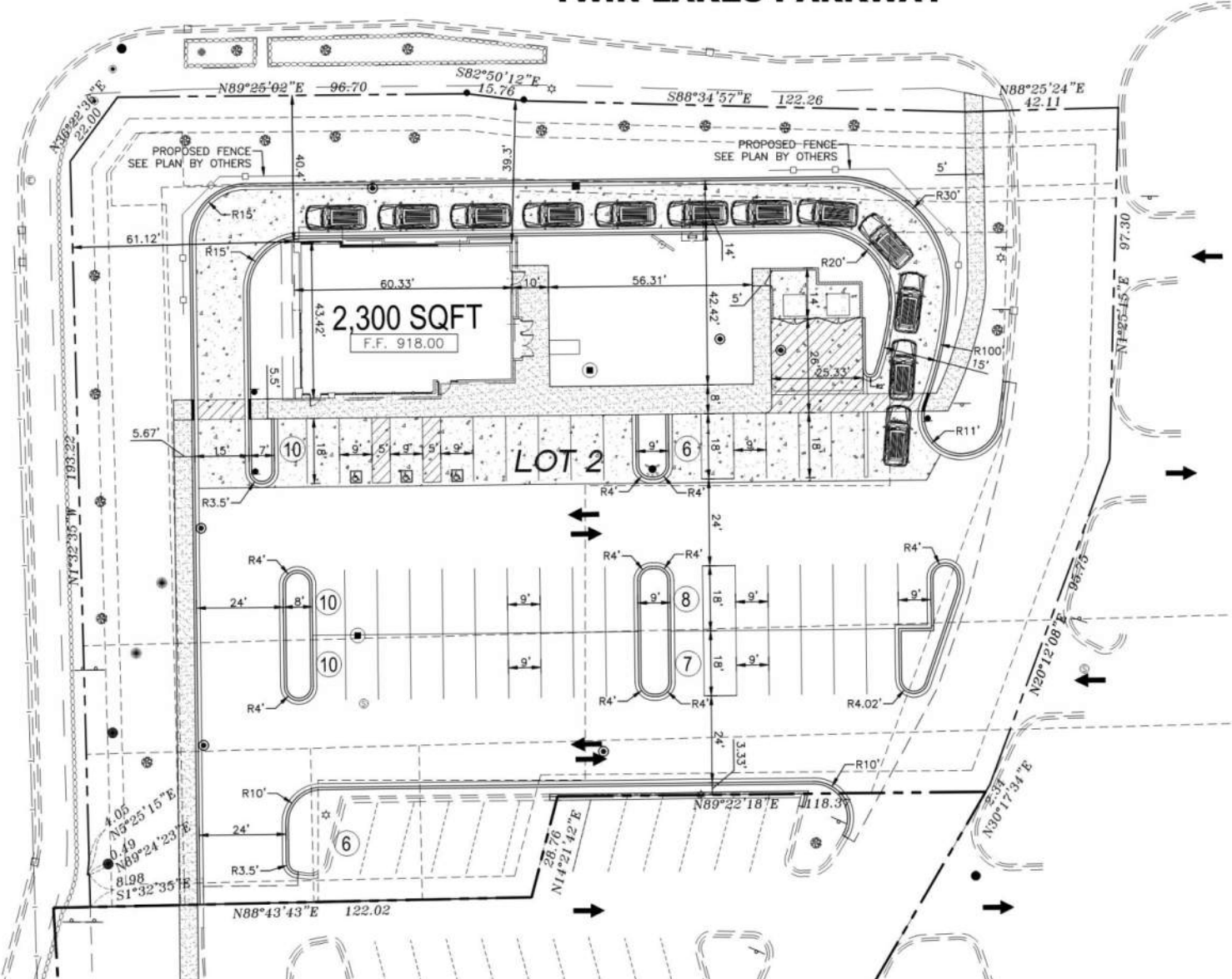
### Trip Generation

To account for traffic impacts associated with the proposed development, trip generation estimates were developed for the weekday midday and p.m. peak hours, as well as on a daily basis. The trip generation estimates were developed using the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 10th Edition*. Results of the trip generation analysis shown in Table 3 indicate that the proposed developments are expected to generate 117 weekday midday peak hour, 75 weekday p.m. peak hour, 1,083 weekday daily trips. Note, no pass-by or multi-use reductions were assumed in order to remain conservative.



### TWIN LAKES PARKWAY

### CLEVELAND AVENUE



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**Site Plan**  
 Panda Express Traffic Study  
 City of Roseville

02114559  
April 2021

Figure 3

**Table 3. Trip Generation Estimate**

Land Use Type (ITE Code)	Size	Weekday				Daily
		Midday Peak Hour		P.M. Peak Hour		
		In	Out	In	Out	
Fast-Food w/ Drive-Thru (934)	2,300 SF	61	56	39	36	1,083

Trips generated were distributed to the study area based on the directional distribution shown in Figure 4, which was developed based on a combination of existing daily traffic volumes/travel patterns, information from the previously completed AUAR, and engineering judgment. The resultant year 2023 peak hour traffic forecasts, which include general background growth and trips generated by the development, are illustrated in Figure 5.

**Year 2023 Build Conditions Intersection Capacity Analysis**

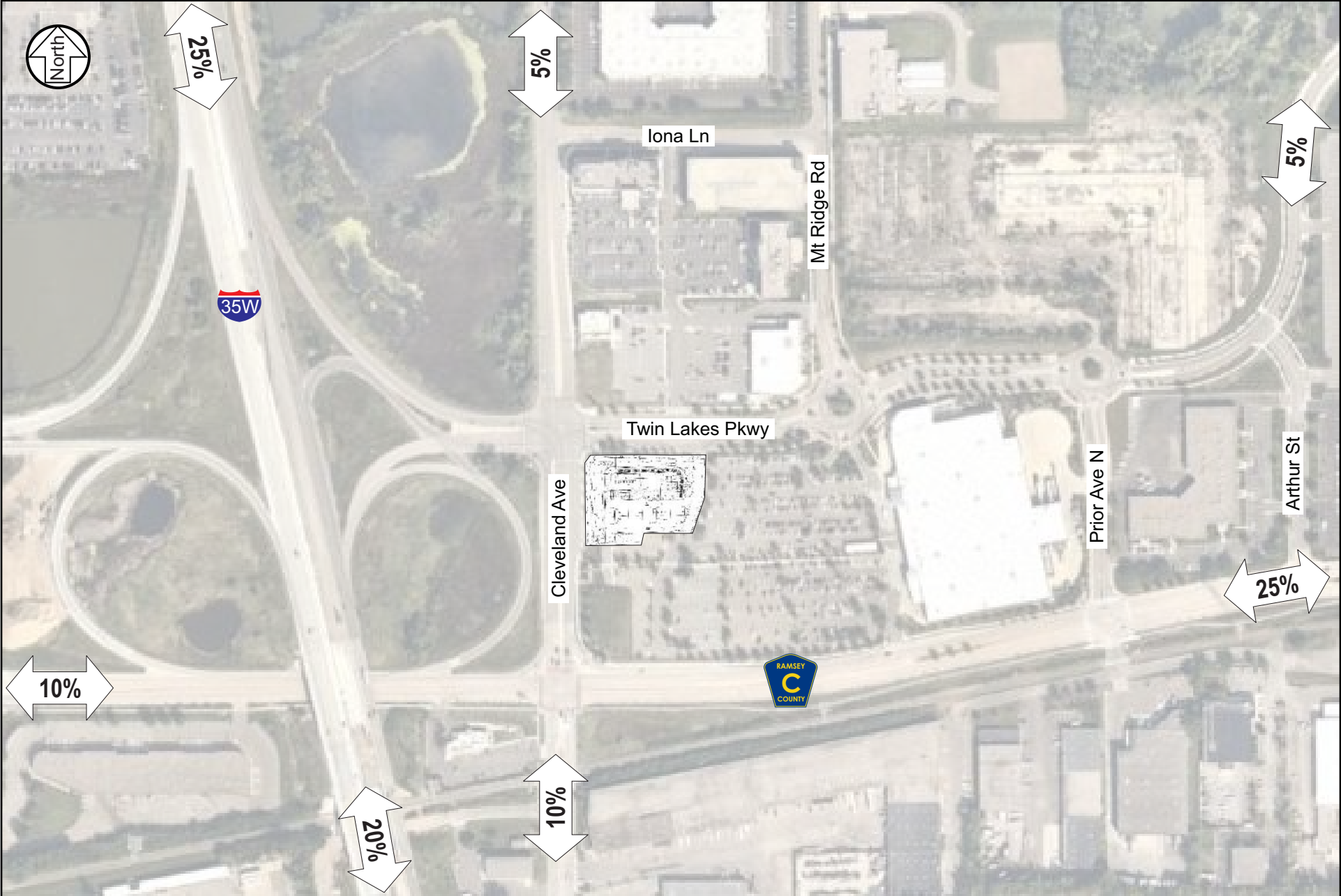
To understand how the existing roadway network is expected to operate under year 2023 build conditions, a detailed intersection capacity analysis was completed using Synchro/SimTraffic (Version 11). Results of the year 2023 build intersection capacity analysis shown in Table 4 indicate that all of the study intersections are expected to operate at an acceptable overall LOS C or better during the midday and p.m. peak hours. Queuing is expected to remain the same along the eastbound approach of the I-35W Off Ramp. There is not expected to be any internal queuing issues entering or exiting the proposed development along Twin Lakes Parkway, with average queues of one (1) vehicle and 95th percentile queues of two (2) vehicles at the right-out access.

**Table 4. Year 2023 Intersection Capacity Analysis**

Intersection	Weekday Midday Peak Hour		Weekday P.M. Peak Hour	
	LOS	Delay	LOS	Delay
	Cleveland Avenue and Twin Lakes Parkway/I-35W <sup>(1)</sup>	B	16 sec.	C
Twin Lakes Parkway and Existing Shopping Mall	A/A	6 sec.	A/A	3 sec.
Twin Lakes Parkway and Mount Ridge Road <sup>(3)</sup>	A	4 sec.	A	4 sec.

- (1) Indicates a signalized intersection, where the overall LOS is shown.
- (2) Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.
- (3) Indicates a roundabout intersection with all-way yield control, where the overall LOS is shown.

Based on the results of the year 2023 build conditions intersection capacity analysis, there is not expected to be any significant delay or queuing issues that require mitigation.



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**Directional Distribution**  
Panda Express Traffic Study  
City of Roseville

02114559  
April 2021

Figure 4

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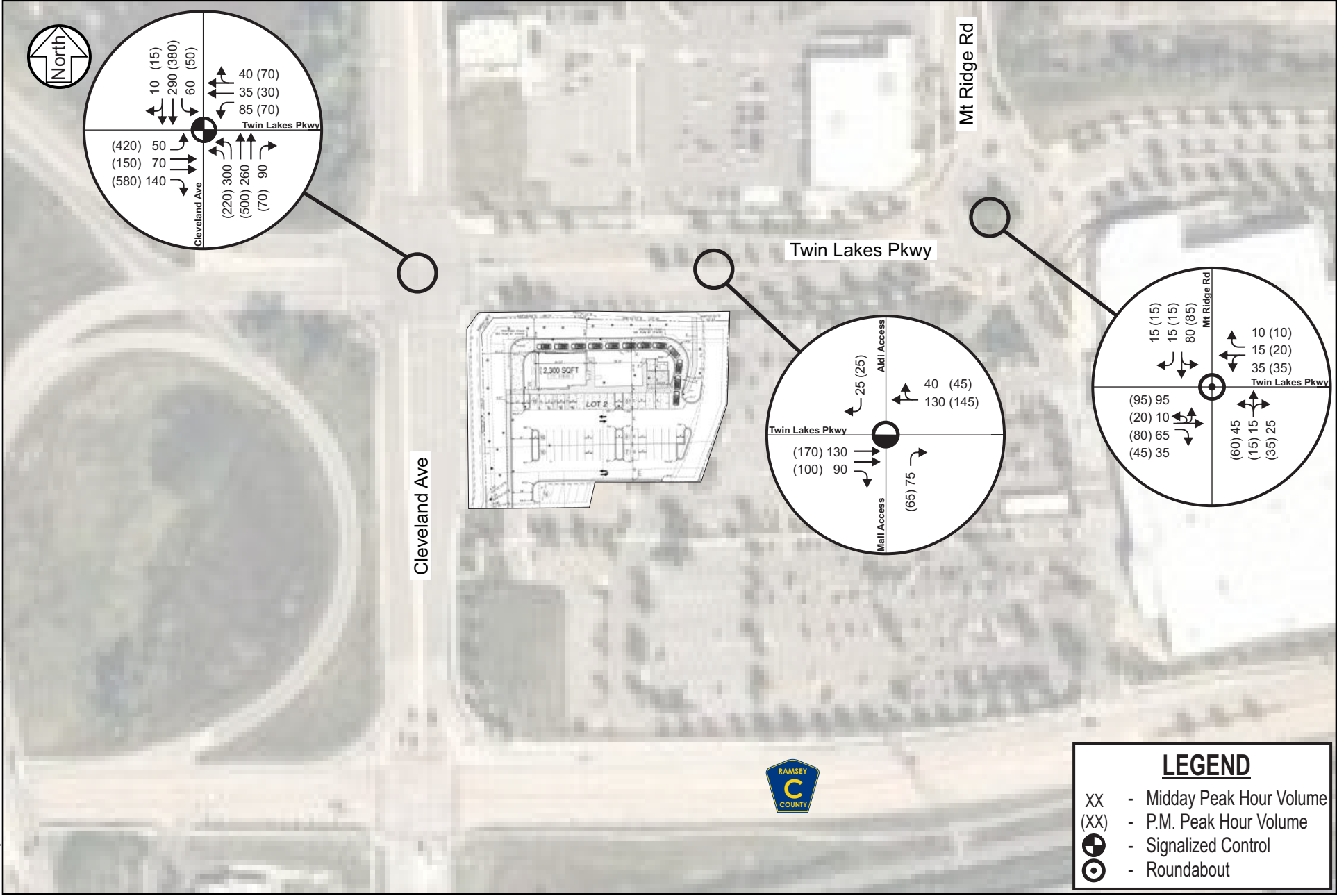


Figure 5

## Site Plan Review

A review of the proposed site plan was completed to identify any issues and recommend potential improvements with regard to drive-through operations, access, and circulation. Based on field observations, there is adequate sight distance at the existing right-in/right-out access location on Twin Lakes Parkway to clearly identify approaching vehicles. Special consideration should be made to limit any sight distance impacts from future landscaping and signing. No other traffic control or circulation issues are expected.

### Drive-Thru Queuing Review

According to the *Drive-Through Queue Generation Study* completed by Counting Cars in 2012, maximum queues at a fast food restaurant range between five (5) and 13 vehicles, with an 85th percentile queue of 12 vehicles. Based on the site plan provided, there is room for 12 vehicles to queue within the drive-thru lane before impacting the internal parking lot circulation. Therefore, it is expected that the proposed drive-thru will accommodate the majority of the expected drive-thru queues at the site. The site should be monitored to determine if the maximum drive-thru queues ever extend beyond the available storage and spill into the parking lot. If issues do occur, the first entrance driveway to the site may need to be restricted to exit only and additional entering vehicles may need to proceed south to the southern drive aisle and circulate around the lot to enter the drive-thru queue.

### AUAR Review.

A trip generation comparison to the Twin Lakes AUAR for the proposed development area was completed to determine if the proposed development trip generation is consistent with previous assumptions. The proposed land use change for site was compared to the allocation based on the current Twin Lakes AUAR (Subarea 1, Block 4). Block 4 includes 160,000 SF of free-standing discount store and 13,200 sf of high-turnover sit-down restaurant broken into two (2) 6,600 sf parcels. A comparison was completed in Table 5 for the proposed land use to one (1) of the 6,600 sf restaurant parcels. The parcel's current trip allocation is 65 weekday p.m. peak hour trips. The proposed development is expected to generate 75 p.m. peak hour trips, indicating that the density of the proposed land use is higher than the previously assumed traffic generation for this site. However, Block 4 still has one (1) remaining empty parcel and there is potential that Block 4 as a whole will generate trips lower or consistent with the Twin Lakes AUAR. Trip generation for the remaining parcel should be reviewed if/when development occurs.

**Table 5. AUAR Trip Generation Estimate**

Land Use (ITE Code)	Size	P.M. Peak Hour		Daily
		In	Out	
Proposed Land Use				
Fast-Food w/ Drive-Thru (934)	2,300 SF	39	36	1,083
Assumed AUAR Land Use				
High Turnover (Sit Down) Restaurant	6,600 SF	39	26	839
	Difference	0	+10	+244

### Conclusions and Recommendations

The following study conclusions and recommendations are offered for consideration:

- 1) Results of the existing operations analysis indicate that all study intersections currently operate at an acceptable overall LOS C or better during the midday and p.m. peak hours.
  - a. Average eastbound left-turn queues along the I-35W off ramps are approximately 400 feet during the p.m. peak hour
- 2) The proposed development includes a 2,300 SF fast-food restaurant with a drive-through. The development is planning direct right-in right-out access to Twin Lakes Parkway with the existing Shopping Mall Access, located approximately 250 feet east of Cleveland Avenue.
- 3) Results of the trip generation estimates indicate the proposed development site is expected to generate a total of 117 midday peak hour, 75 p.m. peak hour, and 1,083 daily trips.
- 4) Results of the year 2023 build intersection capacity analysis indicate that all of the study intersections and proposed access locations are expected to operate at an acceptable overall LOS C or better during the midday and p.m. peak hours.
  - a. Queuing is expected to remain the same along the eastbound approach of the I-35W Off Ramp. There is not expected to be any internal queuing issues entering or exiting the proposed development along Twin Lakes Parkway, with average queues of one (1) vehicle and 95th percentile queues of two (2) vehicles at the right-out access.
- 5) The proposed fast-food restaurant drive-through is expected to be able to accommodate expected 85th percentile queues for the site.
- 6) The parcel’s current trip allocation is 65 weekday p.m. peak hour trips. The proposed development is expected to generate 75 p.m. peak hour trips, indicating that the density of the proposed land use is higher than the previously assumed traffic generation for this site. However, Block 4 still has one (1) remaining empty parcel and there is potential that Block 4 as a whole will generate trips lower or consistent with the Twin Lakes AUAR.

**INTEROFFICE MEMORANDUM**

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**Date:** May 24, 2021

**To:** Thomas Paschke, City Planner

**From:** **Stephanie Smith, Asst. City Engineer**

**RE:** Panda Express – Conditional Use Permit

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The Public Works Department reviewed the proposed plans for the project noted above and offers the following comments with regard to the project's impact on City services and/or infrastructure:

1. A traffic study was performed on the Cleveland Ave. & Twin Lakes Pkwy and Twin Lakes Pkwy. And Mount Ridge Rd. intersections. The report is attached and summarized below:
  - The study intersections currently operate at an acceptable overall LOS C or better during the midday and p.m. peak hours; queuing associated with the I-35W off ramps does extend into the eastbound through lane during the p.m. peak hour.
  - The proposed development would include a 2,300sf fast-food restaurant with drive-thru. The site is expected to generate 117 midday peak hour, 75 p.m. peak hour, and 1,083 weekday daily trips.
  - The access is the right-in/right-out 250 ft east of Cleveland Avenue. Additionally, the site can be accessed from the Wal-Mart parking lot.
  - Once the development is completed in 2023, the study intersections are expected to operate at an acceptable overall LOS C or better during the midday and p.m. peak hours; queuing associated with the I-35W off ramps would be similar to the existing condition.
  - There is not expected to be any queueing issues entering/exiting the public streets as a result of the additional trips to/from the proposed development.
  - Roadway network improvements are not anticipated to be needed from an intersection capacity perspective as a result of the proposed development.
  - A review of the proposed drive-thru operations and queuing indicates that it would accommodate anticipated 85<sup>th</sup> percentile queues for the site. If needed based on operations, the first entrance driveway to the site may need to be restricted to exit only to allow additional queuing space.
  - When compared to the AUAR traffic impact assumptions, this parcel is expected to generate more trips than the AUAR's assumptions. Block 4 still has an empty parcel that, when developed, has potential to result in a trip generation consistent with the AUAR for Block 4 as a whole. The remaining parcel should be evaluated when it develops.



2. The circulation plan submitted separates vehicle operations from the pedestrian circulation route. Pavement markings are proposed to delineate the pedestrian route across the drive-thru queue to promote driver awareness.
3. Sidewalk improvements off-site shall be performed to connect to the pedestrian routes to the southwest of the parcel. Plans shall be revised to include this work.
4. Storm sewer improvements are required to meet both the City of Roseville and Rice Creek Watershed Requirements
5. Sanitary sewer is available at the site. A permit is required for the connection.
6. Private watermain is located on the Wal-Mart site. The applicant proposes constructing a greater than 400-foot, 6-inch private service from the Wal-Mart line to their site.
7. Staff recommends upsizing the watermain to 8-inch and providing a watermain stub to the vacant parcel south of the site, so as to reduce future impacts when the vacant site is developed and provide a looped connection.
8. Water valves and bends shall be shown on the final utility plan. A permit is required for the water service connection.
9. Staff recommends stubbing water service to the vacant parcel south of the proposed site.
10. Agreements for installation and maintenance are required for infrastructure that crosses the parcels boundaries within this commercial area, including the water service, parking aisles and sidewalks.
11. An encroachment agreement is required for improvements made over City easements.
12. Drainage and utility easements are required over the private utilities, including the proposed hydrant.

Thank you for the opportunity to provide feedback and on this project at this time. As the project advances, Public Works Department staff will continue to review any forthcoming plans and provide additional reviews and feedback as necessary. Please contact me should there be questions or concerns regarding any of the information contained herein.

**ROSEVILLE**  
**REQUEST FOR PLANNING COMMISSION ACTION**

Date: June 2, 2021  
Item No. 6c

Department Approval

*Janice Gundlach*

Agenda Section  
**Public Hearings**

Item Description: Request for preliminary approval of a Major Plat to subdivide the development site into 11 lots for single-family, detached homes (PF21-003)

**Application Information**

Applicant: Integrity Land Development, Inc.  
Location: 2395 County Road B; 2224 Eustis Street  
Property Owner: Jane Shannon; Emma Kydd

Community Engagement: Virtual and in-person open house meetings on 3/10/2021  
Application Submittal: Received 3/7/2021; Considered complete 5/19/2021  
City Action Deadline: 9/16/2021, per Minn. Stat. 462.358 subd. 3b

**General Site Information**

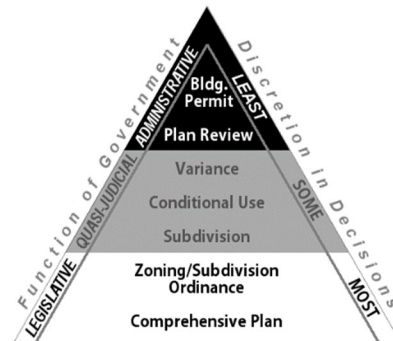
Land Use Context

	Existing Land Use	Guiding	Zoning
<b>Site</b>	One-family residential, detached	MR	MDR
<b>North</b>	One-family residential, detached	LR	LDR-1
<b>West</b>	One-family residential, detached	LR	LDR-1 LDR-2
<b>East</b>	One-family residential, detached	LR	LDR-1
<b>South</b>	Midland Hills golf course	GC	PR

Notable Natural Features: the site contains steep grades, many mature trees, and a substantial wetland

Land Use History: none

Level of City Discretion in Decision-Making: quasi-judicial.



1 **Proposal Summary**

2 The applicant proposes to subdivide two residential parcels generally located in the northeast corner of  
 3 County Road B and Eustis Street resulting in the *Midland Legacy Estate 3<sup>rd</sup> Addition* plat for  
 4 development of 11 single-family, detached homes with an outlot for future platting into three additional  
 5 lots for single-family detached homes and park dedication of approximately 0.6 acres. The developer  
 6 proposes to realign the intersection of County Road B and Eustis Street into a conventional “T” shaped  
 7 intersection and build a private cul-de-sac street serving Lots 1, 2 and 11, the future lots platted in the  
 8 outlot, and providing access to a shared driveway departing from the eastern end of the cul-de-sac  
 9 serving another four homes. Illustrations and other information about the proposed development are  
 10 included with this RPCA in Attachment C.

11 When exercising the “quasi-judicial” authority on subdivision requests the role of the City is to  
 12 determine the facts associated with a particular proposal and apply those facts to the legal standards  
 13 contained in the ordinance and relevant state law. In general, if the facts indicate the application meets  
 14 the relevant legal standards and will not compromise the public health, safety, and general welfare, then  
 15 the applicant is likely entitled to the approval. The City is, however, able to add conditions to a  
 16 subdivision approval to promote the public health, safety, and general welfare, and to provide for the  
 17 orderly, economic, and safe development of land, and to promote housing affordability for all levels.

18 **Preliminary Plat**

19 Roseville’s Development Review Committee (DRC) met on several occasions in late 2020 and early  
 20 2021 to review the proposed subdivision plans. Some of the comments and feedback based on the  
 21 DRC’s review of the application are included in the analysis below, and the full comments offered in  
 22 memos prepared by DRC members are included with this RPCA in Attachment E.

23 Proposed Lots

24 Lots zoned LDR-1 have two different size requirements, depending on whether they are corner or  
 25 interior lots. The table below shows how the proposed lots compare to the relevant requirements in City  
 26 Code §1004.08.

Interior Lots				
	Front Width (ft)	Rear Width (ft)	Depth (ft)	Area (sq ft)
<b>Minimum</b>	<b>85</b>	<b>45</b>	<b>110</b>	<b>11,000</b>
Lot 1	≥85	>45	110	14,664
Lot 2	65	39	>260	14,052
Lot 3	>100	>50	>300	20,770
Lot 4	85	>45	>290	18,296
Lot 5	85	45	>250	16,821
Lot 6	85	>45	>180	14,096
Lot 7t	85	85	130	11,803
Lot 9	85	>50	>180	12,938
Lot 10	>150	>160	>120	19,645
Lot 11	>120	>240	>220	41,639

Corner Lot				
	Front Width (ft)	Rear Width (ft)	Depth (ft)	Area (sq ft)
<b>Minimum</b>	<b>100</b>	<b>45</b>	<b>100</b>	<b>12,500</b>
Lot 8	100	>100	>110	12,526

\*per City Code, front lot width is measured at the required front yard setback (30 feet from the front lot line) and rear lot width is measured at the required rear yard setback (30 feet from the rear lot line).

27 All but one of the proposed lots meet or exceed the pertinent measurable standards of the zoning code,  
 28 but none of the proposed lots have the preferred “simple, regular shapes” described in §1103.05 (Lot

Standards). This section of the subdivision code provides exceptions to the preference for simple, regular lot shapes, however, in an acknowledgment that such regular shapes may be impractical to create within a plat when the exterior boundaries of a plat are irregular to begin with. And while this acknowledgement does not specifically say so, it may be reasonable to recognize that the organic shapes and arbitrary locations of wetlands or lakeshores within a parcel can also justify some flexibility in a subdivision proposal's lot shapes. Even in this context, Planning Division staff finds the shapes of Lots 8 and 9, in particular, difficult to accept. There may nominally be enough land area and street frontage for three parcels in the southwestern corner of the plat by a strict application of the minimum dimensional standards, but in practice dividing this land area into three lots necessitates significant manipulations of the lot boundaries in order to secure enough area for Lot 8 and enough width for Lot 9, resulting in Lot 8 having an irregular shape and Lot 9 having a boundary closely wrapping around the back side of the home. Additionally, the deflection in the middle of the shared boundary between Lots 10 and 11 seems unnecessary and should perhaps be avoided. Consequently, while the proposal does not strictly violate measurable dimensional standards staff cannot recommend approval of these lots as being consistent with purpose and intent of §1103.05.

#### Setbacks and Impervious Coverage

Although building setbacks are not specifically reviewed and approved as part of a plat application, the building footprints represented in the preliminary development plans generally appear to conform to the minimum property line setbacks of the LDR-1 district as well as the 50-foot wetland setback specified in Chapter 1017 (Shoreland, Wetland, and Storm Water Management) of the City Code. One exception is Lot 1; the house as shown may be 30 feet from the apex of the northeastern corner of the lot, but it is within 30 feet of both the northern and eastern lot boundaries, one of which must be designated as a rear lot line—staff would suggest the eastern boundary be considered the rear, being most opposite to the cul-de-sac frontage. The other exception is the building pad in Lot 10, which is within 50 feet of the existing wetland boundary; in this case, though, the developer proposes to correct this by legally relocating that portion of wetland boundary.

Likewise, the impervious coverage limits established in the zoning code are not strictly regulated in the plat review process. The impervious surfaces represented in plat application materials are intended to show a maximum development condition for the purpose of being able to design a storm water management plan that meets the applicable requirements. Lots 3 – 7 appear to have impervious coverage beyond the 25% limit established in §1017.26.B.1. For these lots, the developer may utilize an exception in this same section of code to treat the storm water on site such that this reduced limit no longer applies, and this is worth noting as part of the plat review process.

#### Shared Driveway, Private Street, and Public Pathways

Consistent with Ordinance 1591 passed by the City Council in October 2020, the proposed private street is at least 110 feet from any existing residential parcels adjacent to the northern and eastern sides of the plat. The proposed private street would directly serve Lots 1, 2, and 11 as well as the lots that may be platted in the future within the outlot to the north, and it serves as the connection to a proposed shared driveway providing access to Lots 3 – 6. Although the ordinance prohibits building a new street within 110 feet of the side or rear yards of existing residential parcels, neither this ordinance nor any other part of the zoning or subdivision codes provides guidance for differentiating between a street and a shared driveway. After the passage of Ordinance 1591, therefore, City Public Works staff drafted a policy to identify when a proposed vehicular access becomes too large to be called a “shared driveway” and should be considered a street. Importantly, this policy established 150 feet as an acceptable maximum length and a limit of serving no more than three lots for single-family, detached homes. The proposed shared driveway is approximately 450 feet in length and serves four lots for single-family, detached homes. Because the proposed shared driveway exceeds the length and service parameters of staff's

76 policy, staff cannot recommend approval of this aspect of the proposed preliminary plat. Consequently,  
77 staff would recommend the preliminary plat be reconfigured in this area to conform to the policy on  
78 shared driveways.

79 The City Engineer's memo indicates the following:

- 80 • Pathway improvements, as shown on the proposed plans, on the west and south side of the  
81 development should be required to make connections to the existing pathway on County Road B.
- 82 • A future pathway connection from the end of Eustis Street to St. Croix Street just south of TH  
83 36/I35W is in the pathway master plan. This will be required to be constructed as part of the  
84 development.
- 85 • The proposed private road, Legacy Path, is 26' curb to curb. Parking would be allowed on one  
86 side of the roadway.

87 Anticipating that the existing conditions of County Road B and the additional traffic associated with the  
88 proposed development may be a concern of nearby homeowners, the City Engineer has also provided an  
89 analysis of the street and anticipated traffic, which is included as part of Attachment E.

90 And although the Fire Department was unable to provide a separate memo, the Battalion Chief reviewed  
91 the proposed plans and identified the following concerns.

- 92 • The length of the driveway far exceeds an acceptable distance (150' max)
  - 93 ○ The current proposed distance also has significant issues with water supply availability at the  
94 end of the driveway.
- 95 • The number of homes served by the driveway/road (4 vs. 3)
- 96 • Lack of any capacity for vehicle turnaround
- 97 • Width of the driveway would be something we would need additional information on.

#### 98 Storm Water Management

99 The City Engineer's memo indicates the following:

- 100 • Storm sewer within the site would be private.
- 101 • Rice Creek Watershed District and NPDES permits will be required.
- 102 • The draft plans submitted to the city do not take into account our Stormwater Standards.  
103 Engineering staff consider the submittal incomplete given the amount of missing information,  
104 and cannot provide a thorough review to ensure rate and water quality standards are met. The  
105 developer will need to submit additional information prior to staff review.

#### 106 Tree Preservation

107 The tree preservation and replacement requirements in §1011.04 of the City Code provide a way to  
108 quantify the amount of tree material being removed for a given project and to calculate the resulting tree  
109 replacement obligation. The applicant has provided these calculations, and they are included in  
110 Attachment C. At the time this RPCA was prepared, Roseville's consulting forester continues to review  
111 the tree preservation plan but the applicant's preliminary calculation based on the proposed development  
112 would elicit the obligation to plant 645 replacement trees or pay a fee in lieu of replacement plantings of  
113 up to \$61,900 (i.e., 10% of Ramsey County's estimated market value of the land of 2395 County Road  
114 B), although Planning Division staff and the consulting forester are continuing to validate the data. The  
115 proposal could entail a combination of replacement trees and "cash-in-lieu" payments of \$500 per tree  
116 should the developer be unable to replace all 645 trees.

117 Park Dedication

118 This subdivision proposal elicits the park dedication requirement because the subject property is greater  
119 than one acre in size and the proposal results in a net increase of development lots. Since the subject  
120 property includes three existing residential parcels, City staff has determined that the proposed 11-lot  
121 plat represents a net increase of nine developable lots. As such, the City could accept a dedication of up  
122 to approximately six-tenths of an acre of park land (based on the requirement to dedicate up to 10% of  
123 the land of the 6.35-acre development site) or a dedication of cash in lieu of land, or an equivalent  
124 combination of land and cash. The Parks and Recreation Commission (PRC) reviewed the proposal at its  
125 meeting of May 4, 2021, and recommended a dedication of park land to satisfy the park dedication  
126 requirement. An excerpt of the draft May 4 PRC minutes is included with this RPCA as part of  
127 Attachment E, and staff wishes to highlight the following comments:

- 128 • The developer submitted a letter regarding the condition in which the park parcel will be turned  
129 over to the City, and it is extremely important that the developer follow through with each of the  
130 terms that he listed in this document.
- 131 • The developer stated that “We are saving all the beautiful large Oak trees on the east side of the  
132 existing driveway to keep the park beautiful and relaxing with shade.” It does not appear that the  
133 tree preservation plan accounts for preservation of these trees. These trees were emphasized by  
134 the developer in his meeting with the PRC and important to the possible future park, and they  
135 must be vigorously protected, particularly while construction is in progress. We would want to  
136 see these trees identified for protection on the tree preservation plan (as others are) and a full  
137 protection plan for these trees, which our staff could review and provide input on.

138 **PUBLIC COMMENT**

139 As required for plats creating more than three lots, the applicant conducted a pre-application community  
140 engagement effort with a series of virtual and in-person open house meetings on March 10, 2021. The  
141 applicant has submitted a summary of the discussions that occurred during the meetings, which is  
142 included with this RPCA as part of Attachment D. Staff has also received one email that is included in  
143 Attachment D.

144 **RECOMMENDED ACTIONS**

145 While the applicant has provided information to suggest the property can support a subdivision of  
146 multiple single-family lots, based upon the analysis provided in this RPCA, Planning Division staff finds  
147 revisions to the plat are necessary in order to demonstrate substantial conformance to the City’s  
148 requirements and standards, especially as it pertains to the shapes of lots, length of a private driveway,  
149 and number of single-family detached residential lots served by a private driveway. Therefore, staff is  
150 recommending the Planning Commission table action to allow the applicant to submit a revised plat for  
151 consideration by the Planning Commission.

152 **A. By motion, table the proposed *Midland Legacy Estate 3<sup>rd</sup> Addition Preliminary Plat*, based**  
153 **on the content of this RPCA, public input, and Planning Commission deliberation, with the**  
154 **following comments:**

- 155 1. Pursuant to the memo from Public Works staff in Attachment E of this RPCA, the applicant  
156 shall:
  - 157 a. Install pathway improvements as shown on the proposed plans, on the west and south  
158 side of the development, to make connections to the existing pathway on County Road B.
  - 159 b. Install a pathway connection from the end of Eustis Street to St. Croix Street just south of  
160 TH 36/I35W.

- 161 c. Submit additional storm water management information to meet pertinent requirements  
162 for staff review.
- 163 d. Enter into a Public Improvement Contract regarding the construction of public  
164 infrastructure.
- 165 2. The applicant shall establish a homeowner's association for the maintenance of the private  
166 street, shared driveway, and storm water management BMPs.
- 167 3. In accordance with the recommendation of the Parks and Recreation Commission, the  
168 applicant shall dedicate the 0.616 acre parcel of park land, which dedication will obviate  
169 further park dedication at such time as the outlot may be further platted into additional  
170 residential lots.
- 171 4. The applicant shall revise the tree preservation plan to indicate adequate protections for the  
172 specified trees in the land identified for park dedication.
- 173 5. The applicant shall revise the plat to ensure that Lot 2 meets the minimum width  
174 requirements at the front and rear building lines.
- 175 6. The applicant shall revise the plat such that the centerline measurement of the length of the  
176 shared driveway does not exceed 150 feet and the shared driveway does not serve more than  
177 three lots for single-family, detached homes.
- 178 7. The applicant shall continue to address with Fire Department staff any issues regarding the  
179 driveway width, turn-around facility, and water supply at the end of the shared driveway.
- 180 8. The applicant shall revise the plat to achieve greater compliance with the lot shape provisions  
181 of City Code §1103.05, especially as it pertains to Lots 7 – 9 pursuant to the preceding  
182 comments in this RPCA.

183 The City has until September 16, 2021, under Minn. Stat. 462.358 subd. 3b to take action on the  
184 request, so there is plenty of time for the applicant to make any necessary changes to the  
185 proposal without risk of statutory approval. Moreover, by simply notifying the applicant the  
186 City can extend the action timeline by an additional 120 days to January 14, 2022, if necessary.

187 **ALTERNATIVE ACTIONS**

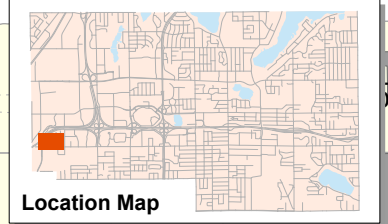
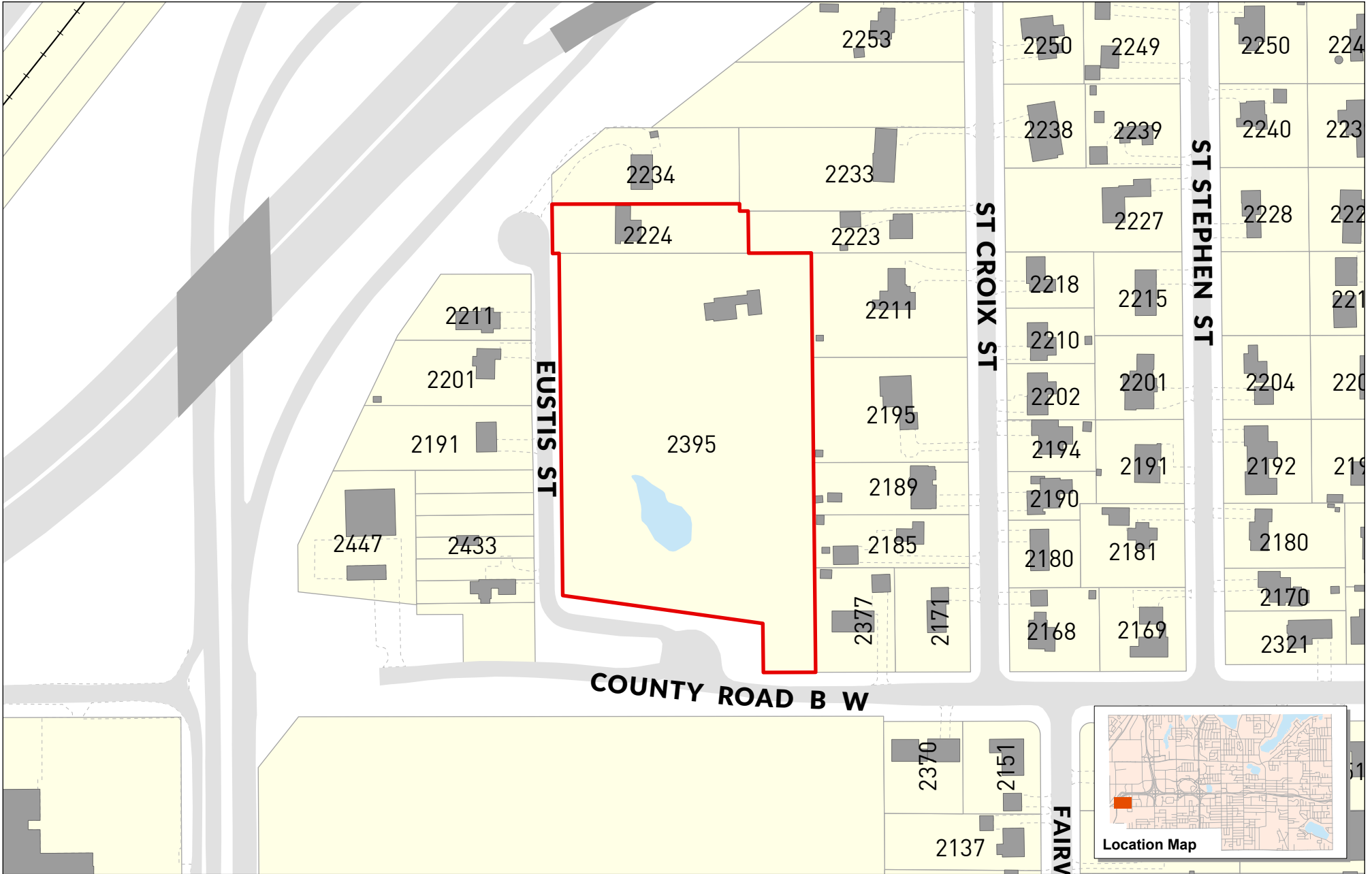
- 188 **A. Pass a motion to recommend denial of the proposed Preliminary Plat.** Recommendations of  
189 denial should be supported by specific findings of fact based on the Planning Commission's  
190 review of the application, applicable zoning or subdivision regulations, and the public record.

Attachments: A: Area map C: Proposed plans  
B: Aerial photo D: Open house feedback and public comment  
E: Comments from DRC

Prepared by: Senior Planner Bryan Lloyd  
651-792-7073  
[bryan.lloyd@cityofroseville.com](mailto:bryan.lloyd@cityofroseville.com)

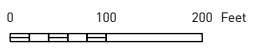


# Attachment A: Planning File 21-003



**Data Sources**  
 \* Ramsey County GIS Base Map [5/4/2021]  
 For further information regarding the contents of this map contact:  
 City of Roseville, Community Development Department,  
 2660 Civic Center Drive, Roseville MN

**Disclaimer**  
 This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only. The City does not warrant that the Geographic Information System (GIS) Data used to prepare this map are error free, and the City does not represent that the GIS Data can be used for navigational, tracking or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. If errors or discrepancies are found please contact 651-792-7085. The preceding disclaimer is provided pursuant to Minnesota Statutes §466.03, Subd. 21 (2000), and the user of this map acknowledges that the City shall not be liable for any damages, and expressly waives all claims, and agrees to defend, indemnify, and hold harmless the City from any and all claims brought by User, its employees or agents, or third parties which arise out of the user's access or use of data provided.





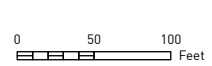
# Attachment B: Planning File 21-003



Disclaimer

**Data Sources**  
 \* Ramsey County GIS Base Map (5/4/2021)  
 \* Aerial Data: Surdex (4/2020)  
 For further information regarding the contents of this map contact:  
 City of Roseville, Community Development Department,  
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MIDLAND LEGACY ESTATE-3<sup>RD</sup> ADDITION  
ROSEVILLE, MINNESOTA  
May 14, 2021

Midland Legacy Estate – 3<sup>rd</sup> Addition residential subdivision consists of eleven single family lots with urban road sections, a park, and storm sewer throughout the 6.4-acre site.

The proposed development will convert an existing home and combine an additional 0.5 acres of land with the existing 5.9-acre site to provide the sufficient area for the proposed 11 residential lots. This project will re-align County Road B-W and remove the cul-de-sac on the south portion of the property to expand home development in that area. The south east lot is proposed to be a City Park to meet the guidelines and slopes as directed by the City Park Department.

Due to the recent change in code requirement for the setback of new roadway, this project has included the additional 0.5 acres in land to provide a private roadway on the northwest portion of the project. The private road will end in a cul-de-sac as shown. There will be a private driveway to allow the 5 parcels to access the proposed cul-de-sac. Due to the wetland in the center of the parcel and excessive relieve between the high area of Eustis Street and the east side of the property, the proposed private drive will provide access to the 5 residential lots.

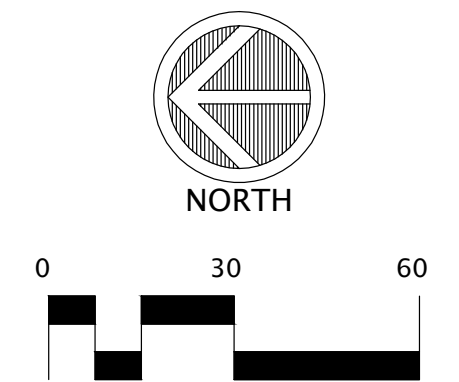
This project will utilize the existing storm wetland that exists north of the existing cul-de-sac of County Road B-W in rate control for the site improvements proposed.

# MIDLAND LEGACY ESTATE 3RD ADDITON

**CONTACT:**  
 HEIFORT HILLS DEVELOPMENT, LLC  
 ATTN: TODD GANZ  
 PHONE: 612-369-2747  
 tdganz@gmail.com

**COUNTY/CITY:**  
**RAMSEY COUNTY**  
**CITY OF ROSEVILLE**

PID#172923120003  
 MIDLAND HILLS COUNTRY CLUB  
 2001 FULHAM ST



**REVISIONS:**

DATE	REVISION
4-8-19	PRELIMINARY PLAT
5-6-21	PRELIMINARY PLAT

**CERTIFICATION:**  
 I hereby certify that this plan was prepared by me, or under my direct supervision, and that I am a duly Licensed Land Surveyor under the laws of the state of Minnesota.

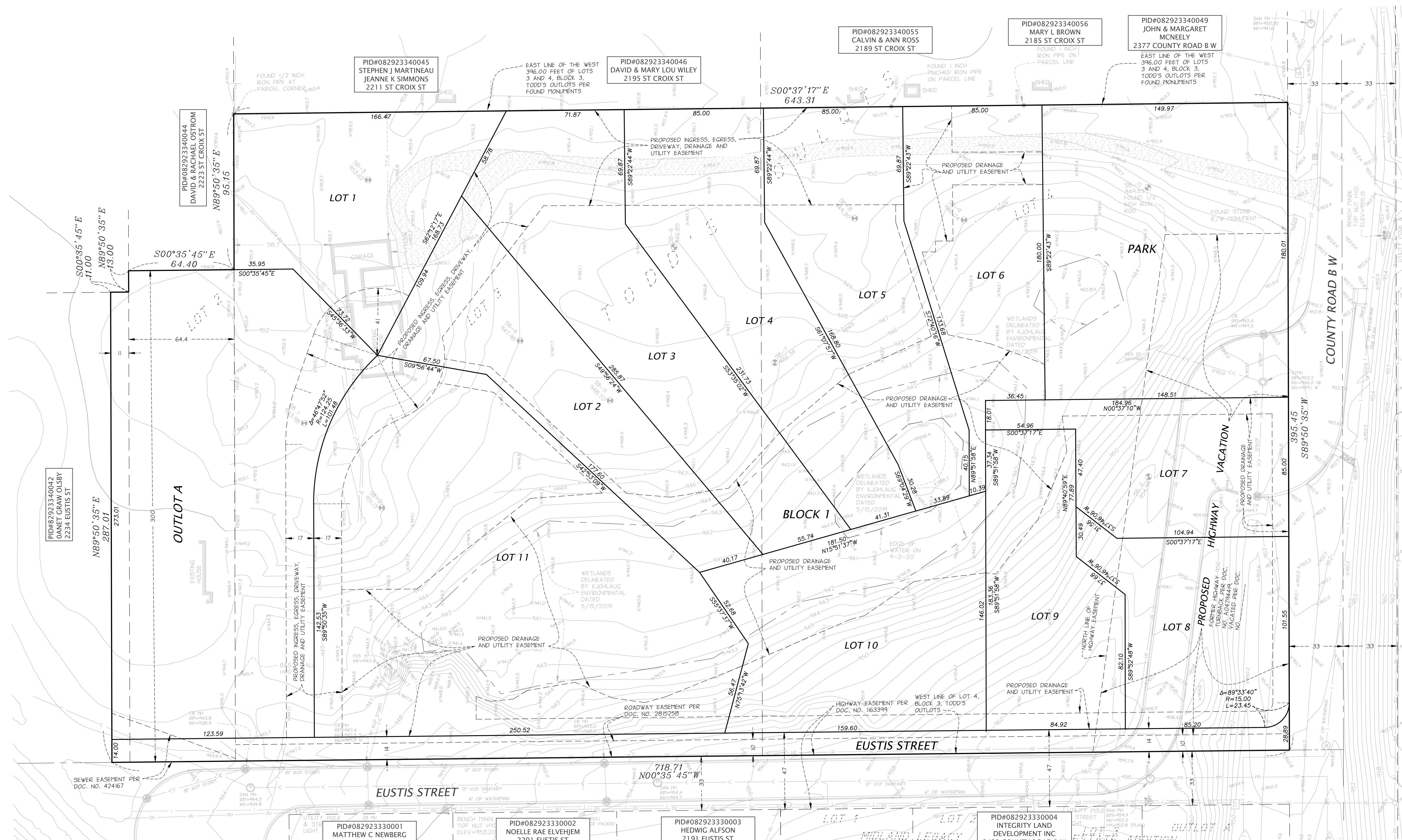
*Daniel L. Thurmes*  
 Daniel L. Thurmes Registration Number: 25718  
 Date: 2-27-20

**PROJECT LOCATION:**  
**2395**  
**COUNTY ROAD B W**  
**PID#082923340057**  
**2224**  
**EUSTIS ST.**  
**PID#082923340043**

Suite #200  
 1970 Northwestern Ave.  
 Stillwater, MN 55082  
 Phone 651.275.8969  
 dan@cssurvey.net

**CORNERSTONE LAND SURVEYING, INC.**  
**PRELIMINARY PLAT S2**

FILE NAME: SURVTG04  
 PROJECT NO: TCG2004



**LEGAL DESCRIPTION:**  
 The following Legal Description is as shown on Legacy Title as agent for Stewart Title Guaranty Company Title Commitment No. MN07623, dated February 4th, 2020.

The West 396 feet of Lot 3 and 4, Block 3, Todd's Outlots, Ramsey County, Minnesota. Certificate of Title No. 35461

AND

(PER CERTIFICATE OF TITLE NO. 577408)  
 The South 11 feet of the West 287 feet of the North 128.8 feet of the South 193.2 feet and the West 300 feet of the South 64.4 feet, front and rear of Lot 2, Block 3, Todd's Outlots.

**EXISTING EASEMENT NOTES:**  
 The following surveying related items appear on the Legacy Title as agent for Stewart Title Guaranty Company Title Commitment No. MN07623, dated February 4th, 2020.

- Subject to the following easement: a right to put in and maintain permanently four poles upon the east 237 feet of the dividing line between lots 2 and 3 in Block 3, Todd's Outlots, said poles being for the purpose of conveying electric light wires, provided that the east one of said poles shall not be less the 75 feet west of the west line of St. Croix Street. See DOCUMENT NUMBER 229904.
- Quit Claim Deed in favor of the City of Roseville for highway purposes, filed November 19, 2019 as Document No. 2652928.
- Subject to 10' Widening Easement per Document No. 2815258.
- Rights of the public and others entitled to the use of that portion of the Land lying within the bounds of County Road B West

Subject to restrictions contained in Document Number 108034 & Easement for sewer per Doc. No. 424167

**PROPOSED HIGHWAY VACATION:**  
 The following Legal Description is for the vacation of the former MNDOT right of way Parcel 303E that was deeded to the City of Roseville.

That part of Tract A described below:  
 Tract A. The west 396 feet of Lot 4, Block 3, Todd's Outlots, according to the plat thereof on file and of record in the office of the County Recorder in and for Ramsey County, Minnesota; the title thereto being registered;  
 which lies westerly of a line run parallel with and distant 80 feet westerly of the east line of said Lot 4 and southerly of Line 1 described below:  
 Line 1. Beginning at a point on the above mentioned 80 foot parallel line, distant 75 feet north of the south line of said Lot 4; thence northwesterly to a point on the west line of said Lot 4, distant 120 feet north of the southwest corner thereof and there terminating.

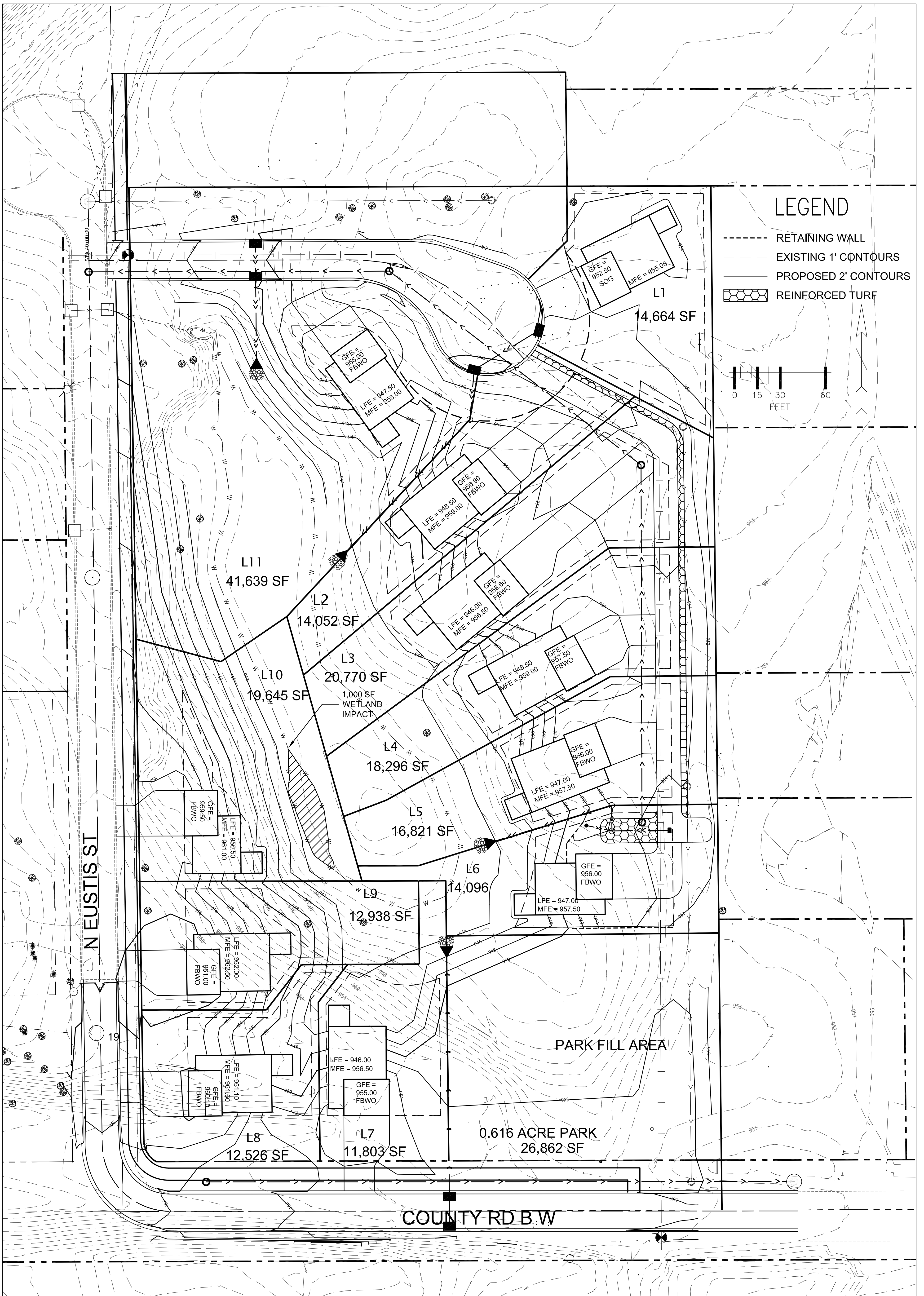
**DEVELOPMENT DATA:**  
 TOTAL AREA OF PARCEL AS SHOWN = 276,770 SQ.FT./6.35 AC.  
 PROPOSED LOTS = 11  
 LOT 1, BLOCK 1 = 15,552 SQ.FT.  
 LOT 2, BLOCK 1 = 14,936 SQ.FT.  
 LOT 3, BLOCK 1 = 20,766 SQ.FT.  
 LOT 4, BLOCK 1 = 18,301 SQ.FT.  
 LOT 5, BLOCK 1 = 16,825 SQ.FT.  
 LOT 6, BLOCK 1 = 14,096 SQ.FT.  
 LOT 7, BLOCK 1 = 11,787 SQ.FT.  
 LOT 8, BLOCK 1 = 12,514 SQ.FT.  
 LOT 9, BLOCK 1 = 12,902 SQ.FT.  
 LOT 10, BLOCK 1 = 19,469 SQ.FT.  
 LOT 11, BLOCK 1 = 45,639 SQ.FT.  
 PROPOSED OUTLOT A = 37,010 SQ.FT.  
 PROPOSED PARK = 26,862 SQ.FT.  
 PROPOSED R/W = 10,109 SQ.FT.

**NOTE:**  
 THE PROPOSED DRIVEWAY EASEMENT WILL HAVE TO BE BY A SEPARATE DOCUMENT NUMBER AS IT CAN'T BE DEDICATED ON THE PLAT

**LEGEND:**

● FOUND MONUMENT 1/2" IP MARKED RLS 14580	⊕ WATER VALVE	— U— UNDERGROUND ELECTRIC
○ SET 1/2" IRON PIPE MARKED RLS NO. 25718	⊕ FLAG POLE	— U— UNDERGROUND CABLE TV
○ CABLE TV PEDESTAL	⊕ MAIL BOX	— U— UNDERGROUND FIBER OPTIC
○ AIR CONDITIONER	⊕ TRAFFIC SIGN	— U— UNDERGROUND TELEPHONE
○ ELECTRIC MANHOLE	⊕ UNKNOWN MANHOLE	— U— OVERHEAD UTILITY
○ ELECTRIC METER	⊕ SOIL BORING	— U— UNDERGROUND GAS
○ ELECTRIC PEDESTAL	⊕ SPOT ELEVATION	— S— SANITARY SEWER
○ ELECTRIC TRANSFORMER	⊕ TRAFFIC SIGNAL	— S— STORM SEWER
○ LIGHT POLE	⊕ CONIFEROUS TREE	— W— WATER MAIN
○ GUY WIRE	⊕ DECIDUOUS TREE	— F— FENCE
○ POWER POLE	⊕ FLARED END SECTION	— C— CURB (TYPICAL)
○ GAS MANHOLE	⊕ STORM MANHOLE	— CO— CONTOURS
○ GAS METER	⊕ FIRE SOFT CONNECTION	— B— BUILDING LINE
○ TELEPHONE MANHOLE	⊕ HYDRANT	— BS— BITUMINOUS SURFACE
○ TELEPHONE PEDESTAL	⊕ CURB STOP	— CS— CONCRETE SURFACE
○ SANITARY CLEANOUT	⊕ WATER WELL	
○ CATCH BASIN	⊕ WATER MANHOLE	
○ STORM DRAIN	⊕ WATER METER	
	⊕ POST INDICATOR VALVE	





Description  
2003 GRADING PLAN – 1:30 SCALE

Drawn  
GJM

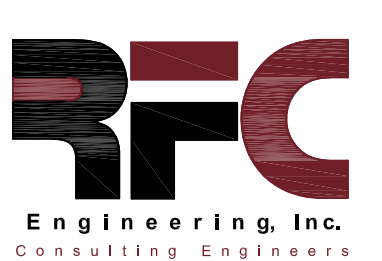
Date  
5/7/21

Project No.  
2003

Sheet No.  
1

Project  
MIDLAND LEGACY ESTATE 3RD ADDITION

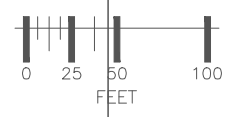
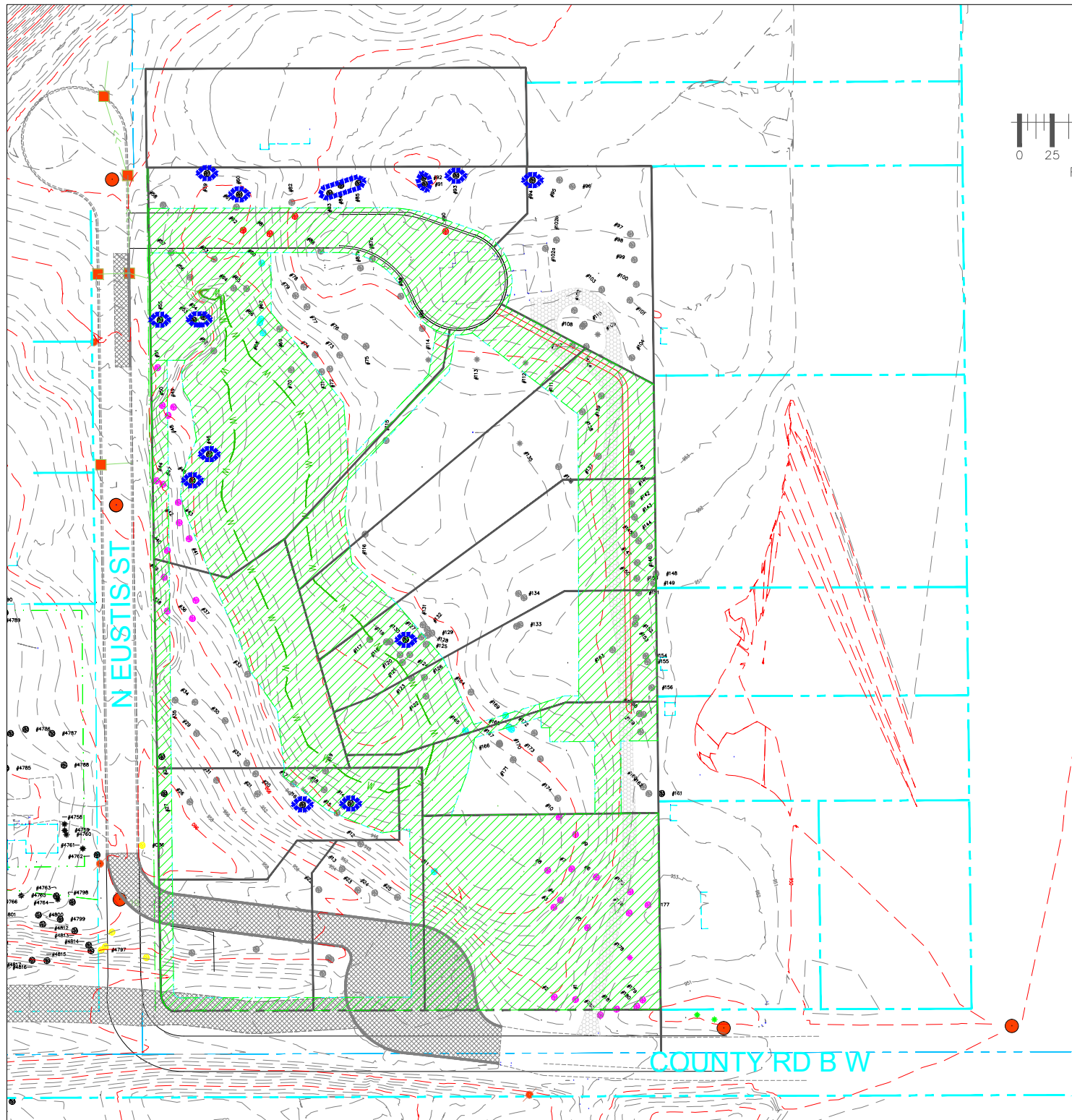
13635 Johnson Street  
Ham Lake, MN 55304  
Voice: 763.862.8000  
Fax: 763.862.8042





DBH	TAG #	SPECIES	CONDITION	CATEGORY	COMMON 7-11" DIAMETER TREE EXEMPTIONS (PUBLIC STREET, UTILITY, OR PONDING EASEMENTS, OR POOR CONDITION)	TOTAL NET COMMON DIAMETERS	COMMON REMOVED	SIGNIFICANT 12-28" DIAMETER TREE EXEMPTIONS (PUBLIC STREET, UTILITY, OR PONDING EASEMENTS, OR POOR CONDITION)	TOTAL SIGNIFICANT DBH	HERITAGE TREE EXEMPTIONS (PUBLIC STREET UTILITY, OR PONDING EASEMENTS OR POOR CONDITION)	TOTAL NET HERITAGE DBH	HERITAGE REMOVED
7	1	Hackberry	Good	Common	7							
22	2	Oak, red	Great	Significant			22					
30	3	Oak, red	Fair	Heritage						30		
7	4	Boxelder	Fair	Common	7							
26	5	Boxelder	Fair	Significant			26					
38	6	Oak, white	Excellent	Heritage						38		
10	7	Boxelder	Fair	Common	10							
16	8	Boxelder	Fair	Significant			16					
21	9	Boxelder	Fair	Significant			21					
16	10	Boxelder	Fair	Significant			16					
18	11	Boxelder	Fair	Significant			18					
20	12	Oak, red	Great	Significant				20	20			
25	13	Cottonwood, eastern	Fair	Significant				25	25			
17	14	Boxelder	Fair	Significant				17				
10	15	Boxelder	Fair	Common	10	10						
38	16	Oak, white	Great	Heritage						38		
6	17	Boxelder	Fair	Common	6	6						
8	18	Boxelder	Fair	Common	8	8						
13	19	Ash, green	Great	Significant				13	13			
9	20	Boxelder	Fair	Common	9	9						
14	21	Boxelder	Fair	Significant				14	14			
11	22	Boxelder	Fair	Common	11	11						
9	23	Boxelder	Fair	Common	9	9						
10	24	Boxelder	Fair	Common	10	10						
15	25	Boxelder	Fair	Significant				15	15			
18	26	Boxelder	Fair	Significant				18	18			
11	27	Ash, green	Fair	Common	11							
10	28	Boxelder	Fair	Common	10							
12	29	Boxelder	Fair	Significant				12	12			
26	30	Oak, white	Great	Significant				26	26			
9	31	Boxelder	Fair	Common	9	9						
9	32	Boxelder	Fair	Common	9	9						
26	33	Oak, white	Good	Significant				26	26			
13	34	Oak, white	Fair	Significant				13	13			
24	35	Oak, white	Fair	Significant				24	24			
9	36	Boxelder	Fair	Common	9	9						
15	37	Boxelder	Fair	Significant			15					
15	38	Boxelder	Fair	Significant			15					
26	39	Boxelder	Fair	Significant			26					
26	40	Oak, white	Fair	Significant			26					
14	41	Boxelder	Fair	Significant			14					
33	42	Oak, white	Good	Heritage						33		
26	43	Oak, white	Good	Significant			26					
7	44	Boxelder	Fair	Common	7							
13	45	Boxelder	Fair	Significant				13				
13	46	Boxelder	Fair	Significant				13				
23	47	Oak, white	Good	Significant			23					
14	48	Boxelder	Fair	Significant			14					
15	49	Boxelder	Fair	Significant			15					
19	50	Boxelder	Fair	Significant			19					
10	51	Boxelder	Fair	Common	10							
15	52	Boxelder	Fair	Significant				15	15			
12	53	Boxelder	Fair	Significant				12				
14	54	Boxelder	Fair	Significant				14				
8	55	Boxelder	Fair	Common	8							
30	56	Cottonwood, eastern	Great	Heritage						30	30	
19	57	Boxelder	Fair	Significant				19	19			
17	58	Boxelder	Fair	Significant				17	17			
19	59	Maple, silver	Good	Significant				19				
7	60	Ash, green	Good	Common	7							
6	61	Boxelder	Good	Common	6	6						
36	62	Oak, white	Good	Heritage						36		
29	63	Cottonwood, eastern	Good	Heritage						29	29	
9	64	Boxelder	Fair	Common	9	9						
6	65	Boxelder	Fair	Common	6	6						
9	66	Boxelder	Fair	Common	9							
12	67	Oak, red	Good	Significant			12					
8	68	Aspen	Good	Common	8							
8	69	Oak, bur	Good	Common	8	8						
7	70	Aspen	Good	Common	7	7						
6	71	Aspen	Good	Common	6	6						
7	72	Aspen	Good	Common	7	7						
6	73	Aspen	Good	Common	6	6						
21	74	Boxelder	Fair	Significant				21	21			
20	75	Oak, red	Good	Significant				20	20			
6	76	Aspen	Good	Common	6	6						
33	77	Boxelder	Fair	Heritage						33	33	
7	78	Boxelder	Good	Common	7	7						
7	79	Boxelder	Fair	Common	7	7						
8	80	Boxelder	Fair	Common	8							
7	81	Boxelder	Fair	Common	7							
25	82	Oak, bur	Fair	Significant			25					
7	83	Boxelder	Fair	Common	7							
13	84	Boxelder	Fair	Significant						13		
14	85	Boxelder	Fair	Significant						14		
14	86	Ash, green	Fair	Significant			14					
32	87	Oak, white	Good	Heritage						32	32	
32	88	Oak, white	Fair	Heritage						32	32	
34	89	Oak, white	Fair	Heritage						34	34	
34	90	Oak, bur	Good	Heritage						34	34	
11	91	Boxelder	Fair	Common	11							
9	92	Boxelder	Fair	Common	9							
13	93	Boxelder	Fair	Significant				13				
16	94	Boxelder	Fair	Significant				16				
13	95	Boxelder	Fair	Significant				13	13			
16	96	Boxelder	Fair	Significant				16	16			
7	97	Ash, green	Fair	Common	7	7						
7	98	Boxelder	Fair	Common	7	7						
13	99	Boxelder	Fair	Significant				13	13			
8	100	Boxelder	Fair	Common	8	8						





LEGEND

- RETAINING WALL
- EXISTING 1' CONTOURS
- PROPOSED 2' CONTOURS
- REINFORCED TURF
- PROPOSED EASEMENT
- TREE PROTECTION FENCING


NO.	DATE	BY	REVISION	DESCRIPTION	DATE	BY	REVISION	DESCRIPTION
1	05/14/21	GJM	1	INITIAL DESIGN				
2	05/14/21	GJM	2	REVISED DESIGN				
3	05/14/21	GJM	3	REVISED DESIGN				
4	05/14/21	GJM	4	REVISED DESIGN				
5	05/14/21	GJM	5	REVISED DESIGN				
6	05/14/21	GJM	6	REVISED DESIGN				
7	05/14/21	GJM	7	REVISED DESIGN				
8	05/14/21	GJM	8	REVISED DESIGN				
9	05/14/21	GJM	9	REVISED DESIGN				
10	05/14/21	GJM	10	REVISED DESIGN				
11	05/14/21	GJM	11	REVISED DESIGN				
12	05/14/21	GJM	12	REVISED DESIGN				
13	05/14/21	GJM	13	REVISED DESIGN				
14	05/14/21	GJM	14	REVISED DESIGN				
15	05/14/21	GJM	15	REVISED DESIGN				
16	05/14/21	GJM	16	REVISED DESIGN				
17	05/14/21	GJM	17	REVISED DESIGN				
18	05/14/21	GJM	18	REVISED DESIGN				
19	05/14/21	GJM	19	REVISED DESIGN				
20	05/14/21	GJM	20	REVISED DESIGN				
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23	05/14/21	GJM	23	REVISED DESIGN				
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25	05/14/21	GJM	25	REVISED DESIGN				
26	05/14/21	GJM	26	REVISED DESIGN				
27	05/14/21	GJM	27	REVISED DESIGN				
28	05/14/21	GJM	28	REVISED DESIGN				
29	05/14/21	GJM	29	REVISED DESIGN				
30	05/14/21	GJM	30	REVISED DESIGN				
31	05/14/21	GJM	31	REVISED DESIGN				
32	05/14/21	GJM	32	REVISED DESIGN				
33	05/14/21	GJM	33	REVISED DESIGN				
34	05/14/21	GJM	34	REVISED DESIGN				
35	05/14/21	GJM	35	REVISED DESIGN				
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37	05/14/21	GJM	37	REVISED DESIGN				
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43	05/14/21	GJM	43	REVISED DESIGN				
44	05/14/21	GJM	44	REVISED DESIGN				
45	05/14/21	GJM	45	REVISED DESIGN				
46	05/14/21	GJM	46	REVISED DESIGN				
47	05/14/21	GJM	47	REVISED DESIGN				
48	05/14/21	GJM	48	REVISED DESIGN				
49	05/14/21	GJM	49	REVISED DESIGN				
50	05/14/21	GJM	50	REVISED DESIGN				

Description  
2003 TREE PRESERVATION PLAN – 1:50 SCALE

Drawn GJM	Date 5/14/21	Project No. 2003	Sheet No. 1
--------------	-----------------	---------------------	----------------

Project  
MIDLAND LEGACY ESTATE 3RD ADDITION

13635 Johnson Street  
Ham Lake, MN 55304  
Voice: 763.862.8000  
Fax: 763.862.8042





To: City of Roseville

From: Integrity Land Development, Inc. – Todd Ganz, President

Summary of Public/Community Discussions

Meeting at Midland Hills Country Club & at 6 PM to 6:50 PM Zoom meeting.

There have been no emails or phone call discussions from the community.

The questions & discussions from the 2 meetings in the Midland Hills Country Club and the Zoom meeting were all the same.

The most common question was, how much more traffic will be going to the north Eustis St? We answered the most for next 3 years and after 3 years will become normal for the neighborhood.

How long will it take to straighten County Road B & Eustis and finish the street & grading in the project? We answered that it would probably be about 60 days with weather permitting.

Are you going to widen Eustis St., so we can park on Eustis and still drive through easily? We answered that the city instructed us to add a 10-foot easement to the east side of Eustis St. so the 8 foot wide asphalt park path will be constructed in the 10 foot easement

Will there be more traffic on Eustis St? We answered, that it would be more traffic for the new 10 lots to drive to their home which are connected to Eustis St, and more traffic for about 3 years as the all the homes are constructed.

Is the park path on the east side of Eustis St. going to be part of the driving area? We answered that it is a walking asphalt path off the street.

How long will it take to get the development and all the houses built? We answered approximately 3 years.

Will there be fences/screening built on the east property line or more trees be planted? We answered that the future owner can build a fence if they choose to. We may require that 6 to 8 pine trees be planted on the east lot line. We are working to save as many of the existing trees as we can. We are not removing all the trees on this property.

Is the shared driveway on lots 3, 4, 5 go through the southeast park? We answered no it has a 40' turnaround on Lot 5, so if FedEx delivers, or the fire department comes in there is room for them to turn around.

Will the County Road B & Eustis St be straightened and put into a standard 90 degree corner? We answered yes it will be straightened and made a 90 degree turn at the beginning of this project.

When will the new 3 new models on the west side of Eustis start in Midland Legacy 2<sup>nd</sup> Addition? We answered that they will be starting construction in May 2021 and then we showed them photos of what the future new houses will look like in this project, with prices of \$650,000 to \$1,100,000.

Why is the main waterline being looped from Eustis St east & south to County Road B? We answered, so the water flows and stays fresh for all the people in the neighborhood.

When is the outlot with 3 lots being developed? We told them once the estate finishes the Probate Court it will be ready to plat. Approximately 3 to 4 months after we plat this project.

What kind of street lighting will be installed? We answered that it would be a low height LED street light that only lights downward to the road and does not send light to the windows of the new houses or the neighbors houses.

What kind of yard lighting will be allowed? We answered that new house exterior lighting will be required to downward lighting. No lighting will be allowed that sends lighting to a neighbors house.

How will the surface water storm pond that exists be protected or survive? We answered that the RCWD and the State of MN representative that met us at the site, recommended that we put in little storm ponds that catch the water run off to get the silt, but keep the height of the perimeter low so that the water can get to existing storm pond so it will stay wet. If we hold too much water the existing wet storm pond will deteriorate.

Will the zoning of this property change? We answered, no we are using the existing zoning of this neighborhood, which is LDR-1 and allows us to get 3 lots per acre.

Will there be a looped path that goes north, northeast off the existing Eustis cul-de-sac paralleling the highway sound wall, over to the Saint Croix Street cul-de-sac? We answered that it was told to us from the city that there is an existing public path (easement) in there, but we do not know when the city will be finishing it.

**Bryan Lloyd**

---

**From:** CenturyLink Customer [REDACTED]  
**Sent:** Monday, May 24, 2021 2:03 PM  
**To:** Thomas Paschke; Bryan Lloyd; Dan Roe; Wayne Groff; Jason Etten; rwillmus; Julie Strahan  
**Subject:** June 2, 2021 Planning Commission Meeting

Caution: This email originated outside our organization; please use caution.

To: Roseville Planning Commission and Roseville City Council

I attended the virtual open house for 2395 County Road B West for the redesign of the Midland Legacy Estate development. This is a second proposed plan by Integrity Land Development for this piece of property. It is essentially the same plan except instead of 16 single family houses along a driveway behind the homes on St. Croix Street there are 6 larger lots and homes off of the approximately 450 foot long driveway. Two of those homes (Lots 1 and 2) have the proposed private road and the proposed driveway as boundaries of their lots.

The developer is not calling it a road, but a driveway in front of the proposed homes and behind the backyards of homes on St. Croix Street. Most delivery trucks (UPS, Amazon, USPS, and other like companies) park their vehicles in the road and walk packages up to houses on driveways and sidewalks. This long driveway will be used as a road for deliveries. The delivery drivers will not want to walk up that long driveway to make deliveries. Also, it looks like from the new design that the T shaped turnaround at the south end of the driveway butts up against the back of a lot on St. Croix. What is the setback for that T-turnaround?

Will the houses on the driveway have to put their garbage, yard waste, and recycling containers on the private road or will the garbage and recycling trucks be permitted to use the driveway. I think the City Council needs to define what a road is and what a driveway is. I have seen 2 houses using the same driveway, but never 6 houses.

If this plan is approved, the people living on St. Croix will have to put up with traffic from garbage haulers, delivery trucks, recycling trucks, and visitors on both sides of their property. It will increase the amount of noise they hear and they will also have headlights shining into their backyards and homes. I am sure it would also lower their property values. Even if they can't access the driveway behind their homes, they will basically be living on a thru-lot.

I also notice from Integrity's second proposal that all but one lot will have wetland setback land in their back yards. In particular Lots 5, 6, 11, and 12 have wetland setback as the majority part of their back yards and Lots 2, 3, and 4 have the wetlands and wetland setback as part of the size of their lots. I would like to know who monitors what those potential home owners do in their back yards. Who monitors that lawn chemicals aren't running off into the wetlands and that there is no encroachment of the wetland setback by the potential home owners?

I urge the Planning Commission not to recommend this proposed second redesign of Midland Legacy Estate to the City Council until the City Council has a definition of a driveway. Hopefully the City Council will decide that more than 2 houses using a driveway is not a driveway, but a road and that they will not make the houses on St. Croix Street thru-lots.

Sincerely, Fulham Street  
Nancy Nelson Roseville

**INTEROFFICE MEMORANDUM**

---

**Date:** May 24, 2021

**To:** Bryan Lloyd, Senior Planner

**From:** Jesse Freihammer, City Engineer - Roseville Public Works

**RE:** 2395 (Midland Legacy Estates) Development Review

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The Public Works Department reviewed the proposed plans for the project noted above and offer the following comments with regard to the project's impact on City services and/or infrastructure:

1. Site Plan

- The proposed private driveway serving lots 2-6, totals five lots on a shared private drive. If Lot 2 is directed out to the private cul-de-sac (road) four lots would still be on the shared private drive. Per Public Works guidelines for roadway and driveway standards either layout would require a private roadway be built instead of a shared driveway.
  - Public Works Guidelines allow a private driveway when serving three or less detached residential units with a maximum shared access centerline length of 150 feet from the connection to a public or private street
  - This proposed plan has at least four lots served from the private drive and exceeds the 150 foot maximum length. Therefore the proposed private drive would have to be constructed as a private roadway with a minimum width of 24 feet (with no on street parking). However, this would be in violation of zoning regulations prohibiting through streets given the single family homes on the east side of the proposed driveway/roadway.
- Per City Code, all lots within 300 feet of a wetland are considered shoreland and can have a maximum impervious surface of 25% of the total lot area.
- The vacation of a portion of excess County Road B will be required to accommodate the new lots and the realignment of County Road B & Eustis Street.
- Pathway improvements, as shown on the proposed plans, on the west and south side of the development should be required to make connections to the existing pathway on County Road B. A future pathway connection from the end of Eustis Street to St Croix Street just south of TH 36/135W is in the pathway master plan. This will be required to be constructed as part of the development.
- See attached Traffic Impact Memo dated May 20, 2021 documenting traffic impacts.

- A public improvement plan will be required for the following
  - New sanitary sewer
  - New watermain
  - New storm sewer which serves public roadways.
  - New pathway improvements
  - Realigned roadway improvements for County Road B & Eustis.
- Overall public improvement plans would need to be approved by the City Engineer prior to permit issuance.
- 2. Roadway Improvements
  - The proposed vacation and realignment will require County Road B and Eustis Street to be reconstructed. These will remain public roads.
  - The proposed private road, Legacy path, is 26' curb to curb. Parking would be allowed on one side of the roadway.
- 3. Utilities
  - Water
    - Watermain will need to be extended from County Road B and looped to Eustis street to serve most of the lots
    - The watermain should be designed to be public improvements
  - Sanitary
    - Some lots will require sanitary sewer to be extended from County Road B and from Eustis Street. Adequate capacity is available.
    - The sanitary sewer should be designed to be public improvements.
  - Storm Sewer
    - The development has to meet city and watershed district stormwater standards. These standards include water quality, rate, and volume requirements that will ensure the property will protect downstream surface waters and not add to any local drainage and flooding issues. Stormwater from the site will ultimately outlet from the existing wetland that is onsite. The development must provide initial treatment of stormwater prior to discharging into the wetland. The draft plans submitted to the city do not take into account our Stormwater Standards. Engineering staff consider the submittal incomplete given the amount of missing information, and cannot provide a thorough review to ensure rate and water quality standards are met. The developer will need to submit additional information prior to staff review.
    - The storm sewer improvements within the site would be private.
    - Rice Creek Watershed District Permit Required
    - NPDES Permit Required
- 4. Wetlands
  - The wetland that is onsite is regulated by the Rice Creek Watershed District as they are the Local Government Unit (LGU) responsible for implementing the Wetland Conservation Act (WCA). Any modifications to the wetland (boundary change, permanent or temporary impacts, etc.) will first need to be approved by the district. Structure and other impervious surface setbacks from the wetland

are regulated by the city. The city will review the plans to ensure the development is meeting these requirements. At this juncture, Engineering Staff can't review the plans as submitted due to the amount of missing stormwater and wetland information.

5. General

- County Road B between Eustis Street and Cleveland Avenue is scheduled for reconstruction by the City of Roseville in 2024. Prior to construction the City will work with the neighborhood to address design features of the roadway. At minimum the new roadway will address the poor pavement condition, drainage and reconstruction of the existing pathway.

Thank you for the opportunity to provide feedback on this project at this time. As the project advances, Public Works staff will continue to review any forthcoming plans and provide additional reviews and feedback as necessary. Please contact me should there be questions or concerns regarding any of the information contained herein.



**INTEROFFICE MEMORANDUM**

**Date:** May 20, 2021  
**To:** Janice Gundlach, Community Development Director  
**From:** Jesse Freihammer, Asst. Public Works Director/City Engineer  
**RE:** Traffic Impacts, Midland Legacy Estate, County Road B & Eustis

**Project Description**

The development on the northeast corner of County Road B and Eustis is on an existing 5.03 acre parcel. The parcel currently only has a single family home located on it. The parcel also has a wetland that is approximately one acre in size. The proposed development is 11 single family townhomes on the parcel. This memo will document the historic, existing and proposed traffic related to the development.

**Analysis**

Historically, County Road B was open to traffic from TH 280 to Cleveland Avenue. The County Road B Road access to TH 280 was closed in 2007 resulting in all traffic in the neighborhood using only Cleveland Avenue to access the neighborhood. County Road B was turned back from Ramsey County and became a city street in 2014. Prior to the road closure at TH 280, traffic counts were 2,650 ADT (2005). After the road closure traffic on County Road B was 1,300 ADT (2009).

Current traffic counts on County Road B is 950 ADT as of 2017. Since this is an isolated neighborhood with only one outlet the count is made very near Cleveland Avenue and should reflect all traffic leaving the neighborhood. Traffic will tend to decrease farther west on County Road B as there limited destinations for neighborhood vehicle traffic to drive to.

Traffic for single family homes is typically estimated at 9.57 trips per day per unit. Traffic for low rise townhomes is typically estimated at 7.32 trips per day per unit. Based on these standards, the new development is estimated to have a net increase of 95 trips per day on County Road B. This is an increase of about 10.0% over existing traffic but is still 60% less than when County Road B was open to TH 280.

<b>County Road B Traffic, West of Cleveland Avenue</b>					
<b>Year</b>	<b>Existing Parcel Traffic (ADT)</b>	<b>Proposed Trips</b>	<b>Total County Road B Average Daily Traffic</b>	<b>% Increase from Current</b>	<b>% Increase from 2005</b>
2005			2650		
2009			1300		
2017 (Current)			950		
Proposed Development (19 Townhomes)	10	105	1045	10.0%	-60.6%

County Road B can handle this level of traffic. This proposed level of traffic is less than many other residential roads that serve as collector streets. The following are a list of residential streets that function similar to County Road B, west of Cleveland Avenue, with higher volumes of traffic.

<b>Traffic Volumes on Residential Collector Streets, 2017 ADT's</b>			
<b><u>Street Name</u></b>	<b><u>Start</u></b>	<b><u>End</u></b>	<b><u>ADT</u></b>
Dale St	County Rd C	S Owasso	2900
Josephine	Hamline	Victoria	2200
Victoria	Larpentuer	Roselawn	2100
Roselawn	Hamline	Lexington	2050
Lydia	Snelling	Hamline	1900
County Rd C2	Hamline	Lexington	1400
Minnesota	Western	Rice	1350
County Rd C2	Lexington	Victoria	1300
Roselawn	Dale	S McCarrpms	1150
<b><i>Proposed County B</i></b>	<b><i>Cul-de-sac</i></b>	<b><i>Cleveland</i></b>	<b><i>1045</i></b>

**Summary**

The proposed development will increase existing traffic in the neighborhood slightly. County Road B can handle the increase in traffic on the road way. The increased traffic levels are less than historic peak traffic in the neighborhood and will be less than many similar residential collector streets in Roseville.

The current pavement of County Road B was designed to handle County traffic loads and volumes. The pavement condition is in need of repair, due to the age of the roadway. The City’s capital improvement plan includes the entire segment of County Road B, west of Cleveland Avenue. The current plan is for a reconstruction project schedule in 2024 which will address the poor pavement condition of the roadway. Any reconstruction or reconditioning project will be able to handle the current and projected traffic from this development. The reconstruction project will potentially allow roadway design improvements to improve drainage, traffic calming measures and other safety improvements, such as upgrades to the pedestrian/bicycle trail to be implemented. Public outreach for this project will be a very important role when planning for this project begins in the future.

Please contact me should there be questions or concerns regarding any of the information contained herein.



**INTEROFFICE MEMORANDUM**

---

**Date:** May 26, 2021

**To:** Bryan Lloyd, Senior Planner

**From:** Matthew Johnson, Assistant Director of Parks and Recreation

**RE:** Midland Legacy Estate, 2395 Co. Rd B

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The Parks & Recreation Department reviewed the proposed plans for the project noted above and offer the following comments with regard to the project's impact on City services and/or infrastructure:

1. The Parks and Recreation Commission (PRC) reviewed this proposed project regarding park dedication on May 5, 2021. Their recommendation was land. Draft meeting minutes are attached.
2. Attached is a letter that the developer submitted regarding the condition in which the park parcel will be turned over to the city. It is extremely important that the developer follow through with each of the terms that he listed in this document.
3. The developer stated that "We are saving all the beautiful large Oak trees on the east side of the existing driveway to keep the park beautiful and relaxing with shade." It does not appear that his tree plan accounts for preservation of these trees. These trees were emphasized by the developer in his meeting with the PRC and important to the possible future park. They must be vigorously protected, particularly while construction is in progress. We would want to see these trees identified for protection on the tree preservation plan (as others are) and a full protection plan for these trees, which our staff could review and provide input on.

Thank you for the opportunity to provide feedback and on this project at this time. As the project advances, Parks & Recreation Department staff will continue to review any forthcoming plans and provide additional reviews and feedback as necessary. Please contact me should there be questions or concerns regarding any of the information contained herein.

## Midland Legacy Estate 3rd Addition

### Parkland Information for Parks and Recreation Commission

April 6, 2021

On the new preliminary plat drawing we have shown the park area to be in the Southeast corner of the property for multiple positive reasons.

1. Being a park in the Southeast corner makes it easy for people in the community to access it from County Road B W
2. We are filling in the low drain area that will make this park area flat and easy to use. (You will see how the grade will slope a little at the Northwest corner). The area of flat area in this proposed park is 95%
3. We are saving all the beautiful large Oak trees on the east side of the existing driveway to keep the park beautiful and relaxing with shade.
4. Once we have the park area graded flat it will be quite easy for the City of Roseville to install a playset for children, a nice tennis court with basketball hoops, or whatever all the people in the community would like installed.
5. The average flat elevation of the new part will be 952 which will be slightly graded so it slowly drains at about 2% to the Northwest corner and towards the South County Road B street.
6. We will also be installing silt fence around the new grading and spraying on new grass seed.
7. The whole plat area of Midland Legacy Estate 3<sup>rd</sup> Addition, is 6.1 acres, which includes the Outlot on the north side.
8. The proposed SE park area is 0.615 of an acre, (26,800 Sq. Ft.) which meets the 10% for park land. 180 feet wide on County Road B and 149 feet deep. These dimensions do not include street right-of-way.

Integrity Land Development, Inc.

Todd D. Ganz, President

\_\_\_\_\_  
Date \_\_\_\_\_

1                                   **ROSEVILLE PARKS AND RECREATION COMMISSION**  
2                                   **MEETING MINUTES FOR**  
3                                   **MAY 4, 2021 6:30 p.m.**  
4

- 5   **PRESENT:**                   Arneson, Baggenstoss, Boulton, Brown, Dahlstrom, Heikkila, Hoag, Kim,  
6                                   Lenhart, Ybarra  
7   **ABSENT:**                   Carlson  
8   **STAFF:**                    Brokke, Christensen, Johnson  
9

10 1) **INTRODUCTIONS**

11   Chair Hoag introduced the virtual Zoom format for the meeting due to the COVID-19 pandemic.  
12   State Law allows for an exception to in-person public meetings during pandemics to ensure the  
13   safety of commissioners, staff and the public. The public was still encouraged to participate in the  
14   meeting using the Zoom platform.  
15

16 2) **ROLL CALL/PUBLIC COMMENT**

17  
18                   **Roll Call Commissioners:** Arneson, Baggenstoss, Boulton, Brown, Dahlstrom, Heikkila,  
19                   Kim, Lenhart, Ybarra and Hoag.  
20

21                   Chair Hoag called for public comment by members of the audience. No audience in  
22                   attendance.  
23

24 3) **APPROVAL OF MINUTES – APRIL 6, 2021 MEETING**

25  
26                   Commissioner Heikkila moved to approve the minutes. Vice-Chair Dahlstrom seconds.  
27

28                   **Roll Call**

29                   **Ayes:** Arneson, Baggenstoss, Boulton, Brown, Dahlstrom, Heikkila, Hoag, Lenhart and  
30                   Ybarra.

31                   **Nays:** None.

32                   **Abstain:** None.  
33

34 4) **PARK DEDICATION REVIEW AND RECOMMENDATION – MIDLAND LEGACY**  
35 **ESTATES – 2395 COUNTY ROAD B WEST**

36   Staff provided an overview of the updated proposal for the Midland Legacy Estates location at 2395  
37   County Road B West which included information on the Park Dedication process, history, purpose  
38   and options.  
39

40 The Commission reviewed location maps of the parcel and the proposed site plan for the  
41 development which would include a proposed sidewalk along Eustis. This parcel is located in  
42 constellation J. There has been a long standing and strong desire for additional park and recreation  
43 opportunities in this area of the city.  
44

45 The project qualifies for Park Dedication. The proposal summary for Midland Legacy Estates  
46 includes:

- 47 • Total Acreage = 6.1
- 48 • 11 new lots (10 new)
- 49 • Cash Amount: \$42,500 (\$4,250 per unit X 10)
- 50 • Land Amount: .61 Acres

51  
52 Staff noted that a similar plat proposal on this same property was reviewed by the Parks and  
53 Recreation Commission in May and June of last year. For that proposal, the Commission made a  
54 recommendation to accept useable parkland somewhere along County Rd B, and specified an area  
55 on the south and southwest side of the development as a preferred location. That overall proposal  
56 ultimately was not approved by the City Council. On this new proposal, the developer focused on the  
57 Commissions previous parkland dedication recommendation for useable parkland on the south side  
58 of the parcel. The developer has stated that the proposed land dedication will be turned over to the  
59 city as useable parkland that is 95% flat and seeded. Additionally, the developer has committed to  
60 protecting the three large existing oak trees on the east side of the current driveway which would  
61 allow for existing tree cover to the new park space.  
62

63 Chair Hoag reminded everyone that the role of the Commission is not to scrutinize the development  
64 but to recommend land or cash to the City Council to satisfy Park Dedication.  
65

66 The Commission discussed the development location in order to orientate themselves with the exact  
67 location of the parcel. They also discussed what a .61 size park could potentially look like.  
68

69 Commissioner Baggenstoss asked if there will be trail connections as part of this development. Staff  
70 responded that pathways would be outside the scope of Park Dedication. However, it is staffs  
71 understanding that the developer has the intention to create pathways as part of the Pathway Master  
72 Plan along the south and west side of the parcel.  
73

74 The Commission looked at pictures and discussed the large oak and maple trees that the developer  
75 has proposed to save as part of the parkland.  
76

77 Commissioner Baggenstoss thanked the developer for bringing back a creative proposal that  
78 addressed the Commissions requests that were made in the previous park dedication  
79 recommendation.

80 Commissioner Baggenstoss proposed to recommend the developer’s parkland proposal of  
 81 useable parkland (.616 acres) located on the south side of the parcel, which is 95% flat  
 82 and seeded with protection of the three large existing oak trees to the City Council.  
 83 Commissioner Brown seconded.  
 84

85 **Roll Call**

86 **Ayes:** Arneson, Baggenstoss, Boulton, Brown, Dahlstrom, Heikkila, Hoag, Lenhart and  
 87 Ybarra.

88 **Nays:** None.

89 **Abstain:** None.  
 90

91 **5) STAFF REPORT**

92 a) **NEW OR RELEVANT COMMUNICATIONS AND UPDATE ITEMS**

93 • **Operational Update**

94 ○ **Recent Events**

- 95 ■ Roseville’s Skating Show took place with COVID-19 protocols on April 23 and
- 96 24
- 97 ■ Kite Day brought 125 residents to Central Park at the Victoria Ballfields on April
- 98 24 (10:00 a.m. – 12:00 p.m.)
  - 99 • Free event
  - 100 • Sponsored by Roseville Parks and Recreation, Do Good Roseville and
  - 101 Kiwanis Malt Shop at the MN State Fair
- 102 ■ Earth Day at the Harriet Alexander Nature Center on April 24

103 ○ **Volunteers**

- 104 ■ April 6 – Reservoir Woods Park clean up (BSA Troop 297, 18 Scouts and 12
- 105 parents)
- 106 ■ April 17 – Rain Garden clean-up at Lexington Park (approximately 13 volunteers,
- 107 3 new to Natural Resources)
- 108 ■ April 22 – Frog and Toad Zoom Training (12 volunteers attended, 2 new
- 109 volunteers will do monitoring)
- 110 ■ April 24 – Kite Day
- 111 ■ April 24 – Earth Day (approximately 20 volunteers)
- 112 ■ April 24 – Ice Show (Parent Volunteers)
- 113 ■ April 24 – Youth Baseball had 100 people out at their field Clean-up
- 114 ■ April 24 – Reservoir Woods Clean-up (3 volunteers)
- 115 ■ April 24 – Volunteer-led garlic mustard pull (8 volunteers)
- 116 ■ April 27 – Volunteer-led garlic mustard pull at Reservoir Woods Park

- 120 ○ Cedarholm
  - 121 ▪ 2,500 Rounds played
  - 122 ▪ 508 Summer league members
  - 123 ▪ 78 Jr. league members
  - 124 ▪ 195 Sr. league members
  - 125 ▪ Rentals are picking up again too
    - 126 • All numbers through April 22
- 127 ○ Recreation
  - 128 ▪ Registration continues for summer
  - 129 ▪ Recreation Programs
    - 130 • Full slate of recreational programs
    - 131 • Summer concerts/puppet wagon
  - 132 ▪ Adult Softball – 135 teams
  - 133 ▪ Adult Sand Volleyball – 70 teams
  - 134 ▪ Bocce Ball
  - 135 ▪ Shelter Rentals – 15 rentals (April 15–May 1)
- 136 ○ Rosefest
  - 137 ▪ Wednesday, June 23
    - 138 • Discover Your Parks – Villa Park
  - 139 ▪ Thursday, June 24
    - 140 • Touch a truck
    - 141 • Kids Garage Sale
    - 142 • Golden Rose Medallion Hunt Kick-off
    - 143 • Concert in the Park – Blue Groove Bluegrass Band
  - 144 ▪ Friday, June 25
    - 145 • 9 and Dine on a Dime Golf at Cedarholm
    - 146 • Superhero Carnival
    - 147 • Family Free Skate at the Roseville Skating Center
    - 148 • Calvary Community Theater in the Park
    - 149 • Golden Rose Medallion Hunt
  - 150 ▪ Saturday, June 26
    - 151 • Run for the Roses 5K
    - 152 • Calvary Community Theater in the Park
    - 153 • Backyard Campout
    - 154 • Golden Rose Medallion Hunt
  - 155 ▪ Sunday, June 27
    - 156 • Rosefest Golf Tournament
    - 157 • Porsche Car Show (tentative)
    - 158 • Teddy Bear Band (2 shows)

- 159                                   • Golden Rose Medallion Hunt
- 160                           ▪ Unable to offer in 2021
- 161                                   • Parade – postponed to 2022
- 162                                   • Party in the Park (fireworks) – postponed to 2022
- 163                                   • Taste of Rosefest postponed until fall 2021
- 164
- 165                   • Work on the inclusive play area at Central Park Victoria West is anticipated to begin in mid-
- 166                    May. Special thanks to the Friends of Roseville Parks and the donor, both made this special
- 167                    project possible!
- 168                   • Flower Power planting at Central Park Lexington on Saturday, May 22 from 9 a.m. – Noon.
- 169                    Come volunteer and make Roseville Blooming Boulevard beautiful.
- 170                   • The Park Dedication Ordinance item that was discussed at the April Commission meeting is
- 171                    anticipated to be on the Agenda at the Monday, May 10 City Council meeting. Staff will send
- 172                    out the agenda and packet to the Commission.
- 173                   • The Discover Your Park (DYP) sign-up sheet is available online for Commissioners to sign-up to
- 174                    attend. Staff reiterated how important it is to have Commissioners available at the DYP events to
- 175                    gather feedback from community members on the Pocahontas Park renaming item. Staff agreed
- 176                    to resend the link to Commissioners.
- 177                   • Staff asked the Commission if they felt they would like to meet with the City Council on June 7
- 178                    or if they would prefer to wait until the Pocahontas Park naming item is more wrapped-up. The
- 179                    Commission discussed wanting to keep Emerald Ash Borer (EAB) before the City Council.
- 180                    However, they agreed that waiting may be beneficial in order to allow for additional updates on
- 181                    the Pocahontas Park name topic.
- 182

183 **6) OTHER**

184 Commissioner Lenhart shared the land survey of pre-Europe settlement Pocahontas Park (circa  
 185 1848). Pocahontas Park was in section 10 of the 1848 land survey. The land description from Section  
 186 10 included:

- 187                   • Land rolling
- 188                   • Soil second rate (not good for agriculture)
- 189                   • Scattered timber, oak (bur, white and black)
- 190                   • Lowlands – marsh and lakes
- 191

192 Commissioner Baggenstoss shared that he greatly appreciates the research on the pre-European land  
 193 information. However, he wants to reiterate that the Commission needs to hear from non-white  
 194 voices and what their history and thoughts of the land may be.

195  
 196 Commissioner Lenhart agreed that she looks forward to hearing and learning about those stories as  
 197 well. However, she relayed that she shared this information as it was readily available and she  
 198 thought the group might want to know about it from a historical perspective.

199 Finally, Commissioner Lenhart asked if the Community Gardens in Oasis Park is a Roseville Parks  
200 and Recreation amenity. Staff confirmed that, yes, it is a proud summer time Park and Recreation  
201 offering.

202

203 Meeting adjourned at 7:47p.m.

204

205 Respectfully Submitted,

206 Danielle Christensen, Department Assistant

207

DRAFT



**Bryan Lloyd**

---

**From:** noreply@civicplus.com  
**Sent:** Saturday, May 29, 2021 12:32 PM  
**To:** \*RVPlanningCommission  
**Subject:** Online Form Submittal: Contact Planning Commission

**Caution:** This email originated outside our organization; please use caution.

## Contact Planning Commission

Please complete this online form and submit.

---

**Subject:** Developement at 2395 County Road B

---

### Contact Information

---

**Name:** Stephen Martineau

---

**Address:** 2211 St. Croix Street

---

**City:** Roseville

---

**State:** MN

---

**Zip:** 55113

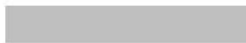
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**How would you prefer to be contacted? Remember to fill in the corresponding contact information.** Email

---

**Phone Number:** *Field not completed.*

---

**Email Address:** 

---

**Please Share Your Comment, Question or Concern**

We have been Roseville residents for over 30 years and object to the continuation of attempts to sub-divide the block with (now partially) with a private driveway that extends from a new cul-de-sac road.

Does the developer and the City of Roseville agree that by elimination of previous plan to connect the private road from Eustis to County B is any different??

Opening our backyards to visits from all sorts of vehicle traffic (not just cars!) behind the houses on St. Croix street is an

---

affront to our general welfare and quality of life. Please reject this new proposal!

Thank you,

Steve and Jeanne Martineau

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Unless restricted by law, all correspondence to and from Roseville City government offices, including information submitted through electronic forms such as this one, may be public data subject to the Minnesota Data Practices Act and/or may be disclosed to third parties.

---

Email not displaying correctly? [View it in your browser.](#)

**Bryan Lloyd**

---

**From:** noreply@civicplus.com  
**Sent:** Sunday, May 30, 2021 8:20 PM  
**To:** \*RVPlanningCommission  
**Subject:** Online Form Submittal: Contact Planning Commission

**Caution:** This email originated outside our organization; please use caution.

## Contact Planning Commission

Please complete this online form and submit.

---

**Subject:** 2395 County Road B Property / Midland Legacy Estate

---

### Contact Information

---

**Name:** Mary Lou and Dave Wiley

---

**Address:** 2195 St. Croix

---

**City:** Roseville

---

**State:** MN

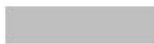
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**Zip:** 55113

---

**How would you prefer to be contacted? Remember to fill in the corresponding contact information.** Email

---

**Phone Number:** 

---

**Email Address:** 

---

**Please Share Your Comment, Question or Concern**

Dear Roseville Planning Commission,

We are writing in response to the revised plan for Midland Legacy Estate. While we appreciate that the new plan calls for fewer homes, it still includes a road along the back of our property. Yes, the plan refers to it as a "private driveway," but how is a strip of pavement that serves as access to several driveways not a road? This plan uses semantics to defy the no thru-lot ordinance passed last year.

Not only that, but with a road running east from Eustis, vehicle

---

headlights will shine directly into the back of our home. This, coupled with traffic noise, will seriously impact our quality of life, as well as that of our neighbors, on a daily basis.

The clear solution is to build the road on the west side of the homes, so that Midland Legacy backyards abut to St. Croix backyards. Isn't that the way most Roseville neighborhoods are designed? Why should ours be excluded from this common consideration?

Of course, we stand with our neighbors in asking for the city to intervene and develop a multi-acre park on that property. But as it seems the city is not inclined to pursue that option, then we ask you to please preserve our neighborhood and our quality of life by denying the plan or insisting that it be revised to place the road within the property and not along our backyard.

Thank you for your time,  
Mary Lou and Dave Wiley

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Unless restricted by law, all correspondence to and from Roseville City government offices, including information submitted through electronic forms such as this one, may be public data subject to the Minnesota Data Practices Act and/or may be disclosed to third parties.

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## Bryan Lloyd

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**From:** Pat Trudgeon  
**Sent:** Wednesday, June 2, 2021 4:07 PM  
**To:** Janice Gundlach; Thomas Paschke; Bryan Lloyd  
**Subject:** FW: 1937 photo of historic Roseville wetland  
**Attachments:** 1937 aerial outlined.JPG; 1937 aerial.JPG; 1937 Wetland closeup.jpg; 1937 Wetland original.jpg; Topographical map of our area 2 original.jpg

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**Patrick Trudgeon | City Manager**  
O: 651.792-7021 | | F: 651.792.7020  
[pat.trudgeon@cityofroseville.com](mailto:pat.trudgeon@cityofroseville.com)



2660 Civic Center Drive | Roseville, MN 55113

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**From:** [REDACTED] >  
**Sent:** Wednesday, June 2, 2021 4:02 PM  
**To:** Julie Kimble [REDACTED]; Michelle Kruzel [REDACTED]; Tammy McGehee [REDACTED]; Erik Bjorum [REDACTED]; Michelle Pribyl [REDACTED]; Karen Schaffenhauen [REDACTED]; Erik Bjorum [REDACTED]; Emily Leutgeb [REDACTED]  
**Cc:** [REDACTED]; Dan Roe [REDACTED]; Wayne Groff [REDACTED]; Jason Etten [REDACTED]; Pat Trudgeon <Pat.Trudgeon@cityofroseville.com>  
**Subject:** 1937 photo of historic Roseville wetland

**Caution:** This email originated outside our organization; please use caution.

To the Roseville Planning Commission:

Please view the 1937 aerial photos of Roseville which includes 2395 West County Road B and adjoining properties.

In public meetings the developer has characterized the wetland at 2395 West County B as "a storm runoff pond".

This wetland no doubt serves the city as a stormwater holding pond, but the wetland is an historic wetland that was here for hundreds or thousands of years before Europeans came to the continent. The proof of that is in the 1937 aerial photo which we obtained from an environmental consultant who has access to these photos.

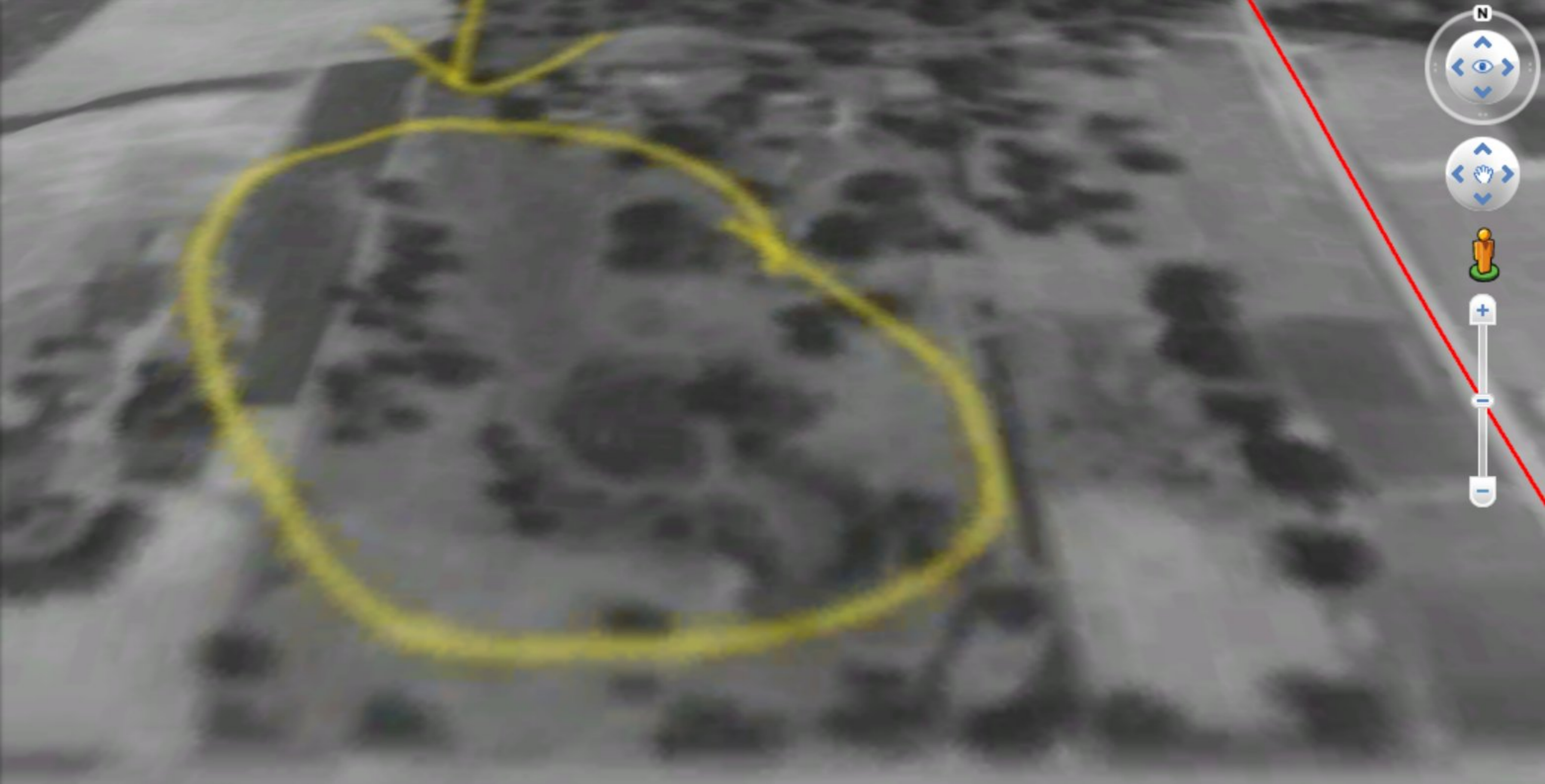
Because wetland delineation within must be part of your decision process, the 1937 aerial photo must also be part of the record of our city, with details from residents, experts, and officials showing why the developer's proposal must not be allowed to negatively impact this part of our landscape environment.

Thank you.

Paul Nockleby  
2171 St. Croix Street  
Roseville, MN 55113





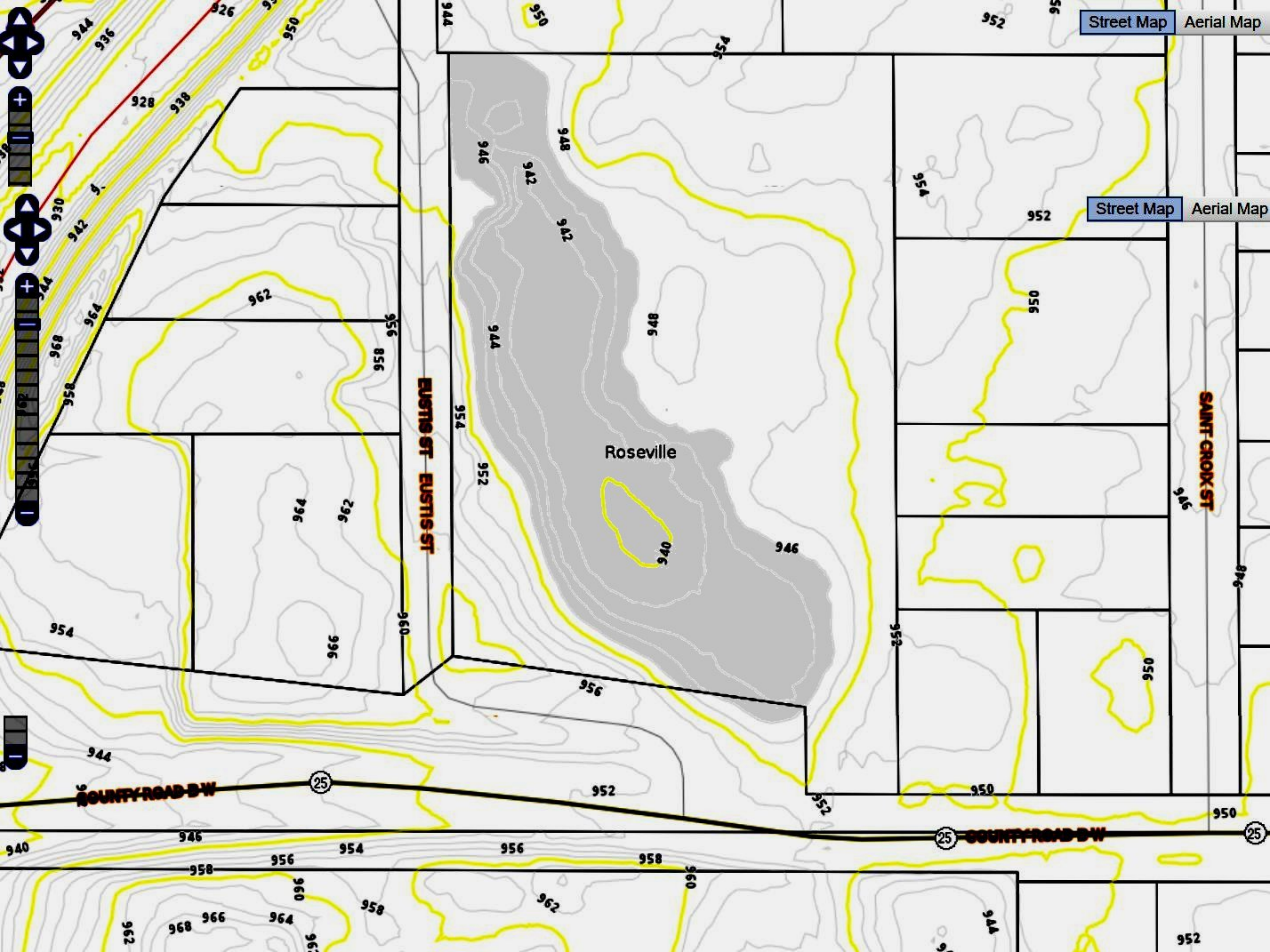




7-1-37

WN-2-153





Street Map Aerial Map

Street Map Aerial Map

EUSTIS ST

SAINT CROIX ST

COUNTRY ROAD SW

COUNTRY ROAD SW

Roseville

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