Commissioners:

Julie Kimble
Michelle Kruzel
Tammy
McGehee
Michelle Pribyl
Karen
Schaffhausen
Erik Bjorum
Emily Leutgeb



Planning Commission Agenda

Wednesday, November 3, 2021 6:30pm

Members of the public who wish to speak during public comment or on an agenda item may do so in person during this meeting or virtually by registering at www.cityofroseville.com/attendmeeting.

Address:

2660 Civic Center Dr. Roseville, MN 55113

Phone:

651-792-7080

Website:

www.cityofroseville.com/pc

- 1. Call To Order
- 2. Roll Call
- 3. Approval Of Agenda
- 4. Review Of Minutes

Documents:

SEPTEMBER 1, 2021 MINUTES.PDF

- 5. Communications And Recognitions
- 5.A. From The Public:

Public comment pertaining to general land use issues not on this agenda.

5.B. From The Commission Or Staff:

Information about assorted business not already on this agenda.

- 6. Public Hearing
- 6.A. Consider A Request By Jerry's Enterprises And Holiday Stores For A Conditional Use To Allow A Drive-Through Car Wash At 1201 Larpenteur Avenue (PF21-015)

Documents:

6A REPORT AND ATTACHMENTS.PDF

6.B. Consider A Request By Crown Castle And LAMN, LLC For A Conditional Use To Allow A Modification Of Telecommunication Devices On A Pre-Existing Non-Conforming Lattice Tower At 2420 County Road C (PF21-016)

Documents:

6B REPORT AND ATTACHMENTS.PDF

- 7. Other Business
- 7.A. Consider 2022 Variance Board And Planning Commission Meeting Calendar

Documents:

7A REPORT.PDF

8. Adjourn



Planning Commission Regular Meeting City Council Chambers, 2660 Civic Center Drive Draft Minutes – Wednesday, September 1, 2021 – 6:30 p.m.

1 2 3	1.	1. Call to Order Chair Kimble called to order the regular meeting of the Planning Commission is approximately 6:30 p.m. and reviewed the role and purpose of the Planning Co			
4 5 6	2.	Roll Call At the request of Cha	air Kimble, City Planner Thomas Paschke called the Roll.		
7 8 9		Members Present:	Chair Kimble, and Commissioners Michelle Kruzel, Tammy McGehee, Erik Bjorum and Emily Leutgeb.		
10 11 12		Members Absent:	Vice Chair Michelle Pribyl, and Commissioner Karen Schaffhausen		
13 14 15		Staff Present:	City Planner Thomas Paschke, Senior Planner Bryan Lloyd, and Community Development Director Janice Gundlach		
16 17	3.	Approve Agenda			
18	٥.	ripprove rigenua			
19		The agenda, as pres	sented, was approved by unanimous consent.		
20		<i>3</i> , 1	, , , , , , , , , , , , , , , , , , , ,		
21	4.	Review of Minutes			
22					
23		a. August 4, 2021 l	Planning Commission Regular Meeting		
24					
25			ted Commissioner McGehee provided some changes to the meeting		
26		minutes which w	ere for clarification and would be updated in the minutes.		
27		O1 ' 77' 11 ' '			
28			licated she had a brief conversation with Ms. Gundlach that it seems		
29			mmarizing a little too much and there are some things that are		
30			minutes and the way in which the conclusions are being reached are		
31		lacking some det	ans.		
32		Chair Kimble no	ted on line 290 the sentence should be " Member McGehee <u>Ms.</u>		
33 34			ed the reason for the Shoreland Ordinance."		
35		dundiaen recapp	ed the reason for the Shoreland Ordinance.		
36		Chair Kimble ext	plained on line 662, "Chair Kimble explained she was a part of this		
37		-	ve Plan process for the entire time"		
38		<u> </u>	101 vite envire vinterin		
39		MOTION			
40			nee moved, seconded by Member Kruzel, to approve the August		
41		4, 2021 meeting			

 Ayes: 5 Nays: 0

Motion carried.

5. Communications and Recognitions:

a. From the Public: Public comment pertaining to general land use issues <u>not</u> on this agenda, including the 2040 Comprehensive Plan Update.

None.

b. From the Commission or Staff: Information about assorted business not already on this agenda, including a brief update on the 2040 Comprehensive Plan Update process.

Chair Kimble reminded the Commission to complete the Commissioner Demographic Survey.

6. Other Business

a. Preliminary Discussion Regarding Section Two Zoning Code Updates

Community Development Director Gundlach explained the Commission is asked to being discussion regarding Section Two Zoning Code updates. She indicated staff recommends five priority topics, Sustainability, Racial equity and inclusion, Chapter 1017, Conditional Uses, and general housekeeping items. She reviewed the topics with the Commission and indicated this will go forward to the City Council on September 13th for discussion and she was not sure when it would come back to the Commission for further discussion.

Chair Kimble suggested the Commission start with the list in the packet and talk about them as organized with adding in comments as the Commission moves forward.

Member Kruzel indicated she sat on the Partners for Energy Project, and she thought that went to the City Council, or at least part of it did but was not sure how far that was moving along. She wondered if that would fit into this section of sustainability. She indicated there will be programs available.

Ms. Gundlach explained the City Council has adopted their energy action plan and there are lots of items in that plan that the City will be launching, sort of separate, from the Zoning Code update process. There may be opportunities as the City decides what to do on sustainability to reference some of the programs or initiatives that are included in the Energy Action Plan. Sustainability is in the City Council's City Policy Priority Plan and that document was adopted in June. There are some references to other things that will be amended as a part of the City documents related to the energy action plan. She noted this topic is going to be tricky because there are

89 some things that they can do in the Zoning Code but there are many things that they 90 cannot, but it does not mean that the City is not already doing sustainability on a bunch of different other topics. There might be opportunities to build incentives into 91 92 the Zoning Code. 93 Chair Kimble thought that was helpful. She thought it might be great to understand generally, if the Commission is in agreement, if the Commission would order the big 95 topics the same as a starting point. 96 97 Member Kruzel thought that would be a great idea. 99 Member Bjorum asked if items three and four should be switched around. He 100 thought conditional uses are always something that comes up with Zoning Code and 101 issues with larger developments and seems to be a rather touchy topic. 102 103 Member Kruzel agreed that item four should go before item three. 104 105 Member McGehee indicated she would put item three with item one. She thought 106 shoreland was a part of sustainability. She thought it was a part of the Commission's 107 purview and she also thought that it is a part of sustainability and also that Public 108 Works would like the Commission to address. 109 110 Chair Kimble thought that was fair. 111 112 Member Leutgeb concurred that shoreline ordinance falls neatly, conceptually under 113 the umbrella of sustainability, while acknowledging that it is a standalone chapter 114 with the limitation of budget, that it is something to consider, not know the full scope 115 of the budget. 116 117 Member McGehee indicated it is a part of zoning. 118 119 Chair Kimble directed that it made sense that shoreland is part of sustainability and 120 would lump it up there. 121 122 Ms. Gundlach noted that consultant help on Chapter 1017 would be extremely 123 important and valuable so if the Commission uses budget to do that she thought that 124 their resources to update that would be really beneficial. The Conditional Uses might 125 be something staff is much more able to tackle without the expertise of a consultant. 126 127 Mr. Paschke concurred and thought staff could take direction from the Planning 128 Commission to search out different things at it relates to what amendments or 129 additions might be needed within the CU category. 130 131 Chair Kimble asked Mr. Paschke if staff would look at other cities as a part of the 132

research and contrast and compare with cities with similar development.

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Mr. Paschke indicated the research would be more for what the conditional use is for, what it is related to and then search out other municipalities in the area with similar conditional uses and what does that municipality do. He thought staff would be looking to come up with specific conditions for certain things versus the general because the general are just standardized and there are many conditional uses that go through that process. They would be looking for things to try to gain some idea as to how different municipalities regulations might apply to their situation or what they are thinking about and then bring them forward for consideration and inclusion in City Code.

Ms. Gundlach thought sometimes it is helpful to just learn by getting some of those Conditional Uses and having to act on them. She thought a lot of the Conditional Uses the City has, the Code has been tweaked multiple times over the years as they review more Conditional Uses and find gaps or holes that they are trying to fix, learning from past mistakes.

1. <u>Sustainability</u> – Zoning requirements and/or incentives for zero-net energy buildings, electric charging stations, etc., landscaping requirements (prioritize use of natives) and tree preservation and/or diversity, screening regulations for solar arrays, revise minimum tree requirements for multi-family housing (current = 1/unit).

Chair Kimble reviewed with the Commission comments from Member Pribyl which includes landscaping and screening, multi-family would be worth discussing reducing the minimum number of trees per unit as long as there still is a sufficient number of trees provided. Solar Arrays make some sense to her. Arrays are on grade. Screening of rooftop arrays could cast shadows on the array on rooftop space as office is often limited. It might be a viable requirement for large industrial buildings, but she does not want to discourage small building owners from adding solar.

Ms. Gundlach indicated the screening for solar arrays, that came from a Council list and was specific to at grade arrays, not rooftop arrays.

Chair Kimble continued explained Member Pribyl had a list of things that could be done for EV charging stations, EV ready, PV/PV array, all electric buildings and incentives for buildings that are zero net energy.

Ms. Gundlach indicated a lot of the comments made by Member Pribyl, she notes sustainability measures that could be incentivized or required. One thing they will need to be aware of is State Statute prohibits the City from adopting any regulation that is more restrictive than the Building Code. They will need to be careful as the process moves along that they are not doing that and that is why staff likes to use the word incentive.

Member McGehee noted on the screening, that originally came up on a specific site. It was up against a residential area, and they put a bunch of arrays up with chain link fence around it so she thought there should be some sort of attractive shield for that. She noticed Council was talking about covering parking areas and she wondered

about and has seen several small or multi-family where they have the covered parking, carport style but the top is actually solar panels. She did not know if that would fit the requirement, they cannot ask anyone to do that, or could this be something the City could ask of the project.

Ms. Gundlach indicated her understanding is the City cannot just outright require it but if the project did it there could be an incentive. Maybe an incentive that would help cover some of the increased costs. She indicated what she is envisioning is a type of sustainability worksheet because there are many things that people could do. She wanted to be careful that the City does not create incentives that devalues the property in some other way. If the Commission is interested in the sustainability topic than they could work with the consultant on what that worksheet might be and what the incentives might be in the Code in order to do it.

Member Bjorum thought it was geared toward the actual development of a project but there is something called Green Communities that St. Louis Park uses, and he thought that would be one thing they should definitely look at as a precedent to establish these things because it basically provides that checklist and a scoring chart to put them within a certain threshold.

Ms. Gundlach thought she was thinking the same thing and there is actually a green building code that the State has not adopted but she knew that could be a mechanism they City could use as well.

Chair Kimble indicated she really liked the idea of incentives, and she was sure there were a lot of examples. She thought if the Commission really wanted to advance sustainability and they want to have this incentive worksheet that would make a meaningful difference, she also would not hesitate to, once there is a draft, to get some input from developers because often times what the City thinks is an incentive is not always. She thought that would be a way to catch things that sometimes really are not as incentivizing as people might think.

Ms. Gundlach thought staff working with the consultant can definitely set up a couple of meetings with some developers to get ideas before staff brings something to the Commission.

The Commission liked the incentives and worksheet.

Member Leutgeb thought it was important that the City prioritizes the use of the consultant's expertise while they have it and to that point they knew this was an area of expertise from the consultant so she would retain this as a top priority to tackle with the consultants and in partnership with them.

Member McGehee explained she was struck while reading through the City Code that when talking about green space for multi-family, for nursing homes the City is requiring 150 square feet and when it comes to parking there are significant amount of parking and parking space per person or per unit/bedroom. It seems to her as

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making more sense to her as something that they go with unit or bedroom, particularly when doing multi-family.

Chair Kimble asked if Member McGehee's point was that in senior housing there might not be as much parking required.

Member McGehee explained she was not talking about density. The Commission was talking about sustainability and more green space around multi-family, and she was pointing out that it was not specified in City Code, but it is specified for nursing homes.

Chair Kimble wondered if there could be different tiers such as multi-unit homes from 0-20 requiring one tree, etc. because they do not want to have the ratio such that it works for really large complexes but does not work for a smaller development.

Mr. Paschke suggested not having a tree per unit or that type of method that the City currently has. He thought it was no different than commercial. If the perimeter of the site is taken or square footage of a building and try to manage it that way, it should work out very well where it maximizes the ability to plant trees and get quite a few shrubs on most of those development sites.

Member Bjorum thought when the Commission starts to look at this more in depth, having precedence to look at that shows the extremes on both sides and one that is balanced will be really helpful to understand in how it is figured out.

Ms. Gundlach explained this is work that the consultant will be able to help them with because they have experience in a lot of different communities and can not only show them what other communities are doing but can give them the real live, how it actually has gone implementing and then they can take a Roseville site and show them what the City Code would require, show what possible other options are for Commission consideration that would require to help for visualization.

Chair Kimble thought that would be great because Roseville has had an abundance of senior housing, which is multi-family, but it has only been recently where the City has had a lot of new market rate that is coming on. She thought it would be interesting to see those cities that have had the past couple of years a little more market rate.

Member McGehee indicated while going through the information, the City's fee schedule for violations of the Code on trees is insufficient. There is nothing there where it states if staff goes out and marks trees for preservation and the developer violates what is in the Code, there is no significant financial punishment. She also thought staff should require more than a five-hundred-dollar escrow to reimburse the expense of overseeing the Code regulations.

Ms. Gundlach noted the City Council did look at the Tree Preservation Ordinance and the cash payments that are required with that ordinance were adjusted because they had single family home lots where they were requiring tree preservation payment of \$15,000 because once setbacks are applied there is only one place the house can go and that is where a couple giant trees are. If the City actually said the trees had to be kept than the lot is not buildable and that constitutes a taking so staff did look at what the monetary fees were, and the Council made an adjustment on single family lots. The Tree Preservation Ordinance is all based on total inches and then what can not be replaced a fee of \$500 per tree is imposed to not plant a tree but the Ordinance has a cap of ten percent of the value of the land. She noted the City has received tree preservation payments in excess of \$100,000 and that is unlike anything she has ever seen in any other community.

Member McGehee thought the City had a pretty good code in terms of protection of those trees and marking of trees in which a homeowner or developer or developer has agreed to preserve but something inadvertently goes wrong, and the tree is cut down and she did not think the solution is to charge the person \$100,000 or to plant eight-five trees. She thought there should be something there when staff has made the effort to mark trees and do the count that really makes the developer pay attention to those marked trees.

Chair Kimble asked if there was any way to monitor that process.

Mr. Paschke indicated staff monitors projects all the way through but that is not to say that somebody is not going to accidentally cut down a tree from time to time on a site but that goes into their formula and from time to time a formula has to be redone. He explained there is not any type of fee setup as a penalty for cutting down trees that were intended to preserve.

Ms. Gundlach explained staff would require the developer to redo their tree preservation plan to reflect the tree they said they were going to save that they did not save and that redoes their calculations.

Mr. Paschke clarified that the tree preservation plan is setup for development, predevelopment until things are done but post development, a homeowner can cut down a tree they do not like without penalties.

Member McGehee indicated she was not talking about trees in general, she was talking about heritage trees which the City does not have many of so to lose a heritage tree by whatever means ought to require something more than planting ordinary trees.

Chair Kimble thought there was consensus around looking at how the multi-family tree preservation is calculated. It sounded like there was consensus around some screening for solar arrays and it seems to her to be a no brainer on electric charging stations. The Commission agreed that it would be great to have some kind of incentive worksheet. There is also the whole issue of shoreland, wetland and storm water management, which she thought everyone agreed it needed to be looked at and that it is best looked at by the Consultants along with staff.

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Member McGehee thought native plantings, particularly with the drought is one thing that has come up.

Ms. Gundlach explained that was one item that came out in one of their engagement sessions as a member of the community really wanted to push natives and the City's landscaping code does not specifically say natives but landscape architects who submit plans generally do not propose plans that are not suitable for this area. Staff thinks there might be opportunities in the City landscaping ordinances to be more specific about what they are asking for as it relates to natives.

Ms. Paschke agreed and was not sure what Member McGehee was referring to or what anybody refers to when native is said. He asked if that meant the wildflowers in the grasses that grew in the prairies that were here before the City developed as an urban community or would it be specific types of trees because over the years trees have been grown by various entities to be tolerant in this type of environment. He asked what would be native to them or other who have spoken about native.

Member McGehee thought some of it is prairie materials that according to climate change are going to be moving into this area. When thinking about landscaping, to reduce the use of sprinklers and watering and a lot of those products are through Prairie Restore as well as other nurseries that carry a lot of native plants. In terms of trees, she did not know if she would use the word native there particularly, more she would use diversity, which she thought is what they are already talking about, so they do not have whole neighborhoods clear cut when they get the next tree plague.

Mr. Paschke thought the Code does speak to plantings that require less water and things, so it supports that. He noted the City is promoting native and prairie plantings in the landscaping plans for less watering.

2. Racial Equity and Inclusion — Definition of family/household/housekeeping unit, lot area, lot width, housing types, Subdivision Code barriers, expand LMDR (formerly LDR-2) areas.

Ms. Gundlach indicated if the Commission had any input on this topic that they felt was important to bring up then to do that, but she explained that some of the changes that are in Section One really speak to this topic and those are fairly significant items that may or may not get approved. The Commission might not want to spend a lot of time right now on this topic until the Section One updates are done.

Member Leutgeb wondered about some things around the definitions and really reviewing the Code or coded language, whether that lies within staff expertise as well or would that be an additional expertise that would be more beneficial for the consultants to review.

Ms. Gundlach indicated staff and the consultants debated a lot about whether or not the housing type changes that are in Section One should be talked about as Section One or Section Two. Staff ultimately decided to propose it to the Commission in Section One because staff felt it aligned with a lot of the general requirements about density and use knowing it could come out. She thought the point about protecting property values, that is certainly a concern for people, and it shows up in the City's Conditional Use criteria as well and a lot of that is very ambiguous and a lot of times that is used for reasons to oppose things without any sort of valid science behind it. She was not sure that they need to consultant to tackle some of that if what is in Section One actually passes.

Chair Kimble thought that was another reason for this topic to be shelved for the time being because the Commission will have a chance to discuss this in the future.

Member McGehee explained she wanted to talk about equity and whether they bring it forward or not because she did not see affordable housing addresses equity at all, and the City has built a lot of affordable housing that certainly provides a lot of equity for the developer, but it does not provide any equity for the people that are using it and equity is what a lot of people are looking for. She noted when a property is foreclosed on the City has the opportunity to purchase that property and she thought the City should consider whether they use one of the outside operations, like Rondo to do kind of a land trust so that the City does not let those houses slip away from them and allow people to come in and build equity. She preferred the Land Trust Model to the Habitat for Humanity model because it brings a person in, gives them an opportunity to live there and then when they get on their feet they get the equity that has accrued during the time that they have lived there plus if they have approved improvements made to the home the person gets that as well so the person leaves with a little cushion to buy their own home and the home then returns to another family who would like to do the same things.

Chair Kimble asked if that was a Planning Commission issue or a City Council issue.

Ms. Gundlach explained she did not disagree with anything Member McGehee stated. All of those things are really important. She did not see the role in Zoning for those items but there is a role with the EDA, and they are talking about those issues right now. She indicated on September 20th the EDA is going to receive information on inclusionary housing and mixed income housing policies which basically require a certain amount of affordable housing with all housing development, and they are going to receive information about a Land Trust program. Staff thinks that is very important. The City's Policy Priority Plan on housing speaks specifically to doing Land Trust deals. Homeownership affordable deals moving forward. She was not sure that there was a role for the Planning Commission, but the Council has already set this as a priority and staff already agrees this is priority and the wheels are already in motion on these things.

Mr. Lloyd explained that while equity in a home certainly is a racial equity issue, it sounded like there was a little bit of conflation of those two ideas. The racial equity that the Zoning Code can help to advance, with eliminating the sort of subtle or in some cases accidental or coded ways that people of color have been kept out of communities, like Roseville, historically, and even though they appear to be race

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neutral provisions, their affects over time and their intentions early on, in particular have not been race neutral and addressing things like that which are in the Zoning Code is quite a bit wider than equity in housing stock but these are the kinds of things that the Zoning Code can start to address.

3. Chapter 1017 – Shoreland, wetland, and storm water management regulations.

Chair Kimble explained Member Pribyl thought it would be good to understand what the differences are between the City and watershed requirements, what is the impact of an alignment, could there be incentives that might go beyond minimum watershed and thought it would be good to look at the Shoreland Ordinance relative to the Minnesota DNR Model Ordinance.

<u>4. Conditional Uses</u> – Create criteria for newly established CUs for density, building height, ensure general CU criteria are still valid/appropriate.

Chair Kimble asked about the idea of parking standards and having the minimum number of covered parking for multi-family and if that was something staff thought was important and should added.

Ms. Gundlach indicated staff could add that because covered parking for multi-family is required in many communities, not in Roseville but could be added for review.

<u>5. General Housekeeping Items</u> – Revise definitions of fence, bed, and breakfast, lodging assisted living and/or memory care, address institutional housing types (convent, rectory, housing affiliated with religious institutions).

Chair Kimble thought this was a really good discussion and there was consensus of the Commission.

7. Adjourn

MOTION

Member Kruzel, seconded by Member McGehee, to adjourn the meeting at 7:51 p.m.

Ayes: 5 Nays: 0

Motion carried.

RESEVILLE REQUEST FOR PLANNING COMMISSION ACTION

Agenda Date: 11/03/21 Agenda Item: 6a

Prepared By Agenda Section

Public Hearings

Department Approval

Janue Gundrach

Item Description: Consider a Request by Jerry's Enterprises and Holiday Stores for a

Conditional Use to allow a drive-through car wash at 1201 Larpenteur

Avenue (PF21-015)

1 APPLICATION INFORMATION

2 Applicant: Jerry's Enterprises

3 Location: 1201 Larpenteur Avenue

4 Application Submission: 10/01/21; deemed complete 10/26/21

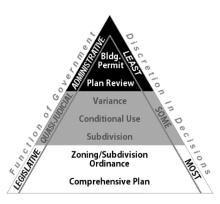
5 City Action Deadline: December 14, 2021

6 Planning File History: PF08-005 – PUD

PUD Cancelled – Oct 25, 2021

8 Zoning: Community Business District

- 9 LEVEL OF DISCRETION IN DECISION MAKING: Action taken on a
- conditional use proposal is quasi-judicial; the City's role is to
- determine the facts associated with the request, and apply those facts
- to the legal standards contained in State Statute and City Code.
- 13 BACKGROUND
- On October 25, 2021, the City Council approved cancelling a 2008
- planned unit development governing the property, which was
- created for the redevelopment of the former Rainbow Foods site
- 17 (now Cub). The decision to cancel the PUD was outlined in
- Ordinance, which is provided as Attachment E. Essentially, the
- 19 PUD was outdated by referring to zoning districts that no longer exist, limiting the site to uses
- 20 the property owner has been unable to realize in over 13.5 years, and requiring development of
- 21 the site in a manner that no longer complies with certain development standards outlined within
- 22 the Zoning Code and Comprehensive Plan. The cancelling of the PUD affords the property
- owner and Jerry's Enterprises greater flexibility to utilize the Community Business zoning
- 24 district standards to redevelop/develop the land area adjacent to Larpenteur Avenue. The
- 25 Community Business zoning district (to become MU-3) outlines development regulations that
- align with guidance outlined within the Comprehensive Plan.



- 27 Jerry's Enterprises owns Cub, as well as manages and leases the existing farmer's market area
- 28 (canopy structure located in the southwest corner of the site). They desire to redevelop this
- 29 portion of the site adjacent to Larpenteur Avenue with a Holiday Gas Station and car wash. The
- 30 convenience store and fuel sales are permitted uses, while the car wash is deemed a drive-
- 31 through and thus requires an approved conditional use that complies with City Code
- requirements, including all applicable general design standards of §1005.02.A and specific
- 33 design standards of §1005.06.
- Planning Division staff have attached a number of development documents, mostly for reference
- purposes (Attachment C). The site plan is germane to the drive-through/conditional use
- discussion as it details access, vehicle site circulation, and drive-through stacking. This report,
- and the associated documents, only reviews the conditional use for the drive-through and
- 38 otherwise assumes the project can or will comply with the required City and Zoning Code
- 39 standards. The CU does not apply to any other aspects of the proposed redevelopment project,
- 40 including the proposed use of a convenience store and fuel sales, which are governed by other
- sections of the Zoning Code. At the time of building permit, a comprehensive review of
- setbacks, parking, exterior building materials, landscaping, building height, etc. would occur
- ensuring the approved plans meet all applicable criteria.
- As a component of the proposed redevelopment with a drive-through facility, a traffic study
- 45 (Attachment D) was conducted, as required by Public Works Department policy, to review
- existing operations, evaluate potential traffic impacts of the proposed redevelopment, review site
- 47 access considerations, and recommend improvements to ensure safe and efficient operations.
- 48 The full traffic study is provided as Attachment D.
- 49 Zoning Code Sections §1009.02.C and §1009.02.D.12 establish general standards and criteria
- applying to all conditional uses and specific standards and criteria applying to drive-through
- facilities. The Planning Division's review of these criteria can be found below in the Conditional
- 52 Use Analysis section.

53 CONDITIONAL USE ANALYSIS

- REVIEW OF GENERAL CONDITIONAL USE CRITERIA: §1009.02.C of the Zoning Code establishes
- 55 general standards and criteria for all conditional uses. When making a decision on whether to
- approve or deny a conditional use, the Planning Commission and City Council must review the
- 57 proposal and determine if compliance can be achieved with the stated findings.
- The general code standards of §1009.02.C are as follows:
- 59 **a.** The proposed use is not in conflict with the Comprehensive Plan. While a drive-through facility doesn't appreciably advance the goals of the Comprehensive Plan aside from
- facilitating continued investment in a property, Planning Division staff believes it does not conflict with the Comprehensive Plan either. More specifically, the General and Commercial
- Area Goals and Policies sections of the Comprehensive Plan include a number of policies
- related to reinvestment, redevelopment, quality development, and scale. The proposed drive-
- through is one component of a larger investment, which would align with the related goals
- and polices of the Comprehensive Plan.
- 67 **b.** *The proposed use is not in conflict with a Regulating Map or other adopted plan.* The subject property is not located in an area that is controlled by a regulating plan or other adopted plan,
- so this standard is not applicable.

c. The proposed use is not in conflict with any City Code requirements. The project addresses all applicable requirements of the City Code as they pertain to the proposed drive-through CU. Moreover, a CU approval can be rescinded if the approved use fails to comply with all applicable Code requirements or any conditions of the approval. As part of the building permit review process, Planning Division staff will conduct a more comprehensive Code compliance analysis, including zoning standards such as landscaping, trash/recycling enclosures, vehicle parking, materials, etc.

- d. The proposed use will not create an excessive burden on parks, streets, and other public facilities. Staff does not anticipate the proposal to intensify any practical impacts on parks, streets, or public infrastructure. A traffic study (Attachment D) was completed for the car wash drive-through indicating no significant increase in traffic or impacts to surrounding intersections. Specifically, the results of the existing intersection capacity analysis indicate that all study intersections (Larpenteur/Fernwood, Larpenteur/Dunlap, Larpenteur/driveway access, and Fernwood/driveway access) currently operate at an acceptable overall LOS B or better during the weekday a.m. and p.m. peak hours with the existing traffic control, geometric layout, and signal timing. Southbound queues at the Larpenteur Avenue and Fernwood Street intersection are expected to reach the driveway access less than five (5) percent of the p.m. peak hour. No other queuing issues were identified and the traffic consultant indicates the proposed redevelopment will improve overall safety in the area. The City Engineer has reviewed and accepted the traffic study and has not offered any additional comments relative to the proposed drive-through.
- e. The proposed use will not be injurious to the surrounding neighborhood, will not negatively impact traffic or property values, and will not otherwise harm the public health, safety, and general welfare. Generally, the proposed drive-through, gas pump area, and convenience store will not be injurious to the surrounding neighborhood; negatively impact traffic or property values; and will not otherwise harm the public health, safety, and general welfare. Planning Division staff anticipates the proposed redevelopment and drive-through will slightly increase vehicle trips on the adjacent roadways, however, not significantly or for extended periods of time, or in a manner that is unmanageable under the proposed site access, drive-through, and circulation plan. It's also worth noting the previously approved PUD allowed for a 1,800 SF fast food restaurant, that if developed, likely would have had a similar, if not greater impact on traffic as compared to the proposed convenience store with fuel sales and car wash.

REVIEW OF SPECIFIC CONDITIONAL USE CRITERIA: §1009.02.D.12 of the Zoning Code establishes additional standards and criteria that are specific to drive-through facilities:

a. Drive-through lanes and service windows shall be located to the side or rear of buildings and shall not be located between the principal structure and a public street except when the parcel and/or structure lies adjacent to more than one public street and the placement is approved by the Community Development Department (Ord. 1443, 6-17-2013). The basis for this criterion is to limit vehicular impacts on adjacent roadways and the pedestrian realm. The proposal by Jerry's complies with this standard. The drive-through car wash does not have a service window, but rather a payment menu board, which will limit negative impacts that often result from food drive-through facilities that have loud speaker boxes. The car wash is oriented north-south in the southwest corner of the site, between Larpenteur Avenue and Cub. While the north-south orientation of the car wash drive-through lane places a drive

- aisle between Larpenteur Avenue and the car wash building, staff finds this criterion to be met because the site lies on a corner lot and the drive-through lane is incorporated into the broader site access, parking lot, and gas fueling design of the site.
- b. Points of vehicular ingress and egress shall be located at least 60 feet from the street right-of-way lines of the nearest intersection. In review of this requirement, the nearest street intersection would be Fernwood and Larpenteur, which is approximately 136 feet from the existing site access to Cub and the out-parcel area. As such, this criterion is met.
- c. The applicant shall submit a circulation plan that demonstrates that the use will not interfere 122 with or reduce the safety of pedestrian and bicyclist movements. Site design shall 123 accommodate a logical and safe vehicle and pedestrian circulation pattern. Queuing lane 124 space shall be provided, sufficient to accommodate demand, without interfering with primary 125 driving, entrance, exit, pedestrian walkways, or parking facilities on site. The circulation 126 plan shall be made a condition of approval and shall survive any and all users of the drive-127 through and may need to be amended from time to time to ensure continued compliance with 128 129 this condition. Said amendments to the circulation plan will require an amendment to the conditional use. The vehicular and pedestrian circulation plan identifies the site traffic flow 130 with arrows and signs to address pedestrian safety. Generally, the drive-through lane is fully 131 separated from pedestrian paths between the fuel pumps and convenience store. While 132 unrelated to the drive-through area, the northeast portion of the site includes a marked 133 pathway crossing the two-way drive lane. Planning Division and Engineering staff reviewed 134 135 the plans and recommended minor modifications to better facilitate traffic flow and pedestrian safety, which have been incorporated and are deemed acceptable. These 136 modifications included signage and pavement markings to delineate the pedestrian route 137 across the drive-thru queue, promoting better driver awareness. Lastly, the Traffic Study 138 indicates the drive-through lane provides adequate queuing sufficient to accommodate 139 expected demand. 140
- **d.** Speaker box sounds from the drive-through lane shall not be loud enough to constitute a 141 nuisance on an abutting residentially zoned property or property in residential use. 142 Notwithstanding this requirement, such speaker boxes shall not be located less than 100 feet 143 from an existing residentially zoned property or property in residential use. The proposal for 144 the drive-through car wash does not have a service window, but rather a payment menu board 145 that lacks the typical speaker box found on menu boards of fast food drive-through facilities. 146 The nearest residential property is the apartment building across Largenteur Avenue in the 147 City of Saint Paul, which lies approximately 190 feet from the payment menu board. Given 148 traffic levels and associated noise on Larpenteur Avenue, Planning Division staff doesn't 149 150 anticipate the menu board will have any audible sound that could be heard at any residential use in the vicinity. 151
- e. Drive-through canopies and other structures, where present, shall be constructed from the same materials as the primary building and with a similar level of architectural quality and detailing. The proposed car wash drive-through does not include a canopy, so this requirement has been deemed not applicable.

- **f.** A 10-foot buffer area with screen planting and/or an opaque wall or fence between 6 and 8 156 feet in height shall be required between the drive-through lane and any property line 157 adjoining a public street or residentially zoned property or property in residential use and 158 approved by the Community Development Department (Ord. 1443, 6-17-2013). The 159 160 proposed drive-through does not lie adjacent to residentially zoned/used property or between the principle building and the public street. Rose Vista apartments are located across 161 Fernwood and slightly north of the proposed redevelopment area, but the nearest building is 162 over 250 feet from the proposed car wash building. Therefore, staff finds this criterion does 163 not apply. 164
 - PLANNING DIVISION RECOMMENDATION
- The Planning Division recommends approval of the CU to allow Jerry's Enterprises a drive-
- through car wash 1201 Larpenteur Avenue, based on the submitted site and development plans,
- subject to the following conditions:

165

- 169 **a.** The site, building, landscaping, and drive-through lane shall be constructed substantially consistent with the plans submitted October 1, 2021 and provided as a component of the report packet, unless otherwise revised to comply with Zoning Code standards.
- 172 SUGGESTED PLANNING COMMISSION ACTION
- 173 By motion, recommend approval of a CONDITIONAL USE for 1201 Larpenteur Avenue,
- allowing a drive-through on the subject property based on the comments, findings, and two
- conditions stated in this report.
- 176 ALTERNATIVE ACTIONS
- a. Pass a motion to table the item for future action. An action to table must be tied to the need
 for clarity, analysis, and/or information necessary to make a recommendation on the request.
- b. Pass a motion recommending denial of the proposal. A motion to deny must include findings of fact germane to the request.

Report prepared by: Thomas Paschke, City Planner, 651-792-7074 | thomas.paschke@cityofroseville.com

Attachments: A. Location Map

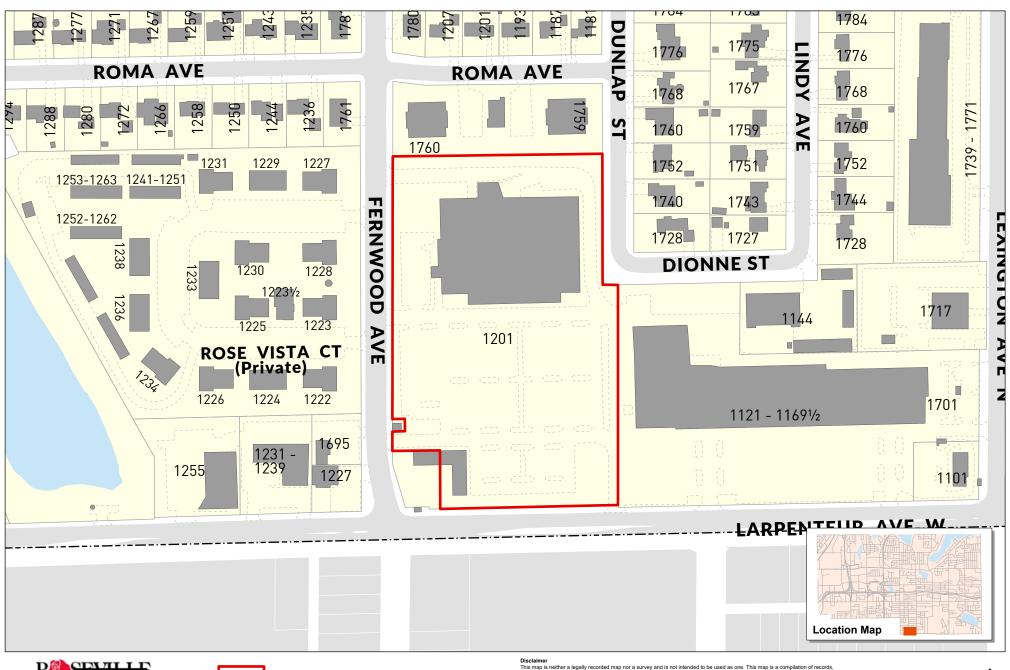
B. Aerial photo

C. Narrative/plans

D. Traffic study

E. PUD Cancellation Ordinance

Attachment A: Planning File 21-015







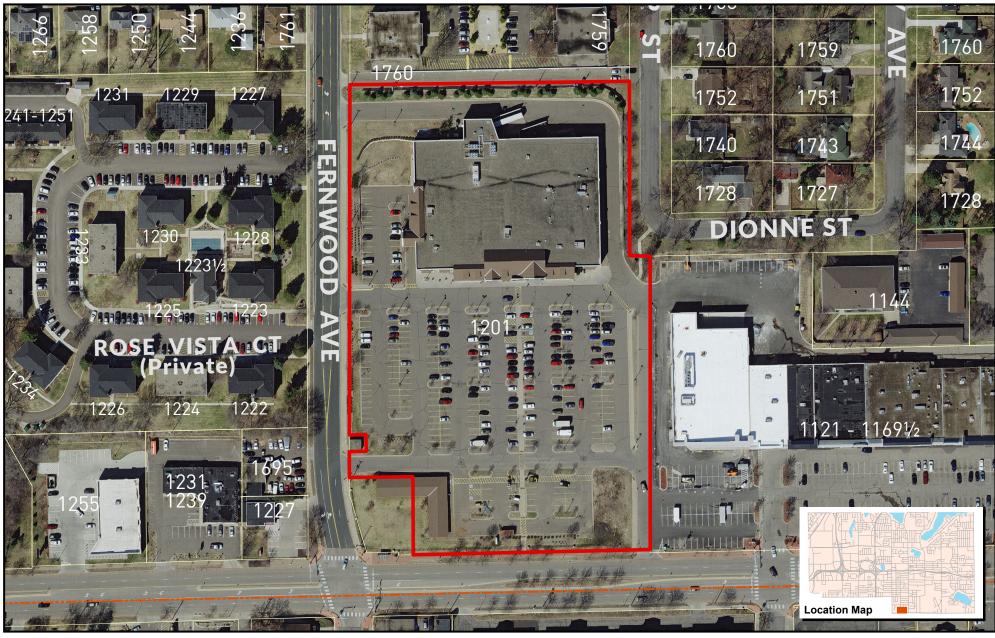
Data Sources

* Ramsey County GIS Base Map (6/30/2021)
For further information regarding the contents of this map contact:
City of Roseville, Community Development Department,
2660 Civic Center Drive, Roseville MN

Disclaimer
This map is a compilation of records.
This map is a compilation of records and the compilation of records and other sources regarding the areas alone, and is to be used for reference purposes easy. The City does not verant that the Geographic Information System (GIS) Data used to prepare this map are error free, and the City does not represent that the GIS Data can be used for navigational tracking or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features: If errors or descrepancies are found please contact 651-792-7085. The preceding disclaimer is provided pursuant to Minnesota Statutes §466.03, Subd. 21 (2000), and the user of this map acknowledges that the City from any and all claims brought by User, its employees or agents, or third parties which arise out of the user's access or use of data provided.



Attachment B: Planning File 21-015





Prepared by: Community Development Department Printed: October 27, 2021

Data Sources

* Ramsey County GIS Base Map (9/30/2021)

* Aerial Data: Surdex (4/2020)

For further information regarding the contents of this map contact: City of Roseville, Community Development Department, 2660 Civic Center Drive, Roseville MN

Disclaimer

This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only. The City does not warrant that the Geographic Information System (ISI) Data used to prepare this map are error free, and the City does not represent that the GIS Data can be used for navigational, tracking or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. If errors or discrepancies are found please contact 61:792-7085. The preceding disclaimer is provided pursuant to Minnesota Statutes §466.03, Subd. 21 (2001), and the user of his map acknowledges that the City shall not be laidle for any damages, and expressly wweeks all claims, and agrees to define the control of the contr







CUP Application Project Narrative for the Holiday Roseville

Location: 1215 Larpenteur Avenue West

Roseville, MN

Applicant: Jerry's Enterprises **Architect: Design Consortium** Civil Engineer: Sambatek, Inc.

Proposal: Jerry's Enterprises is proposing to utilize existing, unused parking area on the site of the Cub

Foods on Larpenteur Avenue. The project includes removal of an existing pavilion and

development of a Holiday gas station and associated car wash.

The property is currently within the CB: Community Business zoning district with a PUD: Planned Unit Development district overlay. The applicant is undergoing the process of PUD

cancellation which will remove the PUD overlay.

The current use is grocery store. This is a permitted use in the CB district.

The proposed use will add motor fuel sales (gas station) and a drive-through facility (car wash). The gas station is a permitted use in the CB district, while the car wash is a conditional use.

The request is for a Conditional Use Permit for the car wash.

Site Access: The project proposes to maintain the existing access points that currently serve the site. There

is one access along Fernwood Street and one along Larpenteur Avenue.

Landscaping & Screening:

Landscape requirements will be met. A variety of coniferous, deciduous, and ornamental trees will be provided throughout the site. Trees will be a hardy mix of native of non-native species.

Lighting: Lighting will be LED on poles with concrete bases. Lighting will be in conformance with City

Code and located/shielded to avoid overlap to adjacent properties.

Signage: One monument sign is being requested at the southeast corner of the site and one pylon sign is

being requested at the southwest corner of the site as indicated in the plans.

CUP Review Criteria:

The proposed use is not in conflict with the Comprehensive Plan.

The property is designated as corridor mixed use in the 2040 Comprehensive Plan.

The proposed use is not in conflict with any Regulating Maps or other adopted plans.

The car wash is currently in conflict with the PUD overlay, as a car wash is not allowed, however, when the cancellation of the PUD occurs, there will be no conflict.

The proposed use is not in conflict with any City Code requirements.

The car wash will meet all applicable City Code requirements.

The proposed use will not create an excessive burden on parks, streets and other public facilities.

There are existing utilities in the area which are adequate to serve the proposed development. Sanitary sewer, storm sewer, water, electric, and gas are existing and available in the perimeter streets of Larpenteur Avenue and Fernwood Street, or within the site already. The proposed development will connect to these utilities.

The proposed use will not be injurious to the surrounding neighborhood, will not negatively impact traffic or property values, and will not otherwise harm the public health, safety, and general welfare.

Anticipated traffic generated by the development will not cause the capacity of adjacent streets to be exceeded. Additionally, the development has been designed such that vehicular parking and pedestrian circulation does not create hazards to safety.

We respectfully request City support for the enclosed application by Jerry's Enterprises for Conditional Use Permit for the proposed Holiday Roseville project. We look forward to reviewing this application with the City in the weeks ahead.

Sincerely,

Brian Frank, PE Sambatek, Inc.

1. ALL DIMENSIONS ARE ROUNDED TO THE NEAREST TENTH FOOT

- 2. ALL DIMENSIONS SHOWN ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
- 3. CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "GUTTER OUT" WHERE WATER DRAINS AWAY FROM CURB. ALL OTHER AREAS SHALL BE CONSTRUCTED AS "GUTTER IN" CURB. COORDINATE WITH GRADING CONTRACTOR.

DEVELOPMENT NOTES

- 4. ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
- 5. ALL PARKING STALLS TO BE 9' IN WIDTH AND 18' IN LENGTH UNLESS OTHERWISE INDICATED.
- 6. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF EXIT PORCHES, RAMPS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- 7. SEE ARCHITECTURAL PLANS FOR PYLON AND MONUMENT SIGN DETAILS
- 8. LIGHT STANDARD LOCATIONS ARE FOR REFERENCE ONLY, SITE LIGHTING PLAN IS DESIGN BUILD BY CONTRACTOR. CONTRACTOR SHALL CONFIRM LIGHT STANDARD LOCATION WITH LIGHTING VENDOR.
- 9. REFER TO FINAL PLAT FOR LOT BOUNDARIES, LOT NUMBERS, LOT AREAS, AND LOT DIMENSIONS.
- 10. ALL GRADIENTS ON SIDEWALKS ALONG THE ADA ROUTE HAVE BEEN DESIGNED WITH A MAXIMUM LONGITUDINAL SLOPE OF 4.5%, AND A MAXIMUM CROSS SLOPE OF 1.5%. THIS IS LESS THAN THE ADA CODE MAXIMUM LONGITUDINAL SLOPE OF 5% (1:20), EXCEPT AT CURB RAMPS (1:12), AND A MAXIMUM CROSS SLOPE OF 2.08% (1:48). THE MAXIMUM DESIGN SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS AISLE IS 1.5%, LESS THAN THE ADA CODE MAXIMUM SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS AISLE OF 2.08% (1:48). THE CONTRACTOR SHALL REVIEW AND VERIFY THE GRADIENT IN THE FIELD ALONG THE ADA ROUTES PRIOR TO PLACING CONCRETE OR BITUMINOUS PAVEMENT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE IS A DISCREPANCY BETWEEN THE GRADIENT IN THE FIELD VERSUS THE DESIGN GRADIENT AND COORDINATE WITH GRADING CONTRACTOR.
- 11. "NO PARKING" SIGNS SHALL BE PLACED ALONG ALL DRIVEWAYS AS REQUIRED BY CITY.

CIVIL 3D MODEL LIMITATIONS

SAMBATEK'S DELIVERABLE AND GOVERNING DOCUMENTS FOR CONSTRUCTION SHALL BE A HARD COPY AND/OR PDF PLAN SHEETS. IF A CIVIL 3D MODEL IS GENERATED IN THE PROCESS OF PREPARING THE PLAN SHEETS, IT IS AS A DESIGN TOOL ONLY AND NOT AS A SEPARATE DELIVERABLE. AT THE OWNER'S REQUEST, WE WILL RELEASE OUR CIVIL 3D MODEL FOR THE CONTRACTOR'S USE. HOWEVER, ITS USE IS AT THE CONTRACTOR'S RISK AND SHALL NOT BE USED FOR STAKING OF CURB, SIDEWALK, OR OTHER HARD SURFACE IMPROVEMENTS. IF A CIVIL 3D MODEL FOR STAKING HARD SURFACE IMPROVEMENTS IS REQUIRED, WE CAN PROVIDE A SUPPLEMENTAL AGREEMENT FOR REFINEMENT AND PREPARATION OF THE CIVIL 3D MODEL.

A. BUILDING, STOOPS, STAIRS (SEE ARCHITECTURAL PLANS)

- B. B-612 CONCRETE CURB AND GUTTER (SEE DETAIL 2 ON SHEET C9.02)
- C. PYLON SIGN, 25' HEIGHT

EXECUTE: KEY NOTES

- D. CONCRETE APRON (SEE DETAIL P2 ON SHEET C9.01)
- E. FLAT CURB SECTION (SEE DETAIL 7 ON SHEET C9.03)
- F. CONCRETE SIDEWALK (SEE DETAIL 6 ON SHEET C9.03)
- G. SEGMENTAL BLOCK RETAINING WALL
- H. ACCESSIBLE RAMP
- I. ACCESSIBLE STALL STRIPING (SEE DETAIL 5 ON SHEET C9.03)
- J. ACCESSIBLE PARKING SIGN (SEE DETAIL 4 ON SHEET C9.03)
- K. TRANSFORMER
- L. MONUMENT SIGN
- M. FILTRATION BASIN (SEE GRADING AND UTILITY SHEETS)
- N. TRASH ENCLOSURE
- O. FUELING STATION CANOPY
- P. FLAG POLE

AREA (WITHIN PROJECT LIMITS)
GROSS SITE AREA 67,839 SF 1.56 AC STORM SEWER EASEMENT 12,221 SF 0.28 AC **NET SITE AREA** 55,618 SF 1.28 AC IMPERVIOUS BREAKDOWN (WITHIN PROJECT LIMITS) EXISTING IMPERVIOUS 36,609 SF 0.84 AC PROPOSED IMPERVIOUS 43,948 SF 1.00 AC 7,339 SF 0.16 AC **NET NEW IMPERVIOUS BUILDING SETBACKS** FRONT YARD 0-25 FEET REAR YARD 10 FEET SIDE YARD 0 FEET PARKING SETBACKS FRONT YARD 5 FEET REAR YARD 5 FEET 5 FEET SIDE YARD ZONING **EXISTING ZONING** PLANNED UNIT DEVELOPMENT (PUD) PROPOSED ZONING COMMUNITY BUSINESS DISTRICT (CB) PARKING SUMMARY

PARKING REQUIRED FOR MOTOR FUEL STATION

PARKING REQUIRED FOR RETAIL (325/SF)

TOTAL PARKING REQUIRED

TOTAL PARKING PROVIDED

DEVELOPMENT SUMMARY

PROPERTY LIMIT PROJECT LIMIT **CURB & GUTTER** EASEMENT BUILDING **RETAINING WALL** WETLAND LIMITS TREELINE SAWCUT LINE SIGN PLANS) PIPE BOLLARD NUMBER OF PARKING STALLS PER ROW $\langle xx \rangle$ **KEY NOTE**

PROPOSED

EXISTING

EGEND =

3 STALLS

12 STALLS

15 STALLS

15 STALLS

STANDARD DUTY ASPHALT PAVING **HEAVY DUTY** ASPHALT PAVING CONCRETE PAVING CONCRETE SIDEWALK PAVEMENT BY OTHERS (SEE ARCHITECTURAL

Engineering | Surveying | Planning | Environmental Client JERRY'S **ENTERPRISES**

12800 Whitewater Drive, Suite 300

Minnetonka, MN 55343

763.476.6010 telephone

763,476,8532 facsimile

5101 VERNON AVENUE SOUTH **EDINA MN 55436**

Project **HOLIDAY ROSEVILLE**

Location ROSEVILLE, MN

1215 LARPENTEUR AVE WEST

Certification

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the land of the state

If applicable, contact us for a wet signed copy of this

plan which is available upon request at Sambatek's,

Minnetonka, MN office. Summary

Designed: EJC Drawn: AMS Approved: BWF Book / Page: Phase: ENTITLEMENT Initial Issue: 10/01/2021

Revision History No. Date By Submittal / Revision

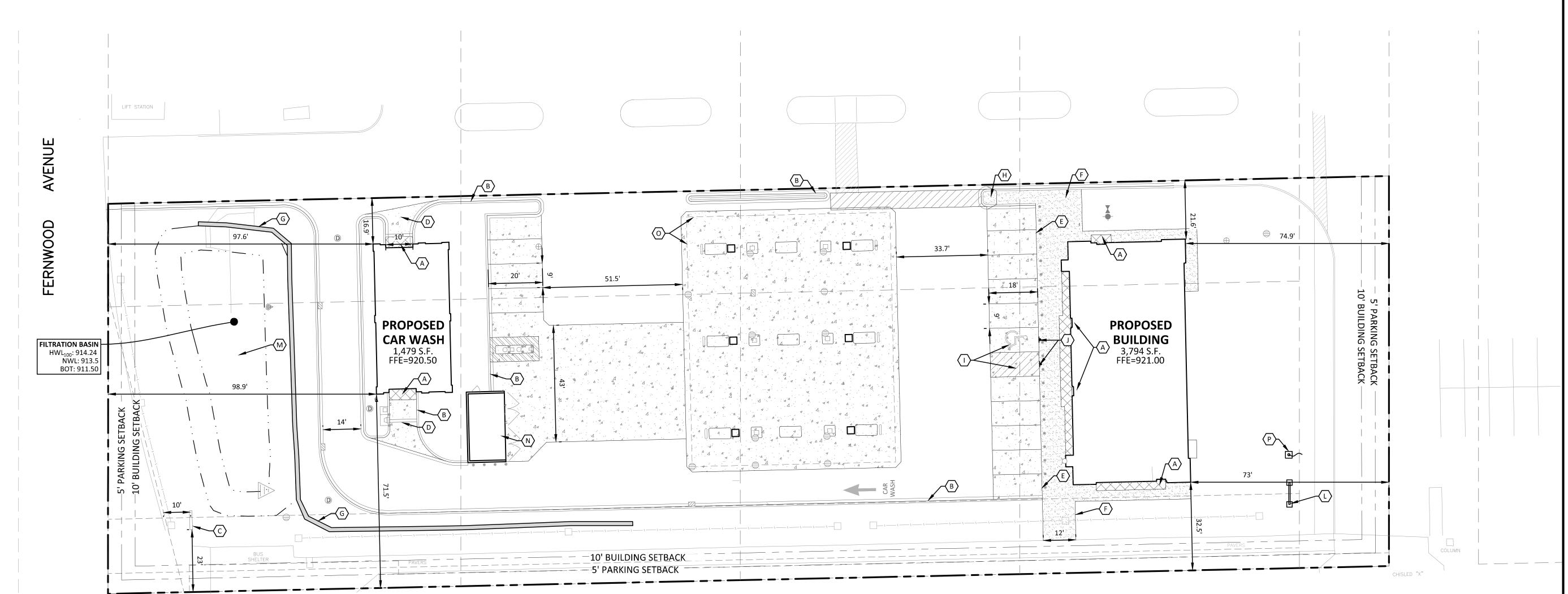
Sheet Title SITE PLAN

Sheet No. Revision

50017

C3.01

Project No.



LARPENTEUR **AVENUE**

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/CI 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA." THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING JTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREE TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES. WHICH MIGHT BE OCCASIONED B' IIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

IF THE CONTRACTOR ENCOUNTERS ANY DRAIN TILE WITHIN THE SITE, HE OR SHE SHALL NOTIFY THE ENGINEER WITH THE LOCATION, SIZE, INVERT AND IF THE TILE LINE IS ACTIVE. NO DRAIN TILE SHALL BE BACKFILLED WITHOUT APPROVAL FROM THE PROJECT ENGINEER.

GRADING NOTES =

GRADING NOTE

- 1. PROPOSED CONTOURS ARE TO FINISHED SURFACE ELEVATION. SPOT ELEVATIONS ALONG PROPOSED CURB DENOTE GUTTER GRADE.
- 2. CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "GUTTER OUT" WHERE WATER DRAINS AWAY FROM CURB. ALL OTHER AREAS SHALL BE CONSTRUCTED AS "GUTTER IN" CURB.
- 3. ALL GRADIENT ON SIDEWALKS ALONG THE ADA ROUTE SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% (1:20), EXCEPT AT CURB RAMPS (1:12), AND A MAXIMUM CROSS SLOPE OF 2.08% (1:48). MAXIMUM SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS AISLE SHALL BE IN 2.08% (1:48). CONTRACTOR SHALL REVIEW AND VERIFY THE GRADIENT IN THE FIELD ALONG THE ADA ROUTES PRIOR TO PLACING CONCRETE OR BITUMINOUS. CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE IS A DISCREPANCY BETWEEN THE GRADIENT IN THE FIELD VERSUS THE DESIGN GRADIENT. COORDINATE ALL WORK WITH PAVING CONTRACTOR.
- 4. CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- 5. SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
- 6. CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
- A GEOTECHNICAL ENGINEERING SOILS REPORT HAS BEEN COMPLETED BY:
- COMPANY:
- ADDRESS:
- PHONE: DATED:
- CONTRACTOR SHALL OBTAIN A COPY OF THE SOILS REPORT.
- 7. CONTRACTOR SHALL COMPLETE DEWATERING AS REQUIRED TO COMPLETE THE SITE GRADING CONSTRUCTION.
- 8. PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL SHALL BE PERFORMED ON THE STREET AND PARKING AREA SUBGRADE. CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE

- COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- 9. REPLACE ALL SUBGRADE SOIL DISTURBED DURING THE CONSTRUCTION THAT HAVE BECOME UNSUITABLE AND WILL NOT PASS A TEST ROLL. REMOVE UNSUITABLE SOIL FROM THE SITE AND IMPORT SUITABLE SOIL AT NO ADDITIONAL COST TO THE OWNER.
- 10. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING VEHICULAR AND PEDESTRIAN TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- 11. EXISTING TREES AND OTHER NATURAL VEGETATION WITHIN THE PROJECT AND/OR ADJACENT TO THE PROJECT ARE OF PRIME CONCERN TO THE CONTRACTOR'S OPERATIONS AND SHALL BE A RESTRICTED AREA. CONTRACTOR SHALL PROTECT TREES TO REMAIN AT ALL TIMES. EQUIPMENT SHALL NOT NEEDLESSLY BE OPERATED UNDER NEARBY TREES AND EXTREME CAUTION SHALL BE EXERCISED WHEN WORKING ADJACENT TO TREES. SHOULD ANY PORTION OF THE TREE BRANCHES REQUIRE REMOVAL TO PERMIT OPERATION OF THE CONTRACTOR'S EQUIPMENT, CONTRACTOR SHALL OBTAIN THE SERVICES OF A PROFESSIONAL TREE TRIMMING SERVICE TO TRIM THE TREES PRIOR TO THE BEGINNING OF OPERATION. SHOULD CONTRACTOR'S OPERATIONS RESULT IN THE BREAKING OF ANY LIMBS, THE BROKEN LIMBS SHOULD BE REMOVED IMMEDIATELY AND CUTS SHALL BE PROPERLY PROTECTED TO MINIMIZE ANY LASTING DAMAGE TO THE TREE. NO TREES SHALL BE REMOVED WITHOUT AUTHORIZATION BY THE ENGINEER. COSTS FOR TRIMMING SERVICES SHALL BE CONSIDERED INCIDENTAL TO THE GRADING CONSTRUCTION AND NO SPECIAL PAYMENT WILL BE MADE.
- a. RESTRICTED AREAS SHALL INCLUDE ALL DESIGNATED TREED AREAS OUTSIDE OF THE DESIGNATED CONSTRUCTION ZONE. ALL VEGETATION WITHIN THE RESTRICTED AREAS SHALL REMAIN.
- b. CONTRACTOR SHALL RESTRICT ALL GRADING AND CONSTRUCTION ACTIVITIES TO AREAS DESIGNATED ON THE PLANS. ACTIVITIES WITHIN THE CONSTRUCTION MAY BE RESTRICTED TO A NARROWER WIDTH IN THE FIELD TO SAVE ADDITIONAL TREES AS DIRECTED BY THE OWNER.
- c. ACTIVITIES PROHIBITED OUTSIDE OF THE CONSTRUCTION BOUNDARIES WOULD INCLUDE, BUT NOT BE LIMITED TO: SOIL AND OTHER MATERIAL STOCKPILING, EQUIPMENT OR MACHINERY STORAGE, DRIVING OF ANY VEHICLE, LEAKAGE OR SPILLAGE OF ANY "WASHOUT" OR OTHER TOXIC MATERIAL. THE COLLECTION OF OTHER DEBRIS AND SOIL STOCKPILING WILL BE IN AN AREA DETERMINED ON-SITE BY THE ENGINEER.
- d. ALL RESTRICTED AREAS SHALL BE FENCED OFF WITH BRIGHT ORANGE POLYETHYLENE SAFETY NETTING AND STEEL STAKES AS SHOWN ON THE TREE PROTECTION DETAIL. AT NO TIME SHALL THIS FENCING BE REMOVED OR ACTIVITY OF ANY KIND TAKE PLACE WITHIN IT. FINAL PLACEMENT OF ALL PROTECTIVE FENCING SHALL BE COMPLETE BEFORE ANY WORK COMMENCES ON-SITE.
- e. BEFORE COMMENCING WITH ANY EXCAVATION CONTRACTOR SHALL COMPLETE ALL PREPARATORY WORK REGARDING TREE REMOVAL, ROOT PRUNING, TREE PRUNING AND STUMP REMOVAL TO THE SATISFACTION OF THE OWNER.
- f. PREPARATORY WORK SHALL INCLUDE THE FOLLOWING AND SHALL BE COMPLETED UNDER THE DIRECT SUPERVISION OF THE OWNER'S REPRESENTATIVE:

- i. TREE REMOVAL: CONTRACTOR SHALL FELL THE TREES. AT NO TIME SHALL TREES BE BULLDOZED OUT, BUT SHALL BE CUT DOWN AND STUMPS REMOVED SEPARATELY. PRIOR TO THE FELLING OF ALL TREES, PROPER REMOVAL OF A PORTION OR ALL OF THE CANOPY SHALL BE COMPLETED SO THAT TREES IN THE RESTRICTED AREAS SHALL NOT BE INJURED IN THE PROCESS.
- ii. ROOT PRUNING: BEFORE ANY STUMPS ARE TO BE REMOVED, ALL ROOTS SHALL BE SEVERED FROM ROOTS IN THE RESTRICTED AREAS BY SAW CUTTING WITH A VERMEER DESIGNED FOR ROOT PRUNING, BY HAND, OR WITH A CHAINSAW. TREE ROOTS PROJECTING INTO THE CONSTRUCTION ZONE SHALL BE EXPOSED PRIOR TO ROOT PRUNING WITH SMALL MACHINERY, I.E..., BOBCAT.
- iii.TUMP REMOVAL: AT SUCH TIME THAT ROOTS HAVE BEEN PROPERLY SEVERED, STUMPS MAY BE REMOVED. WHERE REMOVAL OF CERTAIN STUMPS COULD CAUSE DAMAGE TO EXISTING PROTECTED TREES, TREE STUMPS SHALL BE GROUND OUT. ALL STUMP REMOVAL SHALL BE UNDER THE DIRECT SUPERVISION OF THE OWNER'S REPRESENTATIVE.
- iv.TREE PRUNING: PROPER PRUNING OF TREES IN THE RESTRICTED ZONE SHALL BE DIRECTED BY AND SUPERVISION AT ALL TIMES BY THE OWNER'S REPRESENTATIVE.
- g. AN OWNER'S REPRESENTATIVE WILL BE AVAILABLE AT ALL TIMES DURING THE PREPARATORY AND CONSTRUCTION PERIOD.
- h. MULCH RATHER THAN SEED OR SOD WILL BE USED AT THE BASE OF QUALITY TREES TO A PERIMETER DETERMINED BY THE OWNER'S REPRESENTATIVE. AREAS TO BE SEEDED FOR EROSION CONTROL PURPOSES WITHIN THE CONSTRUCTION ZONE ARE TO BE DETERMINED BY THE OWNER'S REPRESENTATIVE. NATURAL GROUND COVER WILL BE MAINTAINED WHEREVER POSSIBLE.
- i. THE USE OF RETAINING WALLS NEAR TREES, IN ADDITION TO THOSE REQUIRED ON THE PLANS SHALL BE DETERMINED IN THE FIELD, BASED ON TREE LOCATIONS AND TOPOGRAPHY.
- 12. EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
- 13. TRENCH BORROW CONSTRUCTION: IF ALLOWED BY THE OWNER, CONTRACTOR SHALL COMPLETE "TRENCH BORROW" EXCAVATION IN AREAS DIRECTED BY THE ENGINEER IN ORDER TO OBTAIN STRUCTURAL MATERIAL. TREES SHALL NOT BE REMOVED OR DAMAGED AS A RESULT OF THE EXCAVATION, UNLESS APPROVED BY THE ENGINEER. THE EXCAVATION SHALL COMMENCE A MINIMUM OF 10 FEET FROM THE LIMIT OF THE BUILDING PAD. THE EXCAVATION FROM THIS LIMIT SHALL EXTEND AT A MINIMUM SLOPE OF 1 FOOT HORIZONTAL TO 1 FOOT VERTICAL (1:1) DOWNWARD AND OUTWARD FROM THE FINISHED SURFACE GRADE ELEVATION. THE TRENCH BORROW EXCAVATION SHALL BE BACKFILLED TO THE PROPOSED FINISHED GRADE ELEVATION, AND SHALL BE COMPACTED IN ACCORDANCE WITH REQUIREMENTS OF THE QUALITY COMPACTION METHOD AS OUTLINED IN MN/DOT SPECIFICATION 2105.3F2. SNOW FENCE SHALL BE FURNISHED AND PLACED ALONG THE PERIMETER OF THE TRENCH BORROW AREA WHERE THE SLOPES EXCEED 2 FOOT HORIZONTAL TO 1 FOOT VERTICAL (2:1).

14. FINISHED GRADING SHALL BE COMPLETED, CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS. PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, OR BETWEEN SUCH POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISHED GRADED SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND EROSION. REPAIR ALL AREAS THAT HAVE BECOME RUTTED, ERODED OR HAS SETTLED BELOW THE CORRECT GRADE. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.

15. TOLERANCES

- a. THE COMMERCIAL BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.10 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
- b. THE PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT
- c. AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- d. TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
- 16. AFTER THE SITE GRADING IS COMPLETED, IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
- 17. CONTRACTOR SHALL DETERMINE THE LOCATION OF ANY HAUL ROADS THAT MAY BE REQUIRED TO COMPLETE THE SITE GRADING CONSTRUCTION AND SHALL INDICATE HAUL ROADS ON EROSION AND SEDIMENT CONTROL "SITE MAP". CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF THE GOVERNING AUTHORITY OF EACH ROADWAY. CONTRACTOR SHALL POST WHATEVER SECURITY AND COMPLY WITH ALL CONDITIONS WHICH ARE REQUIRED BY EACH GOVERNING AUTHORITY OF EACH ROADWAY.

CIVIL 3D MODEL LIMITATIONS

SAMBATEK'S DELIVERABLE AND GOVERNING DOCUMENTS FOR CONSTRUCTION SHALL BE A HARD COPY AND/OR PDF PLAN SHEETS. IF A CIVIL 3D MODEL IS GENERATED IN THE PROCESS OF PREPARING THE PLAN SHEETS, IT IS AS A DESIGN TOOL ONLY AND NOT AS A SEPARATE DELIVERABLE. AT THE OWNER'S REQUEST, WE WILL RELEASE OUR CIVIL 3D MODEL FOR THE CONTRACTOR'S USE. HOWEVER, ITS USE IS AT THE CONTRACTOR'S RISK AND SHALL NOT BE USED FOR STAKING OF CURB, SIDEWALK, OR OTHER HARD SURFACE IMPROVEMENTS. IF A CIVIL 3D MODEL FOR STAKING HARD SURFACE IMPROVEMENTS IS REQUIRED, WE CAN PROVIDE A SUPPLEMENTAL AGREEMENT FOR REFINEMENT AND PREPARATION OF THE CIVIL 3D MODEL.



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Engineering | Surveying | Planning | Environmental

Client JERRY'S ENTERPRISES

5101 VERNON AVENUE SOUTH EDINA MN 55436

Project HOLIDAY ROSEVILLE

Location ROSEVILLE, MN

1215 LARPENTEUR AVE WEST

Certification

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the law of the state of Minnesota.

Brian Warkank
Registration No. 52728 Date: 10/01/2021
If applicable, contact us for a wet signed copy of this plan which is available upon request at Sambatek's,

Summary

Minnetonka, MN office.

Designed: EJC Drawn: AMS
Approved: BWF Book / Page:
Phase: ENTITLEMENT Initial Issue: 10/01/2021

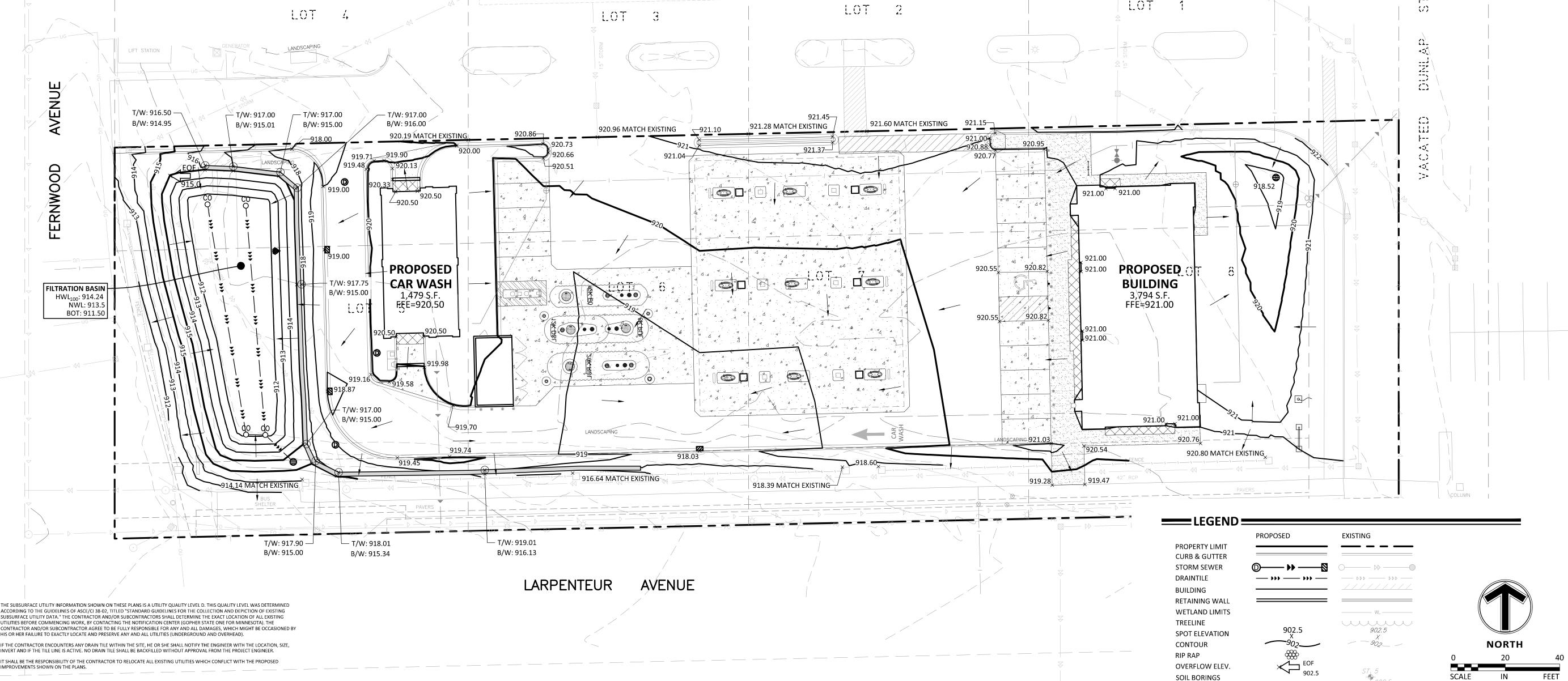
Revision History
No. Date By Submittal / Revision

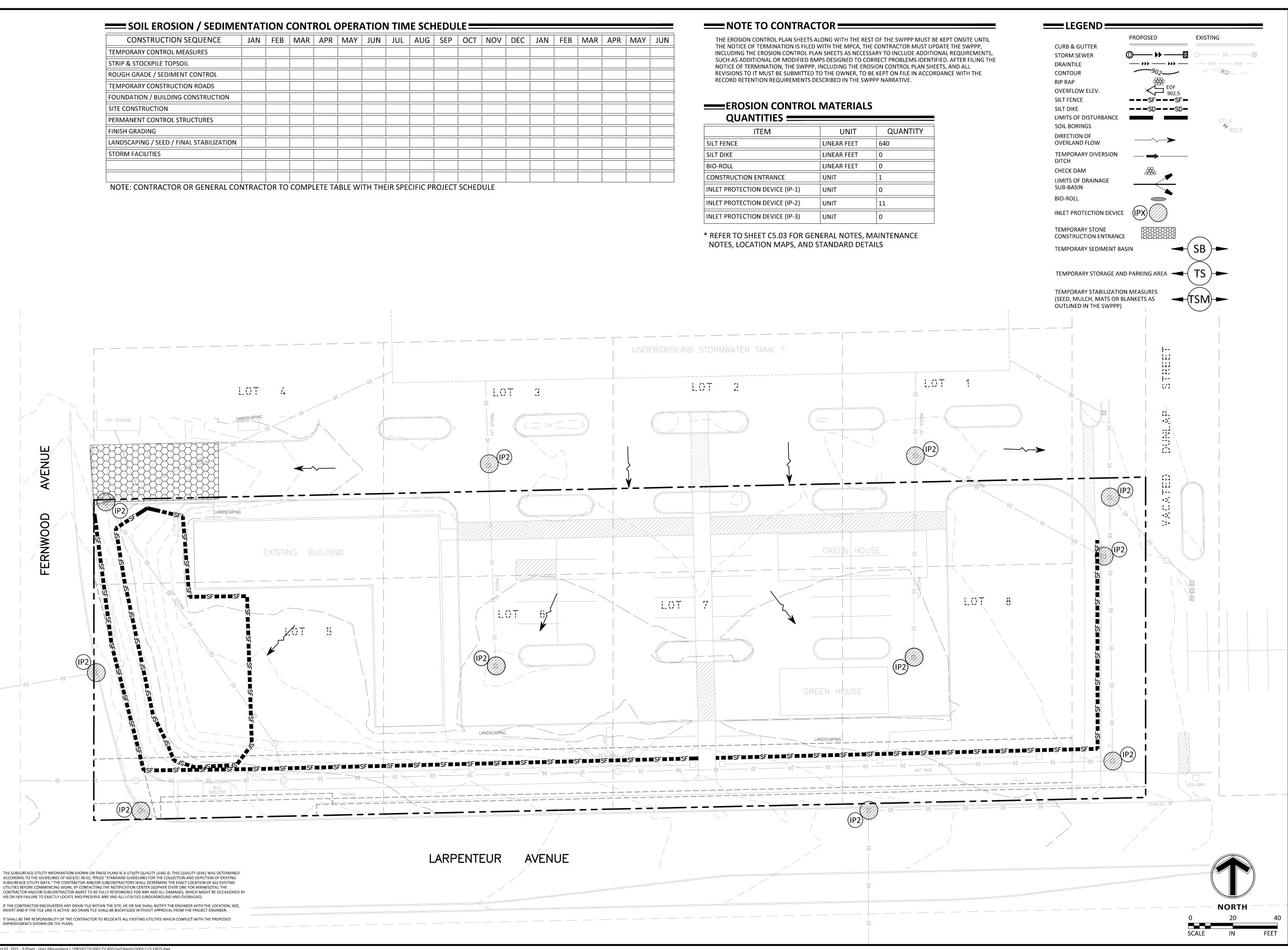
Sheet Title GRADING PLAN

Sheet No. Revision C4.01

Project No.

oject No. 50017







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Engineering | Surveying | Planning | Environmental

Client **JERRY'S ENTERPRISES**

5101 VERNON AVENUE SOUTH **EDINA MN 55436**

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plan which is available upon request at Sambatek's,

Revision History
No. Date By Submittal / Revision

Sheet Title PHASE I EROSION CONTROL PLAN

Sheet No. Revision

C5.01

SOIL EROSION / SEDIMENTATION CONTROL OPERATION TIME SCHEDULE CONSTRUCTION SEQUENCE JAN | FEB | MAR | APR | MAY | JUN | JUL | AUG | SEP | OCT | NOV | DEC | JAN | FEB | MAR | APR | MAY | JUN TEMPORARY CONTROL MEASURES STRIP & STOCKPILE TOPSOIL ROUGH GRADE / SEDIMENT CONTROL TEMPORARY CONSTRUCTION ROADS FOUNDATION / BUILDING CONSTRUCTION SITE CONSTRUCTION PERMANENT CONTROL STRUCTURES FINISH GRADING LANDSCAPING / SEED / FINAL STABILIZATION STORM FACILITIES

NOTE: CONTRACTOR OR GENERAL CONTRACTOR TO COMPLETE TABLE WITH THEIR SPECIFIC PROJECT SCHEDULE

NOTE TO CONTRACTOR

THE EROSION CONTROL PLAN SHEETS ALONG WITH THE REST OF THE SWPPP MUST BE KEPT ONSITE UNTIL THE NOTICE OF TERMINATION IS FILED WITH THE MPCA, THE CONTRACTOR MUST UPDATE THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS SUCH AS ADDITIONAL OR MODIFIED BMPS DESIGNED TO CORRECT PROBLEMS IDENTIFIED. AFTER FILING THE NOTICE OF TERMINATION, THE SWPPP, INCLUDING THE EROSION CONTROL PLAN SHEETS, AND ALL REVISIONS TO IT MUST BE SUBMITTED TO THE OWNER, TO BE KEPT ON FILE IN ACCORDANCE WITH THE RECORD RETENTION REQUIREMENTS DESCRIBED IN THE SWPPP NARRATIVE

EROSION CONTROL MATERIALS

QUANTITIES ====		
ITEM	UNIT	QUANTITY
SILT FENCE	LINEAR FEET	930
SILT DIKE	LINEAR FEET	0
BIO-ROLL	LINEAR FEET	0
CONSTRUCTION ENTRANCE	UNIT	1
INLET PROTECTION DEVICE (IP-1)	UNIT	5
INLET PROTECTION DEVICE (IP-2)	UNIT	12

===LEGEND PROPOSED **EXISTING CURB & GUTTER**

STORM SEWER

OVERFLOW ELEV

SOIL BORINGS

DIRECTION OF

OVERLAND FLOW

LIMITS OF DISTURBANCE

TEMPORARY DIVERSION

LIMITS OF DRAINAGE

---SF---SF-

---SD---SD-

DRAINTILE

CONTOUR

SILT FENCE

SILT DIKE

DITCH

CHECK DAM

SUB-BASIN BIO-ROLL

RIP RAP

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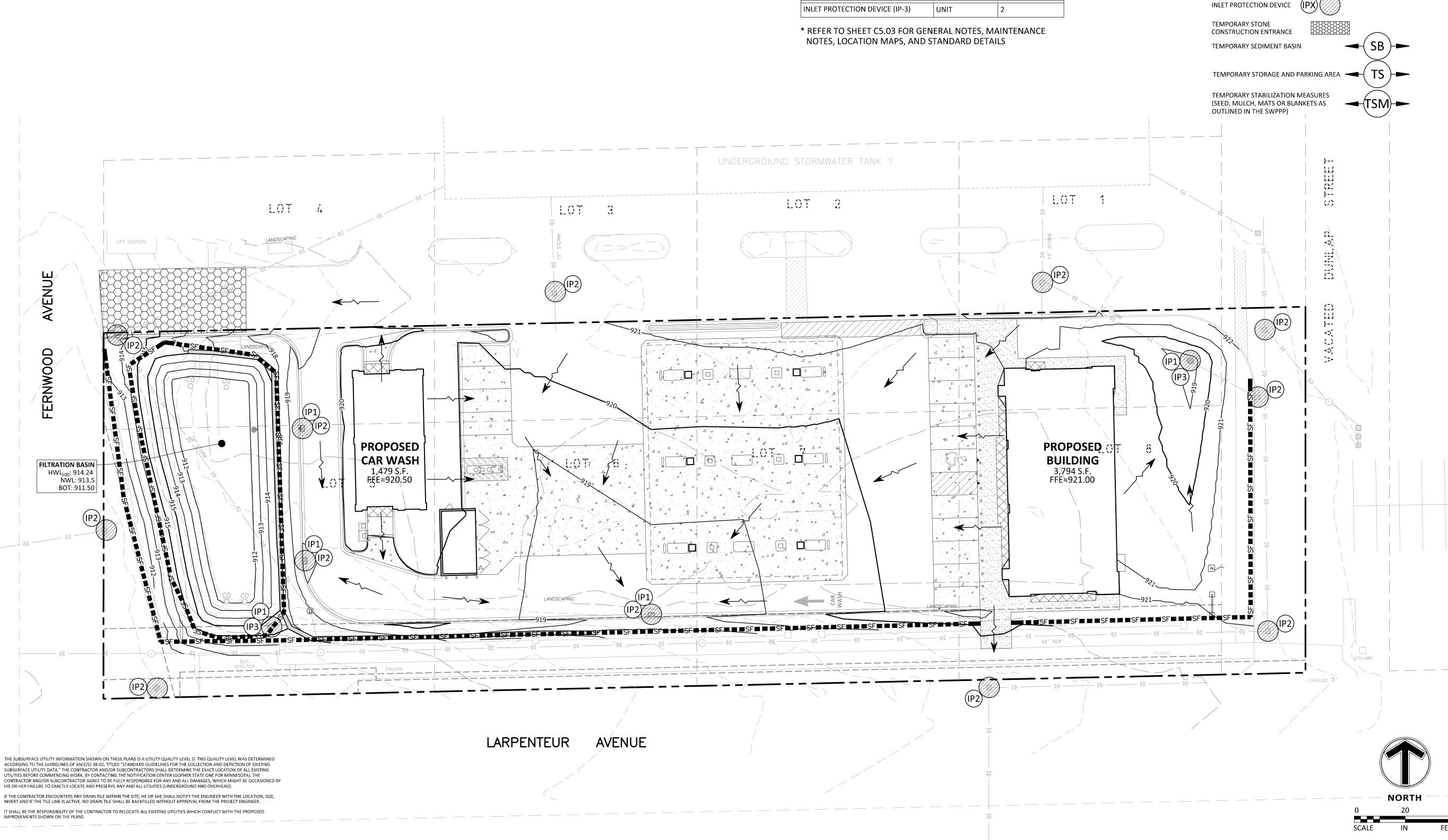
Revision History No. Date By Submittal / Revision

Sheet Title PHASE II EROSION

CONTROL PLAN

Sheet No. Revision

C5.02



1. THE UTILITY IMPROVEMENTS FOR THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD UTILITIES SPECIFICATIONS" AS PUBLISHED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA

—— UTILITY CONSTRUCTION NOTES ————

- (CEAM), EXCEPT AS MODIFIED HEREIN. CONTRACTOR SHALL OBTAIN A COPY OF THESE SPECIFICATIONS.

 a. ALL UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL REQUIRMENTS, INCLUDING BUT NOT LIMITED TO CITY, DEPARTMENT OF LABOR AND INDUSTRY AND
- b. CONTRACTOR SHALL NOT OPEN, TURN OFF, INTERFERE WITH, OR ATTACH ANY PIPE OR HOSE TO OR TAP WATERMAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF ANY SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE THE LIABILITY OF CONTRACTOR.
- c. A MINIMUM VERTICAL SEPARATION OF 18 INCHES, AND HORIZONTAL SEPARATION OF 10-FEET, BETWEEN OUTSIDE PIPE AND/OR STRUCTURE WALLS, IS REQUIRED AT ALL WATERMAIN AND SEWER MAIN (BUILDING, STORM AND SANITARY) CROSSINGS.
- 2. ALL MATERIALS SHALL BE AS SPECIFIED IN CEAM SPECIFICATIONS EXCEPT AS MODIFIED HEREIN.
- a. ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
- b. ALL SANITARY SEWER TO BE PVC SDR-26, UNLESS NOTED OTHERWISE.

MINNESOTA DEPARTMENT OF HEALTH REQUIREMENTS.

- i.a. ALL SANITARY SEWER SERVICES TO BUILDING SHALL BE PVC SDR-26 CONFORMING TO ASTM D2665. c. ALL WATERMAIN TO BE PVC C900, UNLESS NOTED OTHERWISE.
- i.a. ALL WATERMAIN TO HAVE 7.5-FEET OF COVER OVER TOP OF WATERMAIN.
- ii.b. PROVIDE THRUST BLOCKING AND MECHANICAL JOINT RESTRAINTS ON ALL WATERMAIN JOINTS PER
- d. ALL STORM SEWER PIPE TO BE SMOOTH INTERIOR DUAL WALL HDPE PIPE WITH WATERTIGHT GASKETS, UNLESS NOTED OTHERWISE.
- i.a. ALL STORM SEWER PIPE FOR ROOF DRAIN SERVICES TO BUILDING SHALL BE PVC SCH 40 CONFORMING TO ASTM D2665.
- e. RIP RAP SHALL BE Mn/DOT CLASS 3.

CITY STANDARDS.

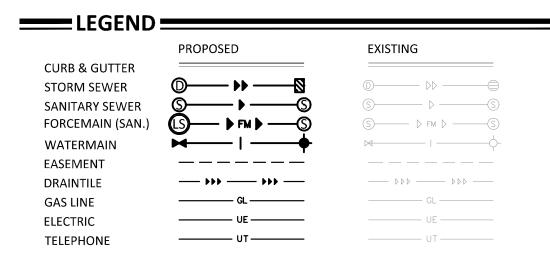
- 3. COORDINATE ALL BUILDING SERVICE CONNECTION LOCATIONS AND INVERT ELEVATIONS WITH MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION.
- 4. ALL BUILDING SERVICE CONNECTIONS (STORM, SANITARY, WATER) WITH FIVE FEET OR LESS COVER ARE TO BE INSULATED FROM BUILDING TO POINT WHERE 5-FEET OF COVER IS ACHIEVED.
- 5. CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- 6. SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
- 7. ALL AREAS OUTSIDE THE PROPERTY BOUNDARIES THAT ARE DISTURBED BY UTILITY CONSTRUCTION SHALL BE RESTORED IN KIND. SODDED AREAS SHALL BE RESTORED WITH 6 INCHES OF TOPSOIL PLACED BENEATH THE SOD.
- 8. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- 9. ALL SOILS TESTING SHALL BE COMPLETED BY AN INDEPENDENT SOILS ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE SOILS ENGINEER. THE UTILITY BACKFILL CONSTRUCTION SHALL COMPLY WITH THE REQUIREMENTS OF THE SOILS ENGINEER. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND SOIL INSPECTIONS WITH THE SOILS ENGINEER. A GEOTECHNICAL ENGINEERING REPORT HAS BEEN COMPLETED BY:

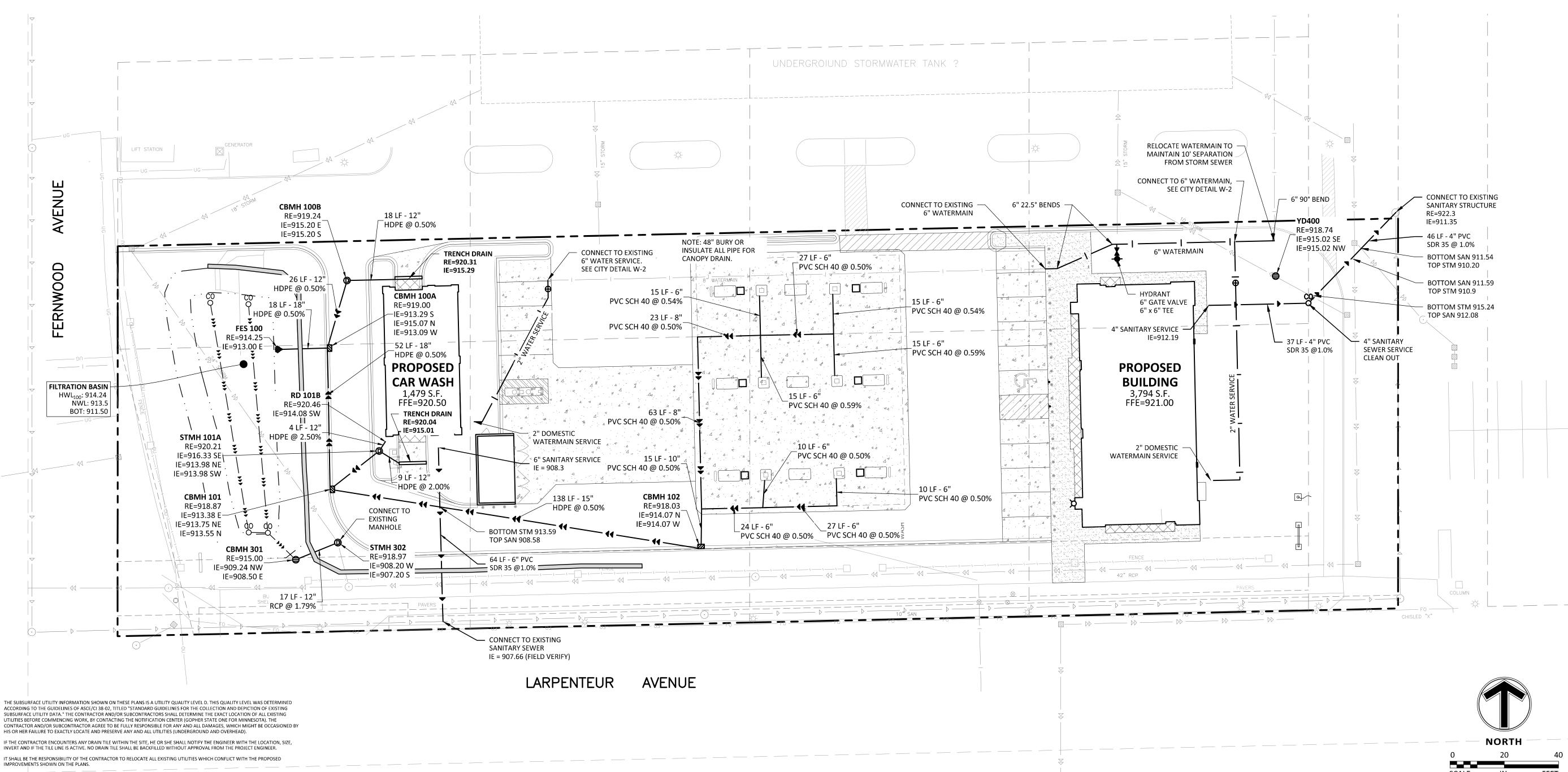
COMPANY:
ADDRESS:
PHONE:

DATED:

CONTRACTOR SHALL OBTAIN A COPY OF THIS SOILS REPORT.

- 3. CONTRACTOR SHALL SUBMIT 2 COPIES OF SHOP DRAWINGS FOR MANHOLE AND CATCH BASIN STRUCTURES TO SAMBATEK. CONTRACTOR SHALL ALLOW 5 WORKING DAYS FOR SHOP DRAWING REVIEW.
- 4. CONTRACTOR AND MATERIAL SUPPLIER SHALL DETERMINE THE MINIMUM DIAMETER REQUIRED FOR EACH STORM SEWER STRUCTURE.







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Engineering | Surveying | Planning | Environmental

Client JERRY'S ENTERPRISES

5101 VERNON AVENUE SOUTH EDINA MN 55436

Project HOLIDAY ROSEVILLE

Location ROSEVILLE, MN

1215 LARPENTEUR AVE WEST

Certification

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional ENGINEER under the last of the state of Minnesota.

Brian Which ik Register ion No. 52728 Date: 10/01/2021 If applicable, contact us for a wet signed copy of this plan which is available upon request at Sambatek's,

Summary

Minnetonka, MN office.

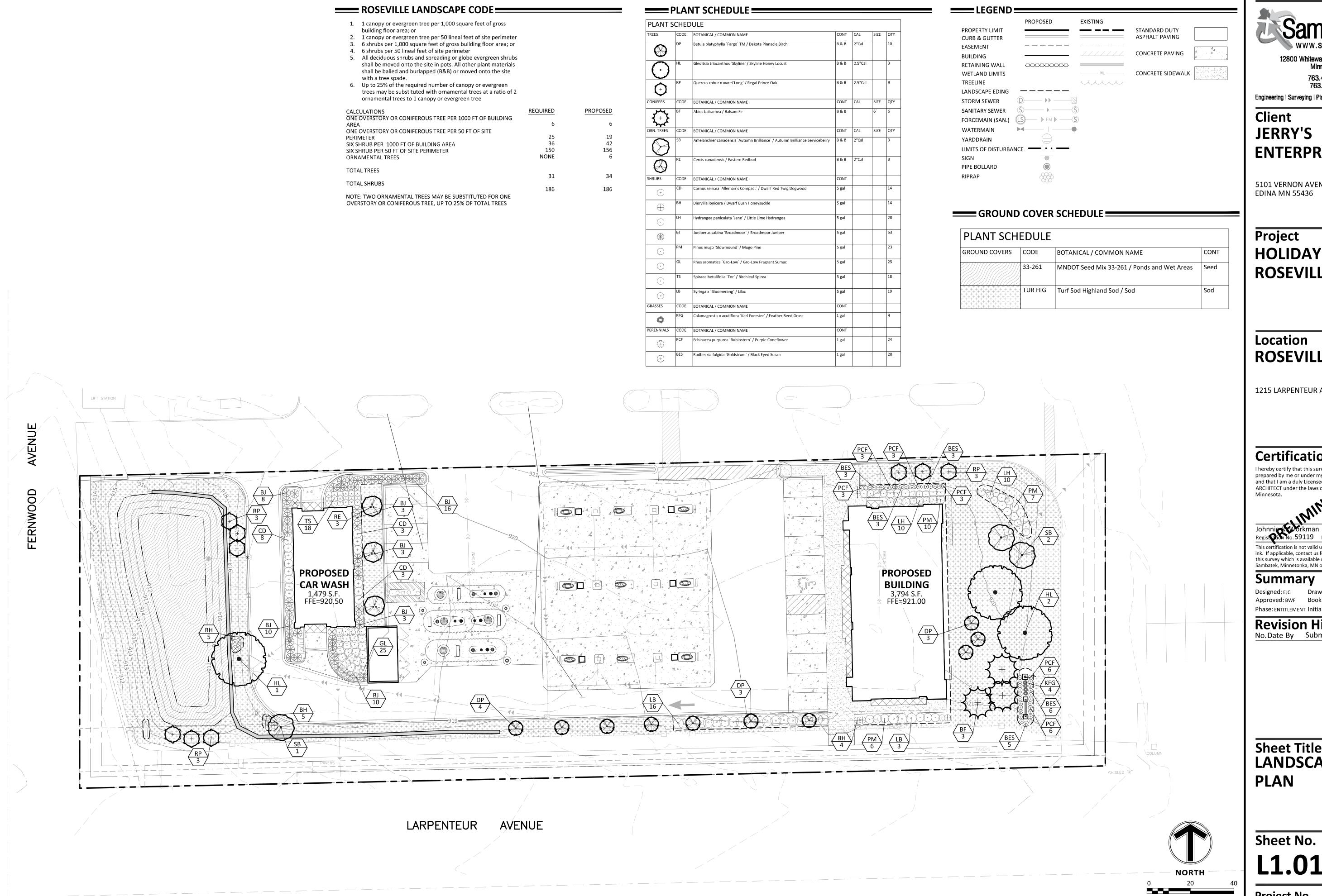
Designed: EJC Drawn: AMS
Approved: BWF Book / Page:
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Sheet Title UTILITY PLAN

Sheet No. Revision

C6.01



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I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed LANDSCAPE
ARCHITECT under the laws of the State of
Minnesota.

ink. If applicable, contact us for a wet signed copy of this survey which is available upon request at Sambatek, Minnetonka, MN office.

Summary

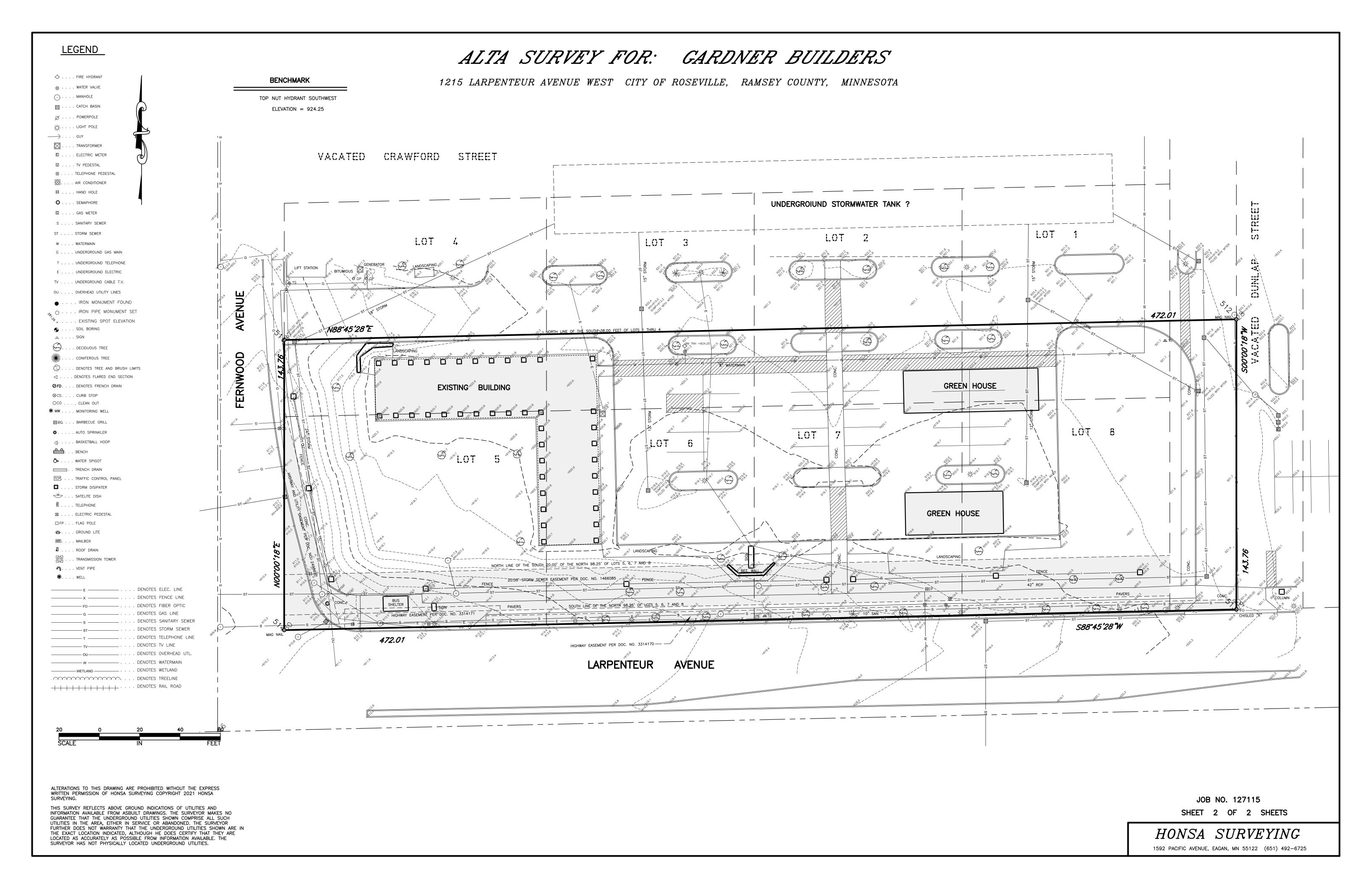
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Revision History
No. Date By Submittal / Revision

Sheet Title LANDSCAPE PLAN

Sheet No. Revision

50017





Memorandum

SRF No. 02114043

To: Jesse Freihammer, PE

Assistant Public Works Director/City Engineer

City of Roseville

From: Tom Sachi, PE, Associate

Mark Powers, PE, PTOE, Senior Engineer

Date: September 29, 2021

Subject: Holiday Gas Station Traffic Impact Study

Introduction

SRF has completed a traffic study for the proposed Holiday Gas Station located in the parking lot of the Roseville Center shopping center in the City of Roseville, MN. The proposed development is located in the northeast quadrant of the Larpenteur Avenue/Fernwood Street intersection (see Figure 1: Project Location). The main objectives of the study are to review existing operations, evaluate potential traffic impacts associated with the proposed development, and recommend improvements to ensure safe and efficient operations, if necessary. The following information provides the assumptions, analysis, and study recommendations offered for consideration.

Existing Conditions

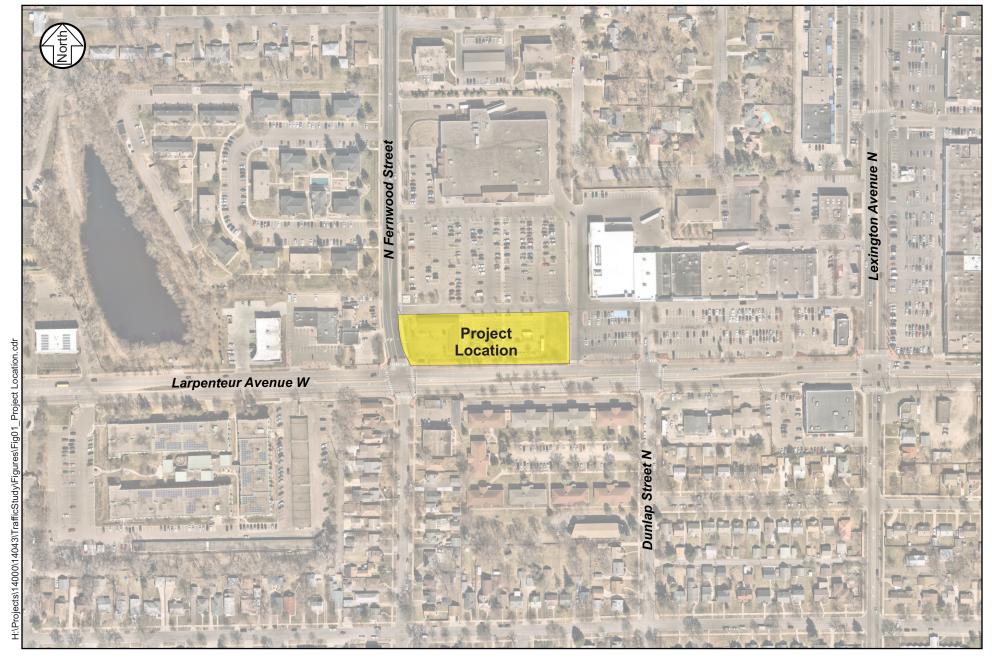
The existing conditions were reviewed to establish a baseline to identify any future impacts associated with the proposed development. The evaluation of existing conditions includes peak hour intersection turning movement counts and an intersection capacity analysis.

Data Collection

Weekday a.m. and p.m. peak period turning movement counts were collected by at the following study intersections:

- Larpenteur Avenue/Fernwood Street
- Larpenteur Avenue/Driveway Access (right in/right out)
- Larpenteur Avenue/Dunlap Street
- Fernwood Street/Driveway Access

Attachment D





Project Location

September 29, 2021 Page 3

In addition to intersection turning movement counts, field observations were completed to identify roadway characteristics within the study area (i.e. roadway geometry, posted speed limits, and traffic controls). Larpenteur Avenue is a four-lane divided roadway with a posted speed limit of 35 miles per hour (mph) and is classified as a minor augmentor roadway. Fernwood Street is classified as a local roadway with a posted speed limit of 30 mph. Existing signal timing was collected from Ramsey County. Existing geometrics, traffic control, and weekday a.m. and p.m. peak hour volumes are shown in Figure 2.

Existing Intersection Capacity Analysis

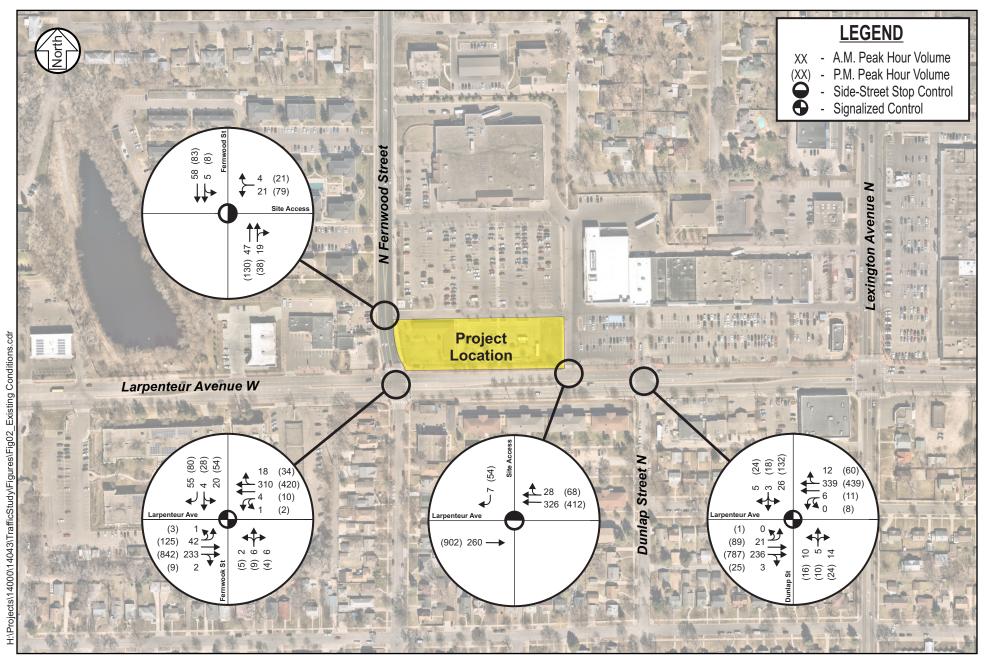
An existing intersection capacity analysis was completed to establish a baseline condition to which future traffic operations can be compared. The capacity analysis was completed for weekday a.m. and p.m. peak hours using Synchro/SimTraffic software (V11.0). Capacity analysis results identify a Level of Service (LOS), which indicates the quality of traffic flow through an intersection. Intersections are given a ranking from LOS A through LOS F. The LOS results are based on average delay per vehicle, which correspond to the delay threshold values shown in Table 1. LOS A indicates the best traffic operation, with vehicles experiencing minimal delays. LOS F indicates an intersection where demand exceeds capacity, or a breakdown of traffic flow. An overall LOS A through D is considered acceptable in the Twin Cities Metropolitan Area.

Table 1 Level of Service Criteria for Signalized and Unsignalized Intersections

LOS Designation	Signalized Intersection Average Delay/Vehicle (seconds)	Unsignalized Intersection Average Delay/Vehicle (seconds)
А	≤ 10	≤ 10
В	> 10 - 20	> 10 - 15
С	> 20 - 35	> 15 - 25
D	> 35 - 55	> 25 - 35
E	> 55 - 80	> 35 - 50
F	> 80	> 50

For side-street stop controlled intersections, special emphasis is given to providing an estimate for the level of service of the side-street approach. Traffic operations at an unsignalized intersection with side-street stop control can be described in two ways. First, consideration is given to the overall intersection level of service. This considers the total number of vehicles entering the intersection and the capability of the intersection to support these volumes.

Second, it is important to consider the delay on the minor approach. Since the mainline does not have to stop, most delay is attributed to the side-street approaches. It is typical of intersections with higher mainline traffic volumes to experience high levels of delay (i.e. poor levels of service) on the side-street approaches, but an acceptable overall intersection level of service during the peak hours.





Existing Conditions

Results of the existing intersection capacity analysis shown in Table 2 indicate that all study intersections currently operate at an acceptable overall LOS B or better during the weekday a.m. and p.m. peak hours with the existing traffic control, geometric layout, and signal timing. Southbound queues at the Larpenteur Avenue and Fernwood Street intersection are expected to reach the driveway access less than five (5) percent of the p.m. peak hour. No other queuing issues were identified.

Table 2 Existing Intersection Capacity Analysis

Intersection	A.M. Pe	ak Hour	P.M. Peak Hour		
	LOS	Delay	LOS	Delay	
Larpenteur Avenue/Fernwood Street	А	4 sec.	А	6 sec.	
Larpenteur Avenue/Driveway Access (1)	A/A	2 sec.	A/A	2 sec.	
Larpenteur Avenue/Dunlap Street	А	5 sec.	В	10 sec.	
Fernwood Street/Driveway Access (1)	A/A	4 sec.	A/A	6 sec.	

⁽¹⁾ Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.

Year 2023 Build Conditions

To help determine impacts associated with the proposed development, year 2023 conditions (i.e. one year after expected completion) were evaluated. For this analysis, it is assumed that there will no changes to the roadway geometry or signal operations.

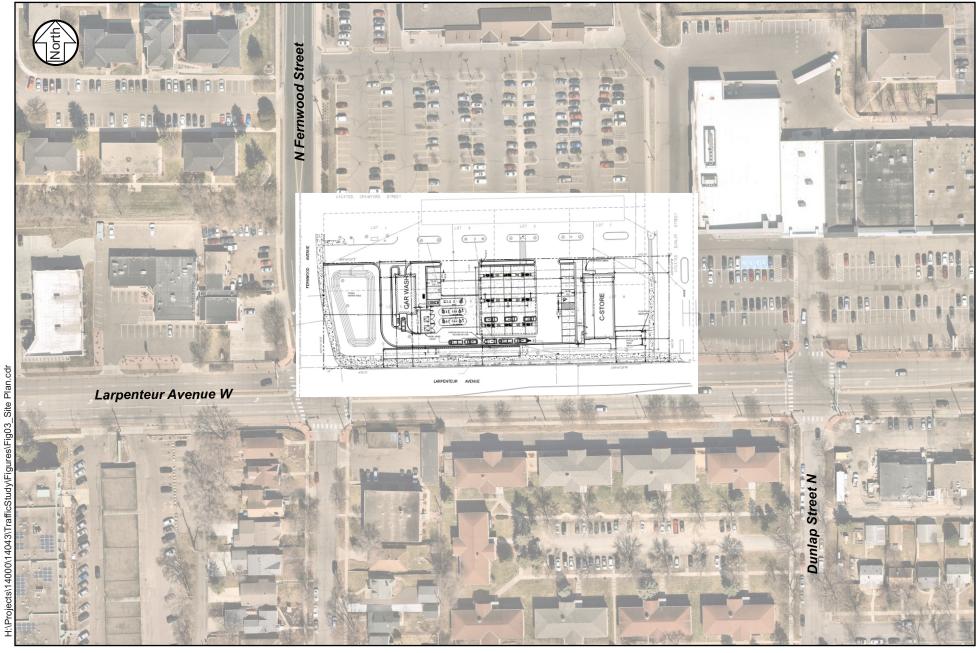
Background Traffic Growth

To account for background traffic growth within the study area a one-half percent annual growth rate was applied to the existing traffic volumes. This is consistent with historical trends within the study area.

Proposed Development

The proposed Holiday Gas Station development, shown in Figure 3, is expected to consist of a total of 18 fueling positions (i.e. 18 passenger vehicles), a 3,700-square foot convenience store, and single-lane drive-thru carwash. The car wash resides on the west side of the site, and the convenience store on the east side with fueling stations between them. The site will provide 11 passenger vehicle parking spaces. Currently the proposed site consists of pavilion area, 14 parking spaces, and a seasonal garden center. Access to the proposed development will be provided by existing driveways to the shopping center located on Larpenteur Avenue and Fernwood Street. The proposed development is expected to be fully constructed by year 2023.

Attachment D





Site Plan

September 29, 2021 Page 7

Trip Generation

To account for traffic impacts associated with the proposed development, trip generation estimates were developed for the weekday a.m. and p.m. peak hours and a daily basis. The estimates were developed using the *Institute of Transportation Engineers (ITE) Trip Generation Manual, Tenth Edition*. Results of the trip generation estimates shown in Table 3 indicate that the proposed development is expected to generate approximately 245 a.m. peak hour, 252 p.m. peak hour trips, and 3,696 daily trips. However, a 40 percent multi-use reduction was applied for trips that are made that are already on-site (i.e. Roseville Center and Cub Foods), which is consistent with historical observations completed by SRF for other gas station/grocery store sites. Accounting for the multi-use reduction, the site is expected to generate approximately 147 a.m. peak hour, 151 p.m. peak hour, and 2,218 daily new trips to/from the site. It should be noted that a portion of the vehicles entering/exiting the site are already traveling along the adjacent study roadways (i.e. Larpenteur Avenue). Therefore, a pass-by reduction of 62 percent in the a.m. peak and 56 percent in the p.m. peak was applied based on *ITE Trip Generation Handbook*.

Table 3 Trip Generation Estimates

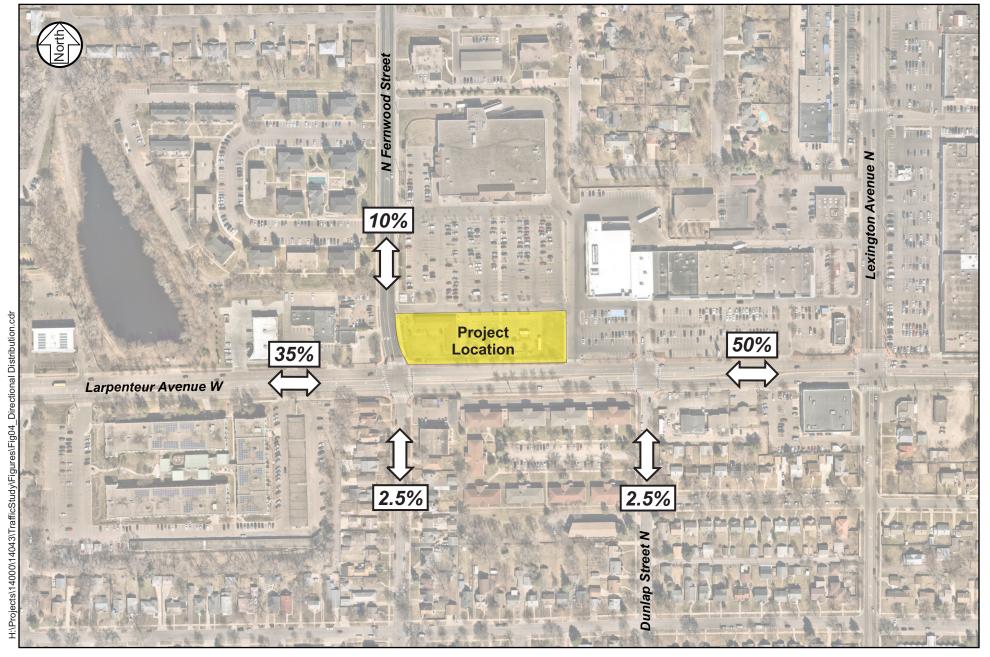
Land Use Type (ITE Code)	Size	A.M. Peak Hour Trips		P.M. Peak Hour Trips		Daily Trips
		In	Out	In	Out	
Gas Station with Convenience Market (945)	18 pumps	125	120	128	124	3,696
Multi-Use Reduction (40%)		(50)	(48)	(51)	(50)	(1,478)
Total Site Trips		75	72	77	74	2,218
Pass-By Trip Reduction (62% AM, 56% PM)		(53)	(45)	(43)	(41)	(1,309)
Total New Network Trips		32	27	34	33	909

Accounting for pass-by reductions, the site is expected to generate approximately 59 new a.m. peak hour, 67 new p.m. peak hour, and 909 new daily trips. The trips generated were distributed throughout the area based on the directional distributions shown in Figure 4, which was developed based on existing travel patterns, traffic volumes, and engineering judgment. The resultant year 2023 build traffic forecasts, which accounts for the background traffic growth and the trip generation are shown in Figure 5.

Year 2023 Build Conditions Intersection Capacity Analysis

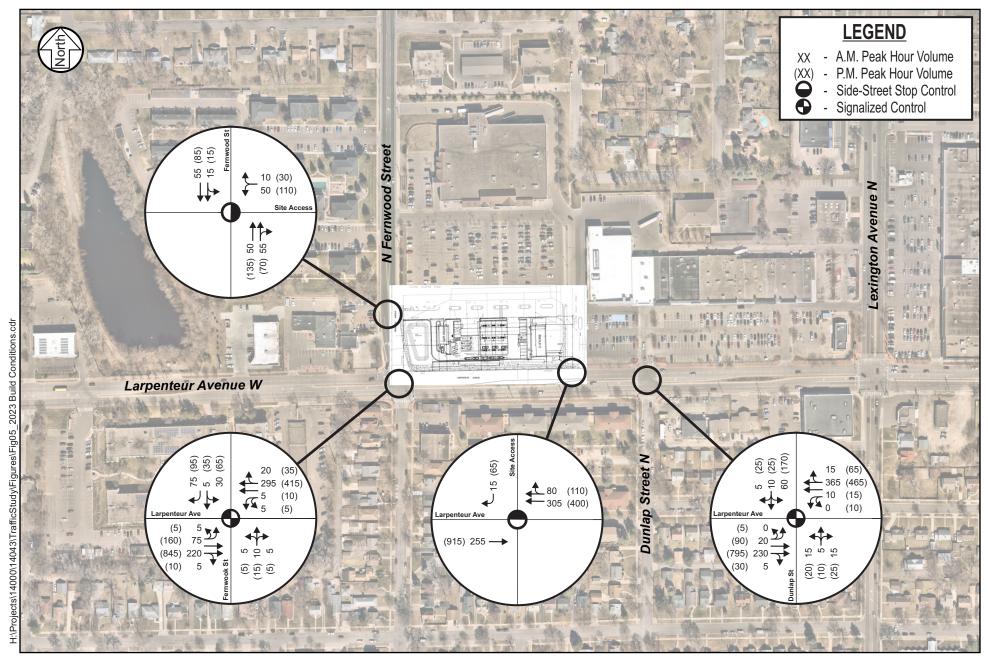
To determine how the roadway network is expected to operate with year 2023 build traffic forecasts, a detailed traffic capacity analysis was completed using Synchro/SimTraffic software. Results of the year 2023 build intersection capacity analysis shown in Table 4 indicate that all study intersections are expected to operate at an acceptable overall LOS B or better during the a.m. and p.m. peak hours. Southbound queues along Fernwood Street are expected to continue to reach the driveway access less than five (5) percent of the p.m. peak hour. No other queuing issues are expected.

Attachment D





Directional Distribution





2023 Build Conditions

September 29, 2021 Page 10

Table 4 Year 2023 Build Conditions Intersection Capacity Analysis

Intersection	A.M. Pe	eak Hour	P.M. Peak Hour	
intersection	LOS	Delay	LOS	Delay
Larpenteur Avenue/Fernwood Street	А	5 sec.	А	7 sec.
Larpenteur Avenue/Driveway Access (1)	A/A	3 sec.	A/A	2 sec.
Larpenteur Avenue/Dunlap Street	A	7 sec.	В	11 sec.
Fernwood Street/Driveway Access (1)	A/A	5 sec.	A/A	6 sec.

⁽¹⁾ Indicates an unsignalized intersection with side-street stop control, where the overall LOS is shown followed by the worst approach LOS. The delay shown represents the worst side-street approach delay.

Based on the results of the intersection capacity analysis there is not expected to be any significant operations impacts as a result of the proposed development. Therefore, no mitigation is expected to be necessary to accommodate the proposed development.

Site Plan/Access Review

A review of the proposed site plan was completed to identify any issues and recommend potential improvements with regard to car-wash operations, access, and circulation. Based on field observations, there is adequate sight distance at the existing right-in/right-out access location on Larpenteur Avenue to clearly identify approaching vehicles. Special consideration should be made to limit any sight distance impacts from future landscaping and signing. The drive aisles for the proposed gas station are generally aligned with the drive aisles from Cub Foods.

A review of the car-wash queuing storage area was completed. Based on *Drive Through Queue Generation* published by CountingCars.com in 2012 for car-wash queueing lengths within the Twin Cities, there is expected to be average queues of four (4) vehicles and 85th percentile queues of six (6) vehicles. The proposed car-wash can accommodate a queue of six (6) vehicles without blocking any drive aisle access. Therefore, it is expected that the proposed car-wash storage area should be sufficient to accommodate queues. Note, there is a bypass lane west of the car-wash to allow for any vehicles that may not wish to wait within the queue. No other traffic control or circulation issues are expected.

Summary and Conclusion

Based on the analysis, the following summary and conclusions are offered for consideration:

- 1. Results of the existing intersection capacity analysis indicate that all study intersections currently operate at overall LOS B or better during the weekday a.m. and p.m. peak hours.
- 2. The proposed Holiday Gas Station is expected to consist of a total of 18 fueling positions for passenger vehicles, a 3,700-square foot convenience store, and single-lane drive-thru carwash.
- 3. Results of the trip generation estimates indicate that the proposed development is expected to generate approximately 245 a.m. peak hour, 252 p.m. peak hour trips, and 3,696 daily trips.
 - Accounting for the multi-use reduction, the site is expected to generate approximately 147 a.m. peak hour, 151 p.m. peak hour, and 2,218 daily new trips to/from the site.
 - A pass-by reduction of 62 percent in the a.m. peak and 56 percent in the p.m. peak was applied based on *ITE Trip Generation Handbook*.
- 4. Results of the year 2023 build intersection capacity analysis indicate that all study intersections are expected to operate at overall LOS B or better during the a.m. and p.m. peak hours.
 - Queueing is expected to remain similar to existing conditions.
- 5. Based on the results of the intersection capacity analysis there is not expected to be any significant operations impacts as a result of the proposed development. Therefore, no mitigation is expected to be necessary to accommodate the proposed development.
- 6. Based on field observations, there is adequate sight distance at the existing right-in/right-out access location on Larpenteur Avenue to clearly identify approaching vehicles. Special consideration should be made to limit any sight distance impacts from future landscaping and signing.
 - The proposed car-wash is expected to have sufficient storage area.

H:\Projects\14000\14043\TraffStudy\Reports\Report\14043_FINAL_HolidayGasStationTrafficStudy_210929.docx

ORDINANCE NO. ____

AN ORDINANCE CANCELLING PLANNED UNIT DEVELOPMENT #1364 REGULATING DEVELOPMENT AT 1201 LARPENTEUR AVENUE

1	The City	Council Council	of the	City	of Rose	ville	does	ordai	n

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- **Section 1. PUD Cancellation Findings.** Pursuant to §1023.11 (PUD Cancellation) of the City Zoning Code of the City of Roseville, and after City Council consideration of PF21-012, the City Council makes the following findings in regards to its decision to cancel Planned Unit Development #1364, located at 1201 Larpenteur Avenue:
- The zoning designation for 1201 Larpenteur Avenue has been updated since PUD #1364 was established and the current zoning allows improvements and redevelopment of the subject property in a manner that is more consistent with development goals and objectives outlined within City's Comprehensive Plan.
- PUD #1364 has been established for over 13.5 years, yet the full build-out consistent with PUD #1364 has never been realized, suggesting the entitlements within the PUD are no longer relevant to the immediate market area.
- As development/redevelopment occurs on the subject property consistent with current
 City zoning, without cancellation of PUD #1364 an amendment may be necessary, the
 process of which is over-burdensome and adds unnecessary and additional steps to the
 approval process.
- Any future/proposed expansion, development, or redevelopment plans on the subject property must comply with the current, underlying Community Business zoning designation, Corridor Mixed-Use zoning designation (if/when applicable), apply for a new Planned Unit Development in accordance with City Code Section 1023.10, and/or apply for variances to underlying zoning regulations should the applicant identify a practical difficulty preventing compliance with underlying zoning regulations.
 - **Section 2. PUD legal description.** The subject property addressed at 1201 Larpenteur Avenue, and applying to PUD #1364 is legally described in Attachment A and is the property effected by the cancellation.
- Section 3. PUD Cancellation. The Roseville City Council does hereby ordain that PUD #1364 established on April 14, 2008 for 1201 Larpenteur Avenue is hereby cancelled.
- Section 4. Effective Date. This Planned Unit Development Cancellation ordinance shall take effect upon:
 - 1. The passage and publication of this ordinance.
- 31 Passed this 25th day of October 2021.

REQUEST FOR PLANNING COMMISSION ACTION

Agenda Date: 11/03/21 Agenda Item: 6b

Public Hearings

Prepared By Agenda Section

Department Approval

Janue Gundiach

Item Description: Consider a Request by Crown Castle (in cooperation with property owner

LAMN, LLC) for a Conditional Use to allow modification of

telecommunication devices on a pre-existing non-conforming lattice tower

at 2420 County Road C (PF21-016)

1 APPLICATION INFORMATION

2 Applicant: Crown Castle

3 Location: 2420 County Road C

4 Application Submission: 09/22/21; deemed complete 10/07/21

5 City Action Deadline: November 20, 2021; extended 60-days to

January 19, 2022

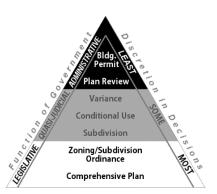
7 Planning File History: None

8 Zoning: Industrial District

- 9 LEVEL OF DISCRETION IN DECISION MAKING: Action taken on a
- conditional use proposal is quasi-judicial; the City's role is to
- determine the facts associated with the request, and apply those facts
- to the legal standards contained in State Statute and City Code.
- 13 BACKGROUND

6

- 14 Crown Castle desires to install Dish Wireless antennas on the pre-
- existing non-conforming lattice design telecommunication tower at
- 16 2420 County Road C. This particular tower was constructed in 1988,
- is 180 feet tall, and includes three existing carriers and various ground
- equipment (some contained in a 360 square foot building). In order
- 19 for Dish to be approved as a carrier on this tower, an approved Conditional Use per
- 20 §1011.12.G.1.h and d (cited below) is required.
- 21 h. Existing Facilities: Existing transmitting and receiving facilities at the time of the adoption of
- 22 this Section may remain in service. However, at such time as any material change is made in the
- 23 facilities, full compliance with this Section shall be required. No transmitting or receiving
- 24 antennas or towers may be added to existing nonconforming facilities. Towers and receiving
- 25 facilities shall be dismantled and removed from the site within 1 year after abandonment of the
- use of the tower or facility for communication purposes.



- 27 d. Commercial Antennas and Towers Non-City Sites: Commercial receiving or transmitting
- antennas and towers not located on a City tower site shall be a conditional use. Commercial
- 29 receiving or transmitting antennas and towers may only be located in Commercial, Community
- 30 Mixed Use and/or Employment Districts. The City may establish permit review periods, tower
- termination, time limits or an amortization schedule specifying the year in which the tower shall
- be taken down by the applicant or assign. A performance bond or other surety may be required
- 33 by the City in order to assure removal of the tower at a specific date.
- 34 As stated previously, this is a pre-existing non-conforming telecommunication tower. The
- installation of this tower was completed prior to code regulations requiring an approved CU.
- 36 The City does have a number of telecommunication sites, however, most of those are considered
- 37 City-owned sites, such as the two monopoles on the City Hall Campus. City-owned antennas
- and towers are permitted uses in Commercial, Community Mixed Use, and Employment
- 39 Districts (1011.12.G.1.b).
- 40 The Planning Division has included the project narrative and other documents pertaining to the
- existing tower and required installation of antenna and ground equipment (Attachment C).
- 42 CONDITIONAL USE ANALYSIS
- 43 REVIEW OF GENERAL CONDITIONAL USE CRITERIA: Section 1009.02.C of the Zoning Code
- establishes general standards and criteria for all conditional uses, which the Planning
- 45 Commission and City Council must determine compliance with those stated findings.
- The general code standards of §1009.02.C are as follows:
- **a.** The proposed use is not in conflict with the Comprehensive Plan. While a telecommunication
- facility doesn't appreciably advance the goals of the Comprehensive Plan aside from
- 49 facilitating continued investment in a property, Planning Division staff believes it does not
- 50 conflict with the Comprehensive Plan either. More specifically, the General and Commercial
- Area Goals and Policies sections of the Comprehensive Plan include a number of policies
- related to reinvestment, redevelopment, quality development, and scale. The proposed
- 53 modifications to the existing tower (upgrading and enhancing the Dish network) will create a
- more robust 911 service, which would align with the related goals and polices of the
- 55 Comprehensive Plan.
- **b.** The proposed use is not in conflict with a Regulating Map or other adopted plan. The subject
- 57 property is not located in an area that is controlled by a regulating plan or other adopted plan
- and so this standard is not applicable.
- 59 **c.** *The proposed use is not in conflict with any City Code requirements.* Although the tower
- predates the Zoning Code's conditional use requirement, Cellular One did receive the
- 61 necessary permit in 1988 to initially construct the tower, install its necessary antennas, and to
- place equipment on the ground. Most towers of this type (lattice) are designed for co-
- location and this tower currently supports three telecommunication providers (AT&T
- Mobility, Choctaw Telecommunications LLC, and Landis and Gyr Technology Inc.); Dish
- would be the fourth. The tower has a height of 180 feet, with the Code not limiting height.
- Section §1011.12.G.1.f, and j regulates the required front yard setback and the size of the
- equipment shelter respectively. Planning Division staff determined the tower complies with
- all dimensional requirements, however the pre-existing equipment structure is larger than
- 69 what is currently allowed by Code and is, therefore, grandfathered-in. All other standards
- appear to be acceptable.

- 71 d. The proposed use will not create an excessive burden on parks, streets, and other public
 72 facilities. A telecommunication tower, such as the one erected on the Lube Tech site, has a
 73 negligible impact on public facilities. Staff does not anticipate the proposal to have a
 74 practical impact, or otherwise intensify use of parks, streets, or other public infrastructure.
- **e.** The proposed use will not be injurious to the surrounding neighborhood, will not negatively 75 impact traffic or property values, and will not otherwise harm the public health, safety, and 76 77 general welfare. This specific telecommunication tower has been in place since 1988 without any incident or appreciable negative impact. The Planning Division concludes the existing 78 79 telecommunication tower is not injurious to the surrounding neighborhood, nor would it be after a fourth provider is added to the tower. Further, the tower currently does not negatively 80 impact traffic or property values, or otherwise harm the public health, safety, and general 81 welfare. Lastly, installation of a fourth provider to the tower will not cause any further 82 impacts to the site and/or immediate neighborhood. 83

84 PLANNING DIVISION RECOMMENDATION

- 85 The Planning Division recommends approval of a Conditional Use permitting installation of
- additional telecommunication antenna to an existing tower at 2420 County Road C (LubeTech),
- based on the comments and findings noted in the RPCA dated 11/03/21 and subject to the
- 88 following conditions:
- a. The tower shall be limited to 180 feet in height;
- b. The existing ground equipment structure shall not increase in size without an amendment tothe CU;
- 92 **c.** Dish Wireless shall be permitted a 7 foot by 8 foot ground equipment platform adjacent the tower and within the secure tower/equipment area.

94 SUGGESTED PLANNING COMMISSION ACTION

- 95 By motion, recommend approval of a CONDITIONAL USE for 2420 County Road C, permitting
- 96 installation of additional telecommunication antenna to an existing tower on the subject property
- based on the comments, findings, and three conditions stated in this report.

98 ALTERNATIVE ACTIONS

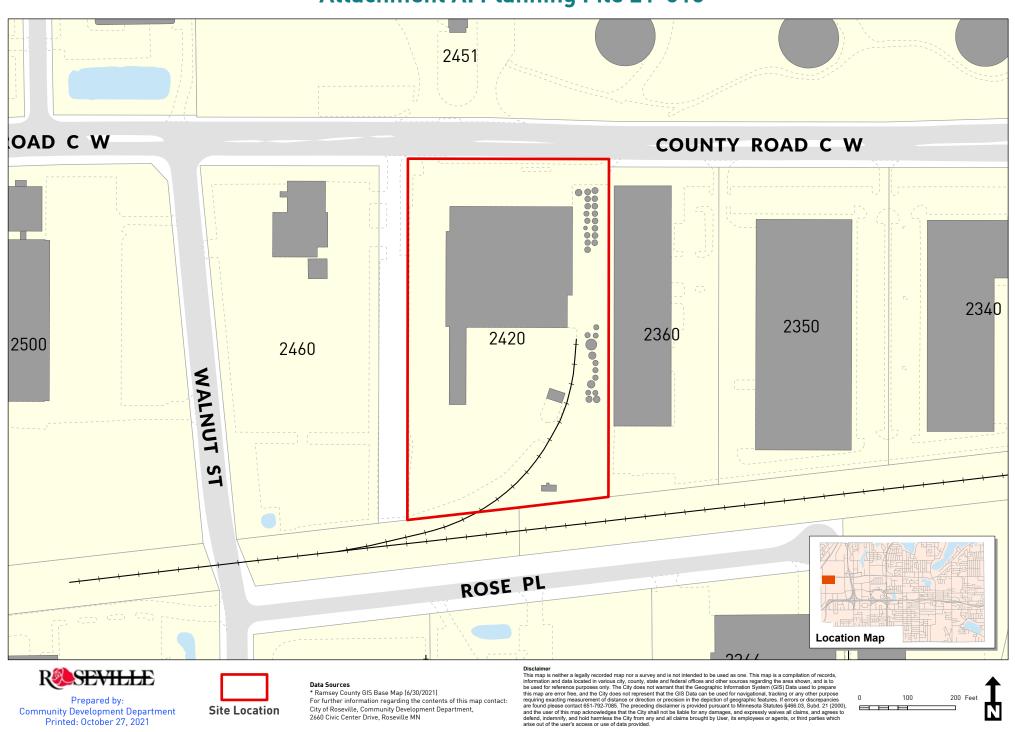
- a. Pass a motion to table the item for future action. An action to table must be tied to the need
 for clarity, analysis, and/or information necessary to make a recommendation on the request.
- b. Pass a motion recommending denial of the proposal. A motion to deny must include findings
 of fact germane to the request.

Report prepared by: Thomas Paschke, City Planner, 651-792-7074 | thomas.paschke@cityofroseville.com

Attachments: A. Location Map B. Aerial photo

C. Narrative/plans

Attachment A: Planning File 21-016



Community Development Department Printed: October 27, 2021

Site Location

City of Roseville, Community Development Department,

2660 Civic Center Drive, Roseville MN

Attachment B: Planning File 21-016





Prepared by: Community Development Department Printed: October 27, 2021

Data Sources

- * Ramsey County GIS Base Map (9/30/2021)
- * Aerial Data: Surdex (4/2020)

For further information regarding the contents of this map contact: City of Roseville, Community Development Department, 2660 Civic Center Drive, Roseville MN

disclaimer

This map is neither a legally recorded map nor a survey and is not intended to be used as one. This map is a compilation of records, information and data located in various city, county, state and federal offices and other sources regarding the area shown, and is to be used for reference purposes only. The City does not warrant that the Geographic Information System (ISI) Data used to prepare this map are error free, and the City does not represent that the GIS Data can be used for navigational, tracking or any other purpose requiring exacting measurement of distance or direction or precision in the depiction of geographic features. If errors or discrepancies are found please contact 61:792-7085. The preceding disclaimer is provided pursuant to Minnesota Statutes §466.03, Subd. 21 (2001), and the user of his map exkonveloges that the City shall not be liable for any damages, and expressly wweeks all claims, and agrees to define the control of the contr





CONDITIONAL USE APPLICATION

Written Narrative for Conditional Use Application for 2420 WEST COUNTY ROAD C

The proposed DISH Wireless application for the telecommunication tower located at: 2420 West County Road C, is in full compliance with the City of Roseville Code (Section 1009.02.C). The project proposes to collocate DISH antennas on the existing tower and ground equipment within the existing leased area. Please see the following scope of work:

Dish 5G- Dish proposes to add (3) antennas, (6) RRU, (1) OVP and (1) hybrid. Dish will have a 5'x7' lease area with (1) cabinet.

This site is in operation 24/7 and serves an essential function for the local community: residents, businesses, first responders and a complex network of existing neighboring cell sites. This facility will allow coverage for 911 calls from wireless devices in and around the area.

wireless

DISH Wireless L.L.C. SITE ID:

MNMSP00115A

DISH Wireless L.L.C. SITE ADDRESS:

2420 WEST COUNTY ROAD C **ROSEVILLE, MN 55113**

MINNESOTA CODE COMPLIANCE

ALL WORK SHALL BE PERFORMED AND MATERIALS INSTALLED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE FOLLOWING CODES AS ADOPTED BY THE LOCAL GOVERNING AUTHORITIES. NOTHING IN THESE PLANS IS TO BE CONSTRUED TO PERMIT WORK NOT CONFORMING TO THESE CODES:

CODE TYPE

2015 MINNESOTA BUILDING CODE/2015 IBC BUILDING

2015 MINNESOTA MECHANICAL AND FUEL GAS CODE/2015 IMC **MECHANICAL** 2015 MINNESOTA ELECTRICAL CODE/2014 NEC ELECTRICAL

	SHEET INDEX					
SHEET NO.	HEET NO. SHEET TITLE					
T-1	TITLE SHEET					
A-1	OVERALL AND ENLARGED SITE PLAN					
A-2	ELEVATION, ANTENNA LAYOUT AND SCHEDULE					
A-3	EQUIPMENT PLATFORM AND H-FRAME DETAILS					
A-4	EQUIPMENT DETAILS					
A-5	EQUIPMENT DETAILS					
A-6	EQUIPMENT DETAILS					
E-1	ELECTRICAL/FIBER ROUTE PLAN AND NOTES					
E-2	ELECTRICAL DETAILS					
E-3	ELECTRICAL ONE-LINE, FAULT CALCS & PANEL SCHEDULE					
G-1	GROUNDING PLANS AND NOTES					
G-2	GROUNDING DETAILS					
G-3	GROUNDING DETAILS					
RF-1	RF CABLE COLOR CODE					
GN-1	LEGEND AND ABBREVIATIONS					
GN-2	GENERAL NOTES					
GN-3	GENERAL NOTES					
GN-4	GENERAL NOTES					

SCOPE OF WORK

THIS IS NOT AN ALL INCLUSIVE LIST. CONTRACTOR SHALL UTILIZE SPECIFIED EQUIPMENT PART OR ENGINEER APPROVED EQUIVALENT. CONTRACTOR SHALL VERIFY ALL NEEDED EQUIPMENT TO PROVIDE A FUNCTIONAL SITE. THE PROJECT GENERALLY CONSISTS OF THE FOLLOWING:

TOWER SCOPE OF WORK:

- INSTALL (3) PROPOSED PANEL ANTENNAS (1 PER SECTOR)
 INSTALL (3) PROPOSED ANTENNA MOUNTS (1 PER SECTOR)
- INSTALL PROPOSED JUMPERS
- INSTALL (2) PROPOSED RRHs (2 PER SECTOR)
- INSTALL (1) PROPOSED OVER VOLTAGE PROTECTION DEVICE (OVP) INSTALL (1) PROPOSED HYBRID CABLE

- **GROUND SCOPE OF WORK:** INSTALL (1) PROPOSED METAL PLATFORM
- INSTALL (1) PROPOSED PPC CABINET
- INSTALL (1) PROPOSED EQUIPMENT CABINET
- INSTALL (1) PROPOSED POWER CONDUIT
- INSTALL (1) PROPOSED TELCO CONDUIT
- INSTALL (1) PROPOSED TELCO-FIBER BOX
- INSTALL (1) PROPOSED GPS UNIT
- INSTALL (1) PROPOSED SAFETY SWITCH
- INSTALL (1) PROPOSED METER ENCLOSURE
- INSTALL (1) PROPOSED FIBER NID (IF REQUIRED)

SITE PHOTO





GOPHER STATE ONE CALL UTILITY NOTIFICATION CENTER OF MINNESOTA (800) 252-1166 WWW.GOPHERSTATEONECALL.ORG

CALL 2 WORKING DAYS UTILITY NOTIFICATION PRIOR TO CONSTRUCTION

GENERAL NOTES

THE FACILITY IS UNMANNED AND NOT FOR HUMAN HABITATION. A TECHNICIAN WILL VISIT THE SITE AS REQUIRED FOR ROUTINE MAINTENANCE. THE PROJECT WILL NOT RESULT IN ANY SIGNIFICANT DISTURBANCE OR EFFECT ON DRAINAGE. NO SANITARY SEWER SERVICE, POTABLE WATER, OR TRASH DISPOSAL IS REQUIRED AND NO COMMERCIAL SIGNAGE IS PROPOSED.

11"x17" PLOT WILL BE HALF SCALE UNLESS OTHERWISE NOTED

CONTRACTOR SHALL VERIFY ALL PLANS, EXISTING DIMENSIONS, AND CONDITIONS ON THE JOB SITE, AND SHALL IMMEDIATELY NOTIFY THE ENGINEER IN WRITING OF ANY DISCREPANCIES BEFORE PROCEEDING WITH THE WORK.

APPLICANT: DISH Wireless L.L.C. PROPERTY OWNER: LAMN LLC 5701 SOUTH SANTA FE DRIVE ADDRESS: 900 MENDELSSOHN AVE N GOLDEN VALLEY, MN 55427 LITTLETON, CO 80120 SELF SUPPORT TOWER TYPE: TOWER OWNER: CROWN CASTLE 2000 CORPORATE DRIVE TOWER CO SITE ID: CANONSBURG, PA 15317 TOWER APP NUMBER: 562282 (303) 274-6568 SITE DESIGNER: GPD GROUP, INC. COUNTY: RAMSEY 520 S MAIN ST, SUITE 2534 AKRON, OH 44311 LATITUDE (NAD 83): 45° 01' 07.1" N 45.018650 N (330) 572-2100 LONGITUDE (NAD 83): 93° 12' 08.6" W 93.202383 W ZONING JURISDICTION: CITY OF ROSEVILLE SITE ACQUISITION: ZACHARY KELLY (724) 416-9958 **ZONING DISTRICT:** CONSTRUCTION MANAGER: BRANDON RAYBERN PARCEL NUMBER: 082923220001 (303) 706-5947 OCCUPANCY GROUP: CONSTRUCTION TYPE: RF ENGINEER: CHONG LEE POWER COMPANY: XCEL ENERGY TELEPHONE COMPANY: AT&T

PROJECT DIRECTORY

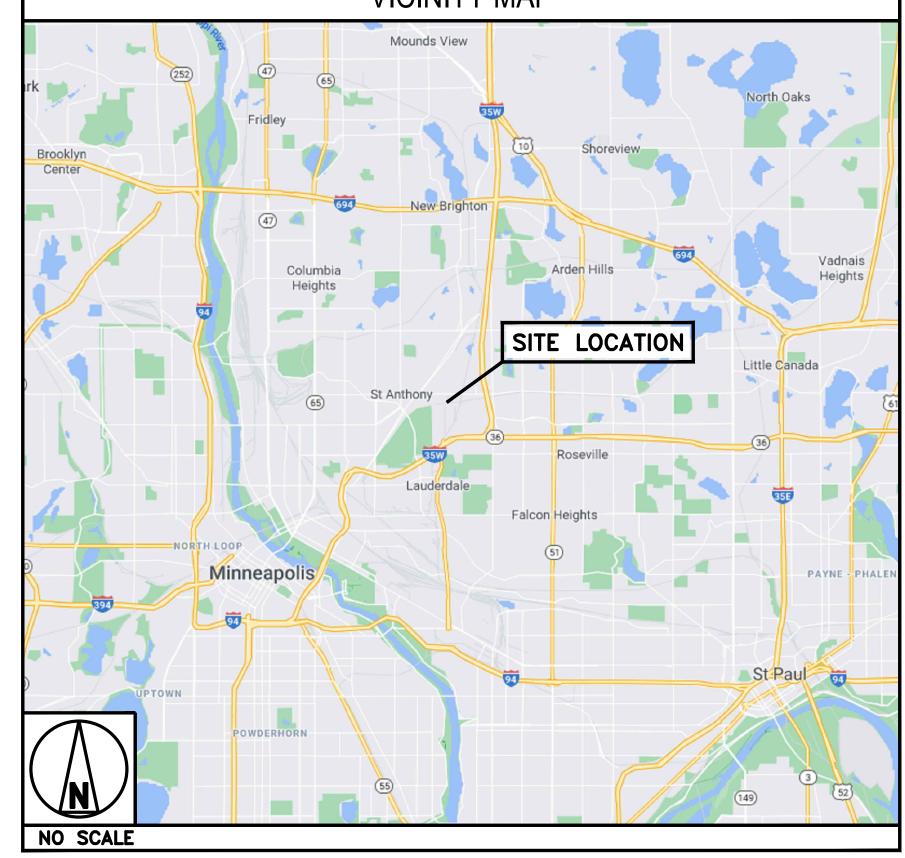
SITE INFORMATION

DIRECTIONS

DIRECTIONS FROM MINNEAPOLIS-SAINT PAUL INTERNATIONAL AIRPORT:

GET ON MN-5 E FROM GLUMACK DR. HEAD SOUTHWEST. CONTINUE ONTO GREEN LN. SLIGHT RIGHT ONTO GLUMACK DR. USE THE LEFT 2 LANES TO STAY ON GLUMACK DR. KEEP LEFT AT THE FORK AND MERGE ONTO MN-5 E. MERGE ONTO MN-5 E. USE THE RIGHT 2 LANES TO TAKE THE MN-55 W/MN-55 E EXIT TOWARD MINNEAPOLIS FORT SNELLING/HASTINGS. KEEP LEFT AT THE FORK, FOLLOW SIGNS FOR MN-55 W AND MERGE ONTO MN-55 W/MN-62 W. CONTINUE TO FOLLOW MN-62 W. TAKE THE EXIT ONTO I-35W N. KEEP RIGHT AT THE FORK TO STAY ON I-35W N. KEEP LEFT TO STAY ON I-35W N. TAKE EXIT 22 FOR INDUSTRIAL BLVD TOWARD MN 280. USE THE LEFT LANE TO KEEP LEFT AT THE FORK AND FOLLOW SIGNS FOR ST ANTHONY BLVD. TAKE WALNUT ST TO ROSE PL W IN ROSEVILLE. TURN LEFT ONTO INDUSTRIAL BLVD NE. TURN RIGHT ONTO WALNUT ST. TURN RIGHT ONTO ROSE PL W. DESTINATION WILL BE ON THE LEFT.

VICINITY MAP





5701 SOUTH SANTA FE DRIVE LITTLETON, CO 80120



520 South Main Street, Suite 2531 Akron, OH 44311 330.572.2100 Fax 330.572.2101

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota Signature:

Typed or Printed Name: CHRISTOPHER J. SCHEKS Date: 08/23/2021License Number: 51499

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT.

LYN CJM	
LIN	I MRL

RFDS REV #:

CONSTRUCTION **DOCUMENTS**

SUBMITTALS						
REV	DATE	DESCRIPTION				
A	07/08/2021	ISSUED FOR REVIEW				
0	08/23/2021	ISSUED FOR REVIEW				
	A&E PROJECT NUMBER					
	2021724.75.844376.01					

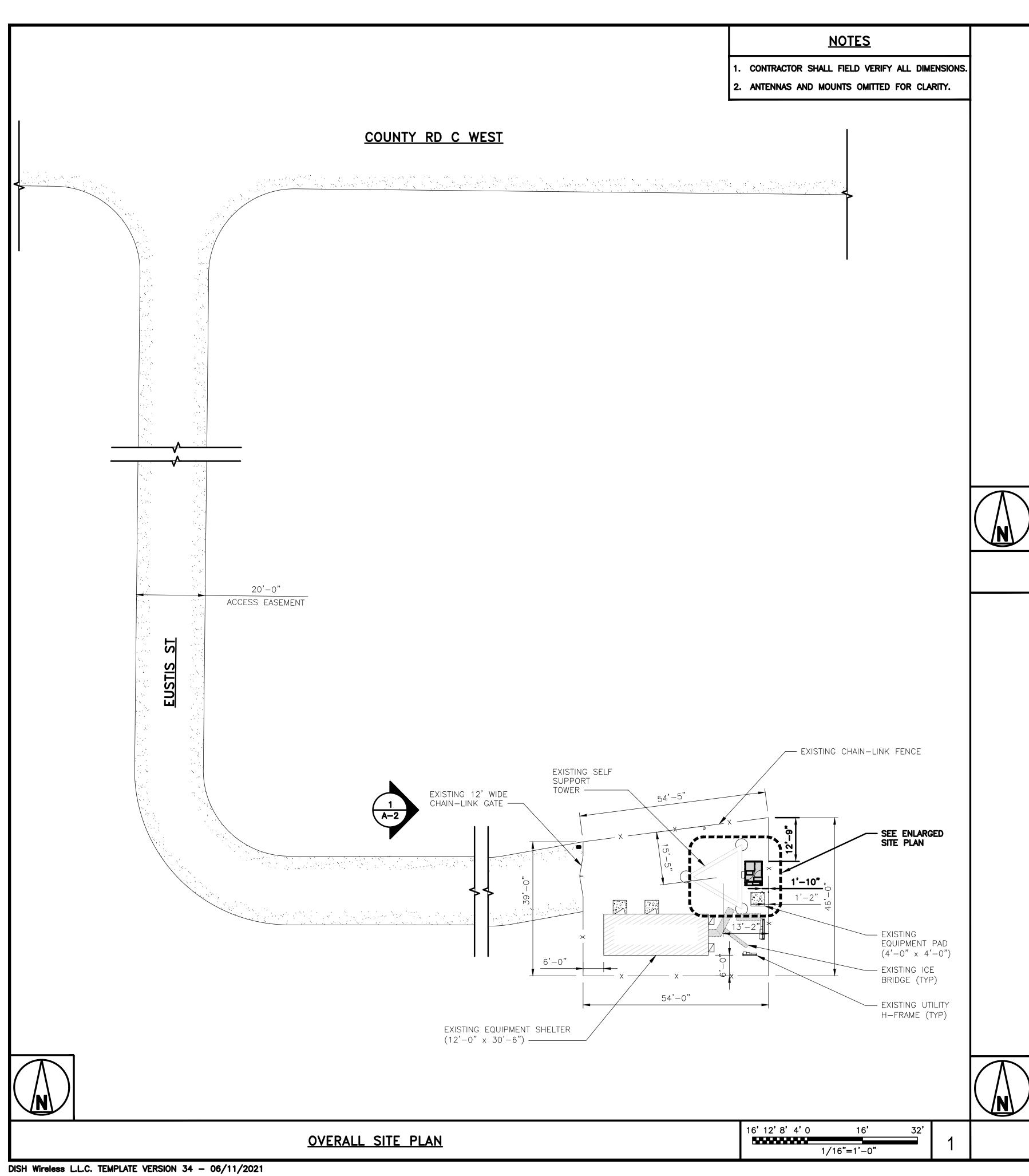
DISH Wireless L.L.C. PROJECT INFORMATION

MNMSP00115A 2420 WEST COUNTY ROAD C ROSEVILLE, MN 55113

> SHEET TITLE TITLE SHEET

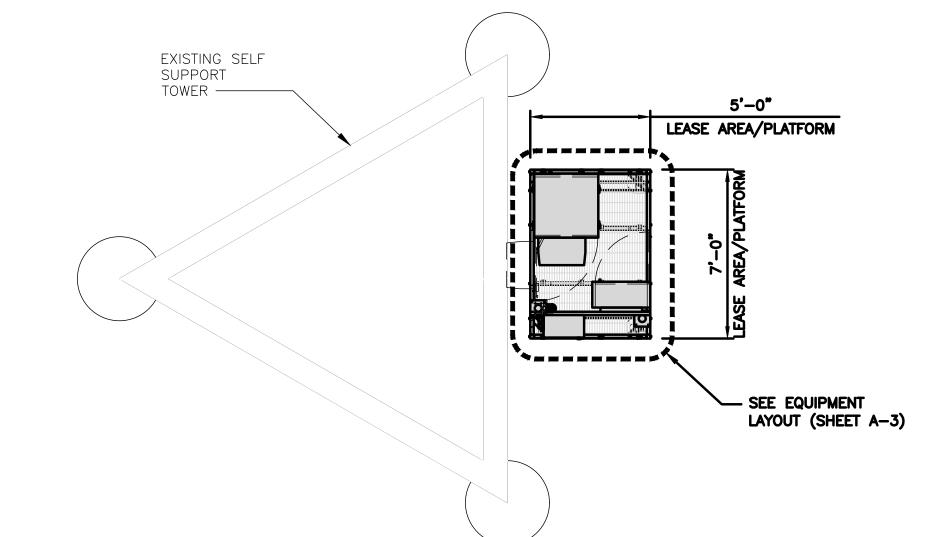
SHEET NUMBER

T-1



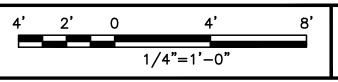
NOTES

- CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS.
- 2. CONTRACTOR SHALL MAINTAIN A 10'-0" MINIMUM SEPARATION BETWEEN THE PROPOSED GPS UNIT, TRANSMITTING ANTENNAS AND EXISTING GPS UNITS.
- 3. ANTENNAS AND MOUNTS OMITTED FOR CLARITY.



ENLARGED SITE PLAN

990 3 300 3 30



IT IS A VIOLATION OF LAW FOR ANY PERSON,
UNLESS THEY ARE ACTING UNDER THE DIRECTION
OF A LICENSED PROFESSIONAL ENGINEER,
TO ALTER THIS DOCUMENT.

	DRAWN BY:		CHECKED	BY:	APPROVED	BY:
	LYN		СЈМ		MRL	
	RFDS F	REV :	#:			

wireless.

5701 SOUTH SANTA FE DRIVE

LITTLETON, CO 80120

GPD GROUP, INC.

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws

of the state of Minnesota Signature:

Typed or Printed Name: CHRISTOPHER J. SCHEKS

Date: 08/23/2021License Number: 51499

520 South Main Street, Suite 2531 Akron, OH 44311 330.572.2100 Fax 330.572.2101

CONSTRUCTION DOCUMENTS

ı	SUBMITTALS						
	REV	DATE	DESCRIPTION				
	A	07/08/2021	ISSUED FOR REVIEW				
	0	08/23/2021	ISSUED FOR REVIEW				
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ı							
ı		A&E F	PROJECT NUMBER				
		2021724.75.844376.01					

DISH Wireless L.L.C. PROJECT INFORMATION

MNMSP00115A 2420 WEST COUNTY ROAD C ROSEVILLE, MN 55113

SHEET TITLE

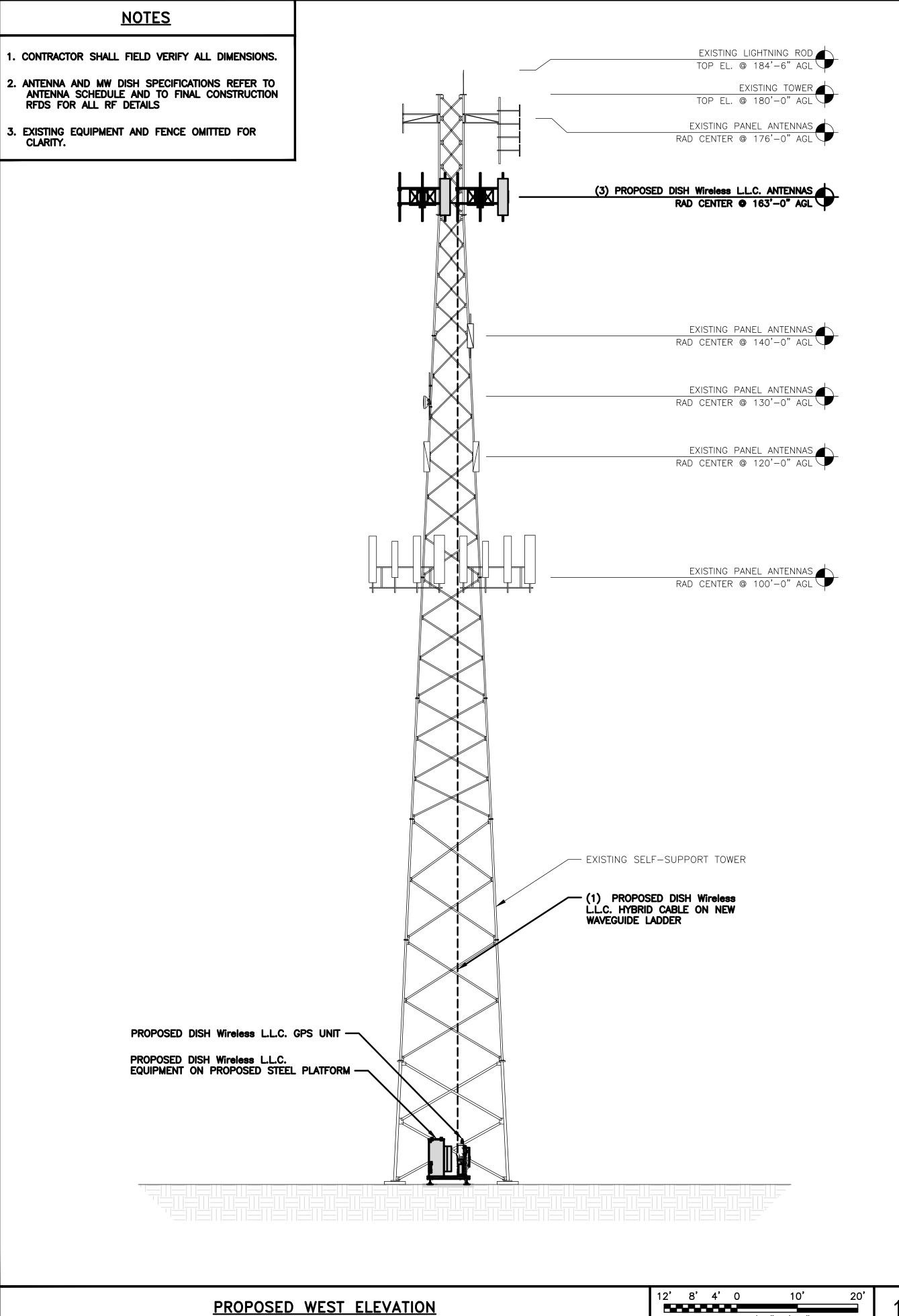
OVERALL AND ENLARGED SITE PLAN

SHEET NUMBER

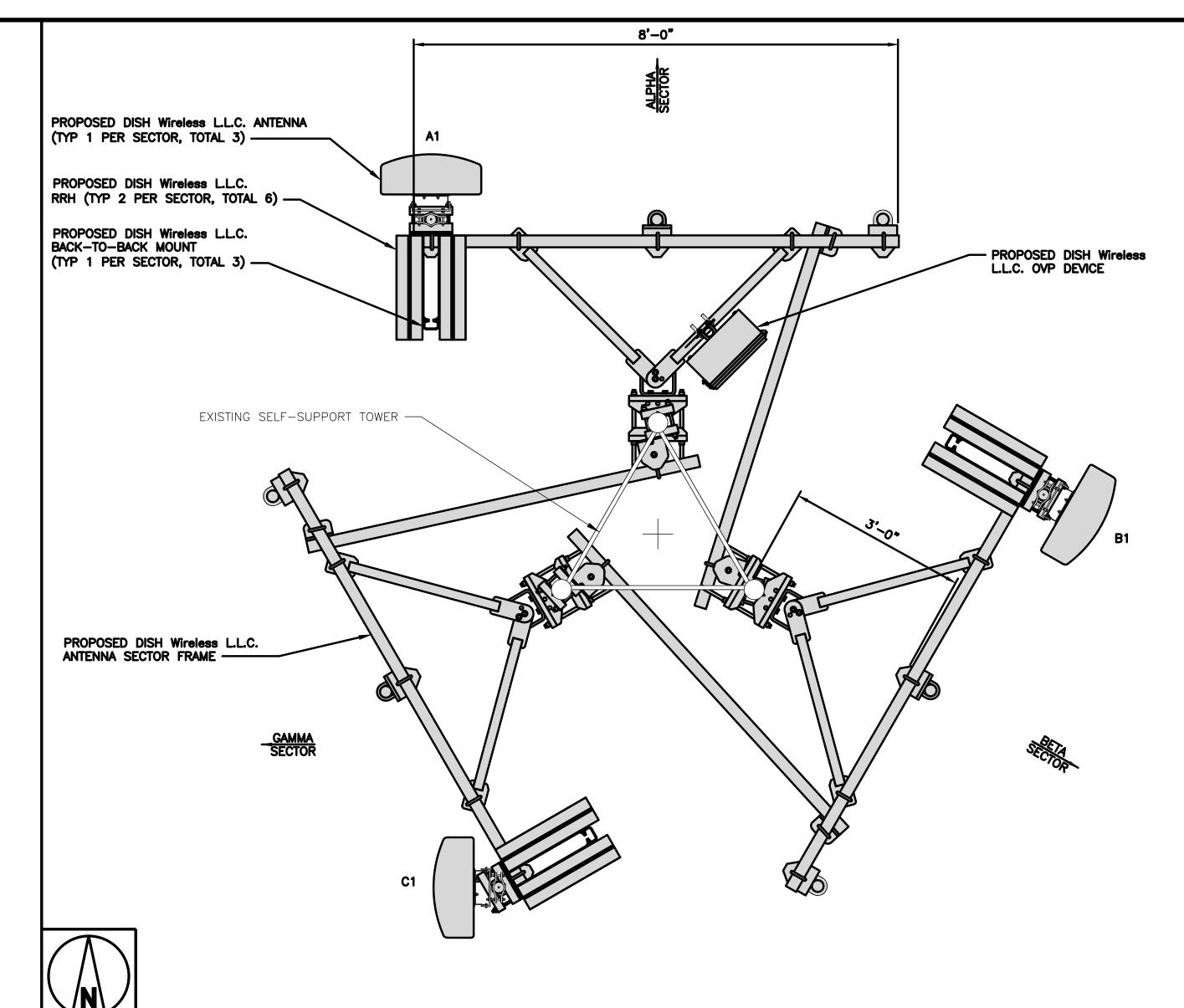
A-1

AERIAL VIEW

PROPOSED FIBER HAND HOLE



3/32"=1'-0"



				AN	ITENNA				TRANSMISSION CABLE
SECTOR	POSITION	EXISTING OR PROPOSED	MANUFACTURER — MODEL NUMBER		TECHNOLOGY	SIZE (HxW)	AZIMUTH	RAD CENTER	FEED LINE TYPE AND LENGTH
ALPHA	A1	PROPOSED	JMA - MX08FR0665-21		5G	72.0" × 20.0"	o	163'-0"	(4) HIGH OADAOITY
BETA	B1	PROPOSED	JMA - MX08FR0665-21		5G	72.0" × 20.0"	120°	163'-0"	(1) HIGH-CAPACITY HYBRID CABLE (195' LONG)
GAMMA	C1	PROPOSED	JMA - MX08FR0665-21		5G	72.0" × 20.0"	270°	163'-0"	(193 LONG)
		RRH			NOTES				
SECTOR	POSITION		RER - MODEL TECHNOLOGY			TOR TO REFER	TO FINAL	CONSTRUCT	ION RFDS FOR ALL RF
ALPHA	A1	Fujitsu — 1	TA08025-B604 5G TA08025-B605 5G		DETAILS.	AND BBU MOD	EIC MAY (NUANCE DI	E TO FOURDMENT
ALPHA	A1	Fujitsu — 1			availabii		MENT CHAN	IGES MUST	E TO EQUIPMENT BE APPROVED AND DESIGN AND
	B1	FUJITSU - TA08025-B604		5G		RAL ANALYSES.			223.3.7
DETA		FUJITSU - TA08025-B605 5G							
BETA	B1	Fujitsu — 1	TA08025-B605	5G					
BETA GAMMA	B1 C1		TA08025-B605	5G 5G					

ANTENNA LAYOUT



5701 SOUTH SANTA FE DRIVE LITTLETON, CO 80120



520 South Main Street, Suite 2531 Akron, OH 44311 330.572.2100 Fax 330.572.2101

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota Signature:

Typed or Printed Name: CHRISTOPHER J. SCHEKS Date: 08/23/2021License Number: 51499

IT IS A VIOLATION OF LAW FOR ANY PERSON,
UNLESS THEY ARE ACTING UNDER THE DIRECTION
OF A LICENSED PROFESSIONAL ENGINEER,
TO ALTER THIS DOCUMENT.

DRAWN BY: CHECKED BY: APPROVED BY: LYN

RFDS REV #:

3/4"=1'-0"

CONSTRUCTION DOCUMENTS

SUBMITTALS						
REV	DATE	DESCRIPTION				
A	07/08/2021	ISSUED FOR REVIEW				
0	08/23/2021	ISSUED FOR REVIEW				
	A&E PROJECT NUMBER					
	2021724.75.844376.01					

DISH Wireless L.L.C. PROJECT INFORMATION

MNMSP00115A 2420 WEST COUNTY ROAD C ROSEVILLE, MN 55113

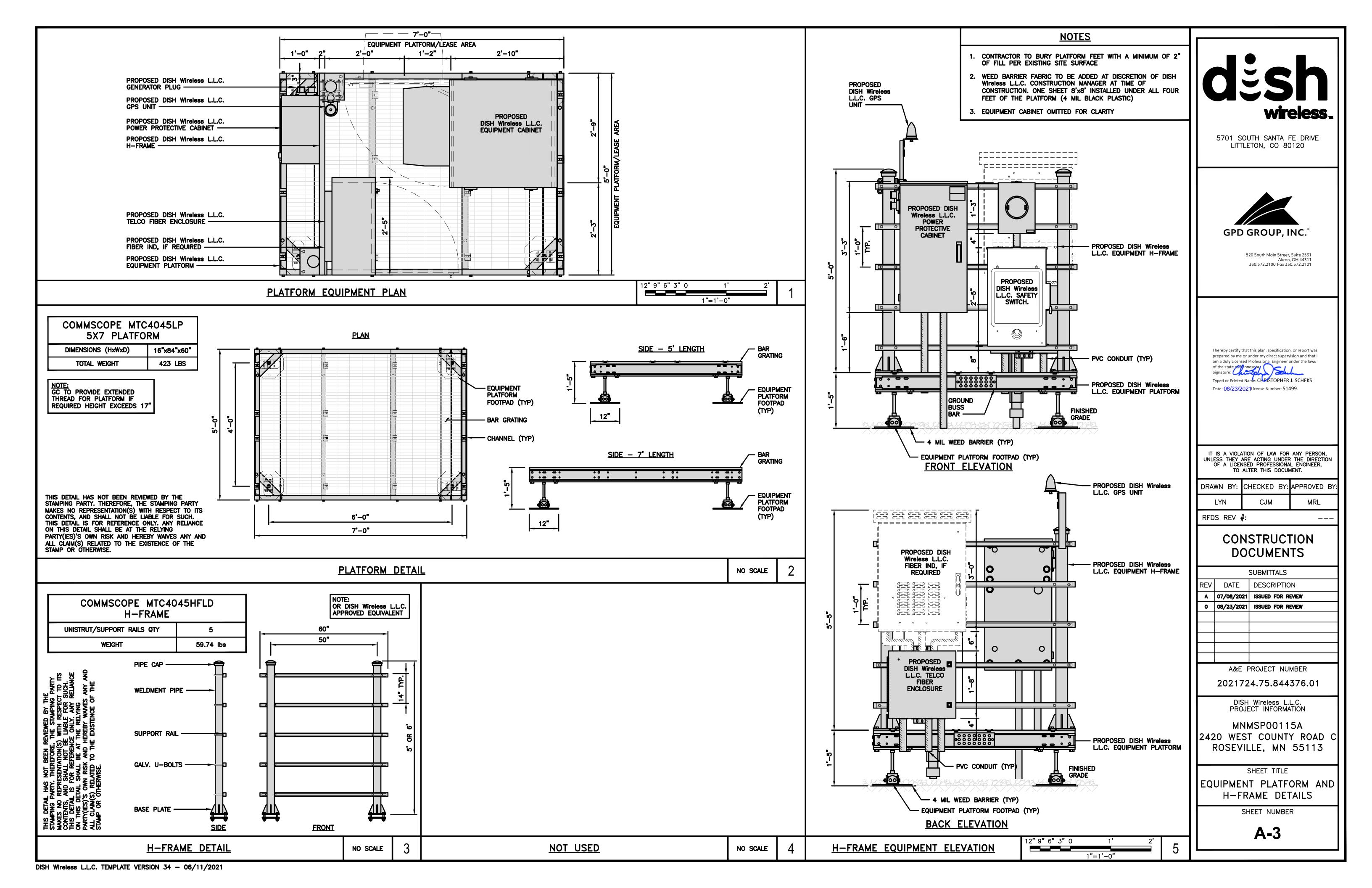
SHEET TITLE ELEVATION, ANTENNA LAYOUT AND SCHEDULE

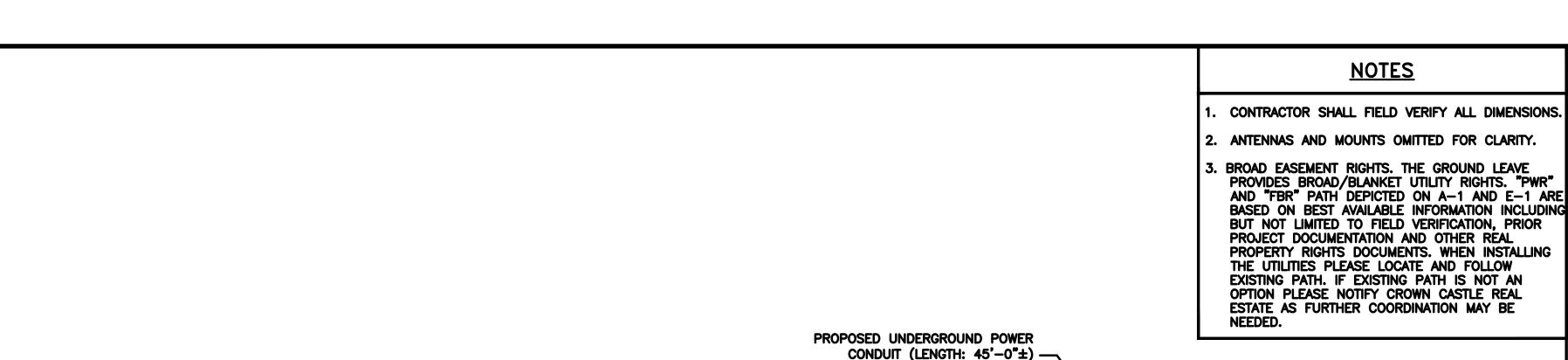
SHEET NUMBER

A-2

ANTENNA SCHEDULE

NO SCALE





PROPOSED UNDERGROUND

FIBER CONDUIT

(LENGTH: 75'-0"±) -

UTILITY ROUTE PLAN

PROPOSED DISH WIRELESS, LLC 200A, 120/240V, 1PH,

3W METER & DISCONNECT

EXISTING SELF SUPPORT

TOWER -

EXISTING EQUIPMENT SHELTER (12'-0" x 30'-6")

ON EXISTING POST -

DC POWER WIRING SHALL BE COLOR CODED AT EACH END FOR IDENTIFYING +24V AND -48V CONDUCTORS. RED MARKINGS SHALL IDENTIFY +24V AND BLUE MARKINGS SHALL IDENTIFY -48V.

- . CONTRACTOR SHALL INSPECT THE EXISTING CONDITIONS PRIOR TO SUBMITTING A BID. ANY QUESTIONS ARISING DURING THE BID PERIOD IN REGARDS TO THE CONTRACTOR'S FUNCTIONS, THE SCOPE OF WORK, OR ANY OTHER ISSUE RELATED TO THIS PROJECT SHALL BE BROUGHT UP DURING THE BID PERIOD WITH THE PROJECT MANAGER FOR CLARIFICATION, NOT AFTER THE CONTRACT HAS BEEN AWARDED.
- 2. ALL ELECTRICAL WORK SHALL BE DONE IN ACCORDANCE WITH CURRENT NATIONAL ELECTRICAL CODES AND ALL STATE AND LOCAL CODES, LAWS, AND ORDINANCES. PROVIDE ALL COMPONENTS AND WIRING SIZES AS REQUIRED TO MEET NEC STANDARDS.
- 3. LOCATION OF EQUIPMENT, CONDUIT AND DEVICES SHOWN ON THE DRAWINGS ARE APPROXIMATE AND SHALL BE COORDINATED WITH FIELD CONDITIONS PRIOR TO CONSTRUCTION.
- 4. CONDUIT ROUGH-IN SHALL BE COORDINATED WITH THE MECHANICAL EQUIPMENT TO AVOID LOCATION CONFLICTS. VERIFY WITH THE MECHANICAL EQUIPMENT CONTRACTOR AND COMPLY AS REQUIRED.
- 5. CONTRACTOR SHALL PROVIDE ALL BREAKERS, CONDUITS AND CIRCUITS AS REQUIRED FOR A COMPLETE SYSTEM.
- 6. CONTRACTOR SHALL PROVIDE PULL BOXES AND JUNCTION BOXES AS REQUIRED BY THE NEC ARTICLE 314.
- 7. CONTRACTOR SHALL PROVIDE ALL STRAIN RELIEF AND CABLE SUPPORTS FOR ALL CABLE ASSEMBLIES. INSTALLATION SHALL BE IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.
- 8. ALL DISCONNECTS AND CONTROLLING DEVICES SHALL BE PROVIDED WITH ENGRAVED PHENOLIC NAMEPLATES INDICATING EQUIPMENT CONTROLLED, BRANCH CIRCUITS INSTALLED ON, AND PANEL FIELD LOCATIONS FED FROM.
- 9. INSTALL AN EQUIPMENT GROUNDING CONDUCTOR IN ALL CONDUITS PER THE SPECIFICATIONS AND NEC 250. THE EQUIPMENT GROUNDING CONDUCTORS SHALL BE BONDED AT ALL JUNCTION BOXES, PULL BOXES, AND ALL DISCONNECT SWITCHES. AND EQUIPMENT CABINETS.
- 10. ALL NEW MATERIAL SHALL HAVE A U.L. LABEL.
- 11. PANEL SCHEDULE LOADING AND CIRCUIT ARRANGEMENTS REFLECT POST—CONSTRUCTION EQUIPMENT.
- 12. CONTRACTOR SHALL BE RESPONSIBLE FOR AS-BUILT PANEL SCHEDULE AND SITE DRAWINGS.
- 13. ALL TRENCHES IN COMPOUND TO BE HAND DUG

am a duly Licensed Professional Engineer under the laws of the state of Minnes Ita.
Signature.

Typed or Printed Name: STEVEN P. SCHAUB Date: 08/23/2021License Number: 50426 NO SCALE

> TO ALTER THIS DOCUMENT. DRAWN BY: CHECKED BY: APPROVED BY:

RFDS REV #:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER,

5701 SOUTH SANTA FE DRIVE

LITTLETON, CO 80120

GPD GROUP, INC.

I hereby certify that this plan, specification, or report was

prepared by me or under my direct supervision and that I

520 South Main Street, Suite 2531 Akron, OH 44311

330.572.2100 Fax 330.572.2101

CONSTRUCTION **DOCUMENTS**

SUBMITTALS DATE DESCRIPTION A 07/08/2021 ISSUED FOR REVIEW 0 08/23/2021 ISSUED FOR REVIEW A&E PROJECT NUMBER 2021724.75.844376.01

DISH Wireless L.L.C. PROJECT INFORMATION

MNMSP00115A 2420 WEST COUNTY ROAD C

ROSEVILLE, MN 55113 SHEET TITLE

ELECTRICAL/FIBER ROUTE PLAN AND NOTES

SHEET NUMBER

E-1

ELECTRICAL NOTES FIBER HAND

1'-10"

1'-2"

4 ₹

1/4"=1'-0"

AERIAL VIEW

NO SCALE

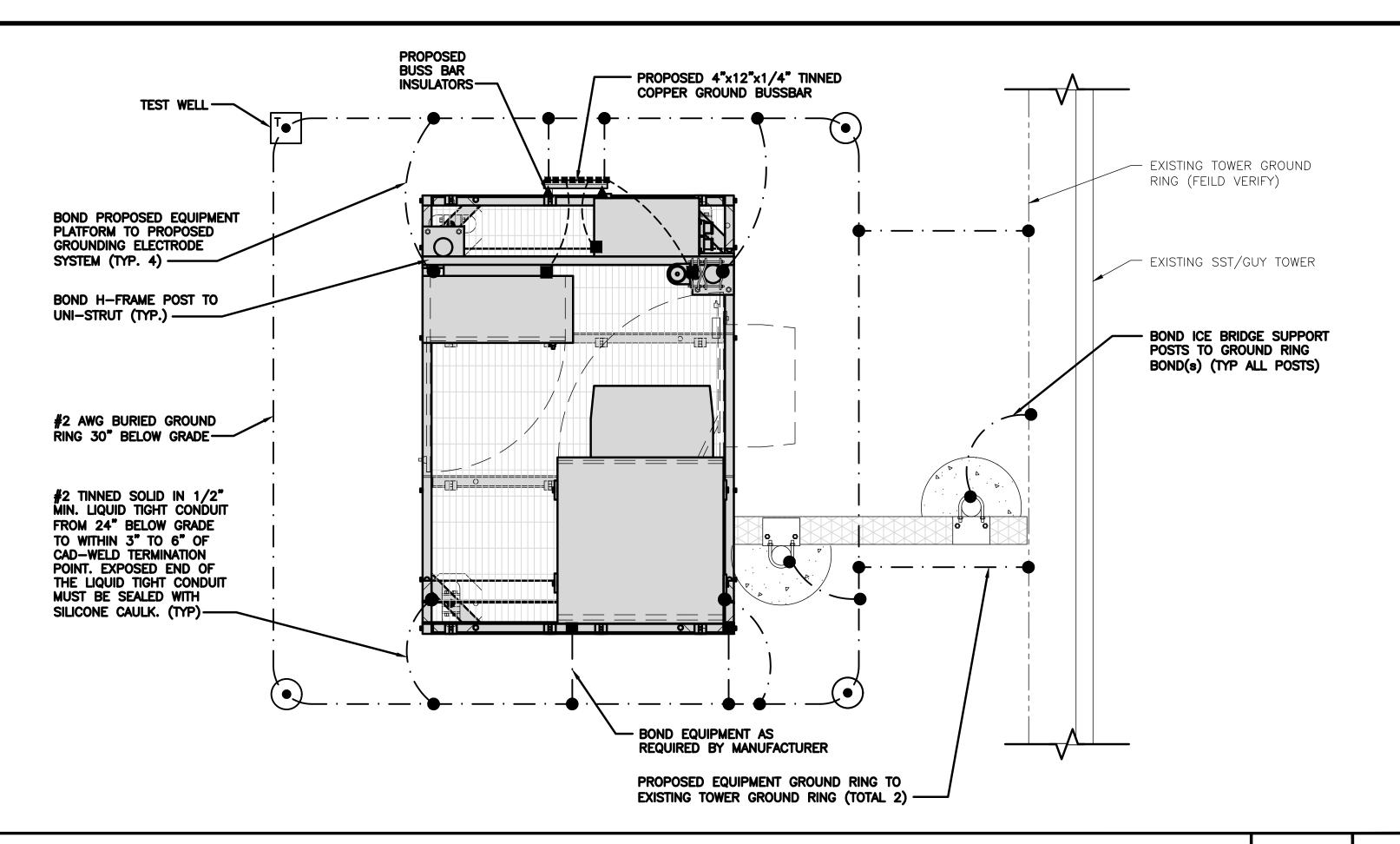
PROPOSED FIBER

HANDHOLE -

DISH Wireless L.L.C. TEMPLATE VERSION 34 - 06/11/2021

6'-0"

FIBER HAND



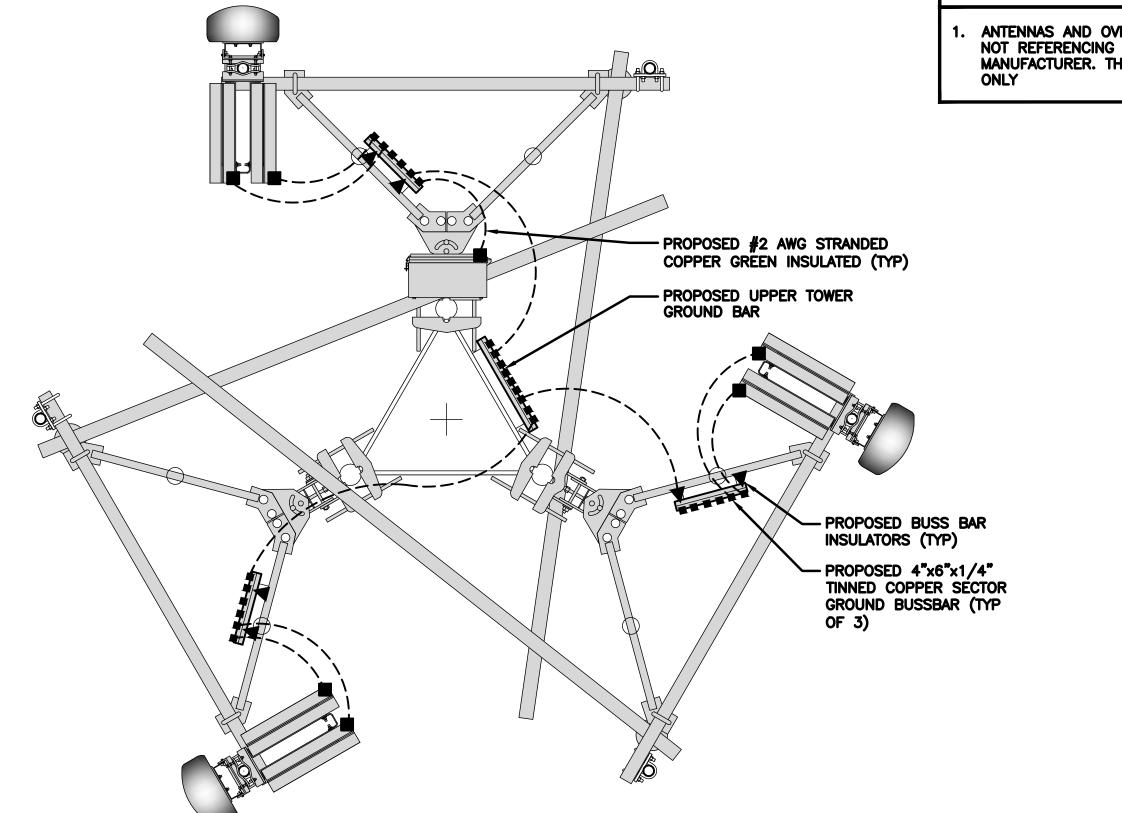
TYPICAL EQUIPMENT GROUNDING PLAN

TYPICAL ANTENNA GROUNDING PLAN

NO SCALE

NOTES

ANTENNAS AND OVP SHOWN ARE GENERIC AND NOT REFERENCING TO A SPECIFIC MANUFACTURER. THIS LAYOUT IS FOR REFERENCE



EXOTHERMIC CONNECTION TEST GROUND ROD WITH INSPECTION SLEEVE MECHANICAL CONNECTION

GROUND BUS BAR

GROUND ROD

#6 AWG STRANDED & INSULATED

▲ BUSS BAR INSULATOR

#2 AWG SOLID COPPER TINNED

GROUNDING LEGEND

- 1. GROUNDING IS SHOWN DIAGRAMMATICALLY ONLY.
- 2. CONTRACTOR SHALL GROUND ALL EQUIPMENT AS A COMPLETE SYSTEM. GROUNDING SHALL BE IN COMPLIANCE WITH NEC SECTION 250 AND DISH Wireless L.L.C. GROUNDING AND BONDING REQUIREMENTS AND MANUFACTURER'S SPECIFICATIONS.
- 3. ALL GROUND CONDUCTORS SHALL BE COPPER; NO ALUMINUM CONDUCTORS SHALL BE USED.

GROUNDING KEY NOTES

- A EXTERIOR GROUND RING: #2 AWG SOLID COPPER, BURIED AT A DEPTH OF AT LEAST 30 INCHES BELOW GRADE, OR 6 INCHES BELOW THE FROST LINE AND APPROXIMATELY 24 INCHES FROM THE EXTERIOR WALL OR FOOTING.
- TOWER GROUND RING: THE GROUND RING SYSTEM SHALL BE INSTALLED AROUND AN ANTENNA TOWER'S LEGS, AND/OR GUY ANCHORS. WHERE SEPARATE SYSTEMS HAVE BEEN PROVIDED FOR THE TOWER AND THE BUILDING, AT LEAST TWO BONDS SHALL BE MADE BETWEEN THE TOWER RING GROUND SYSTEM AND THE BUILDING RING GROUND SYSTEM USING MINIMUM #2 AWG SOLID COPPER CONDUCTORS.
- C INTERIOR GROUND RING: #2 AWG STRANDED GREEN INSULATED COPPER CONDUCTOR EXTENDED AROUND THE PERIMETER OF THE EQUIPMENT AREA. ALL NON-TELECOMMUNICATIONS BELATED METALLIC OF JECTS FOUND PERIMETER OF THE EQUIPMENT AREA. ALL NON-TELECOMMUNICATIONS RELATED METALLIC OBJECTS FOUND WITHIN A SITE SHALL BE GROUNDED TO THE INTERIOR GROUND RING WITH #6 AWG STRANDED GREEN INSULATED CONDUCTOR.
- BOND TO INTERIOR GROUND RING: #2 AWG SOLID TINNED COPPER WIRE PRIMARY BONDS SHALL BE PROVIDED AT LEAST AT FOUR POINTS ON THE INTERIOR GROUND RING, LOCATED AT THE CORNERS OF THE BUILDING.
- E GROUND ROD: UL LISTED COPPER CLAD STEEL. MINIMUM 1/2" DIAMETER BY EIGHT FEET LONG. GROUND RODS SHALL BE INSTALLED WITH INSPECTION SLEEVES. GROUND RODS SHALL BE DRIVEN TO THE DEPTH OF GROUND ROD: UL LISTED COPPER CLAD STEEL. MINIMUM 1/2" DIAMETER BY EIGHT FEET LONG. GROUND GROUND RING CONDUCTOR.
- FRAMES. ALL BONDS ARE MADE WITH #2 AWO LINESCE MOTES OF THE COMMUNICATIONS EQUIPMENT FRAMES. ALL BONDS ARE MADE WITH #2 AWG UNLESS NOTED OTHERWISE STRANDED GREEN INSULATED COPPER CONDUCTORS. BOND TO GROUND RING WITH (2) #2 SOLID TINNED COPPER CONDUCTORS.
- G HATCH PLATE GROUND BAR: BOND TO THE INTERIOR GROUND RING WITH TWO #2 AWG STRANDED GREEN INSULATED COPPER CONDUCTORS. WHEN A HATCH-PLATE AND A CELL REFERENCE GROUND BAR ARE BOTH PRESENT, THE CRGB MUST BE CONNECTED TO THE HATCH-PLATE AND TO THE INTERIOR GROUND RING USING (2) TWO #2 AWG STRANDED GREEN INSULATED COPPER CONDUCTORS EACH.
- EXTERIOR CABLE ENTRY PORT GROUND BARS: LOCATED AT THE ENTRANCE TO THE CELL SITE BUILDING. DO TO GROUND RING WITH A #2 AWG SOLID TINNED COPPER CONDUCTORS WITH AN EXOTHERMIC WELD AND EXTERIOR CABLE ENTRY PORT GROUND BARS: LOCATED AT THE ENTRANCE TO THE CELL SITE BUILDING. BOND INSPECTION SLEEVE.
- TELCO GROUND BAR: BOND TO BOTH CELL REFERENCE GROUND BAR OR EXTERIOR GROUND RING.
- FRAME BONDING: THE BONDING POINT FOR TELECOM EQUIPMENT FRAMES SHALL BE THE GROUND BUS THAT IS NOT ISOLATED FROM THE EQUIPMENTS METAL FRAMEWORK.
- INTERIOR UNIT BONDS: METAL FRAMES, CABINETS AND INDIVIDUAL METALLIC UNITS LOCATED WITH THE AREA OF THE INTERIOR GROUND RING REQUIRE A #6 AWG STRANDED GREEN INSULATED COPPER BOND TO THE INTERIOR GROUND RING.
- FENCE AND GATE GROUNDING: METAL FENCES WITHIN 7 FEET OF THE EXTERIOR GROUND RING OR OBJECTS BONDED TO THE EXTERIOR GROUND RING SHALL BE BONDED TO THE GROUND RING WITH A #2 AWG SOLID TINNED COPPER CONDUCTOR AT AN INTERVAL NOT EXCEEDING 25 FEET. BONDS SHALL BE MADE AT EACH GATE POST AND ACROSS GATE OPENINGS.
- M EXTERIOR UNIT BONDS: METALLIC OBJECTS, EXTERNAL TO OR MOUNTED TO THE BUILDING, SHALL BE BONDED TO THE EXTERIOR GROUND RING. USING #2 TINNED SOLID COPPER WIRE
- N ICE BRIDGE SUPPORTS: EACH ICE BRIDGE LEG SHALL BE BONDED TO THE GROUND RING WITH #2 AWG BARE TINNED COPPER CONDUCTOR. PROVIDE EXOTHERMIC WELDS AT BOTH THE ICE BRIDGE LEG AND BURIED GROUND RING.
- DURING ALL DC POWER SYSTEM CHANGES INCLUDING DC SYSTEM CHANGE OUTS, RECTIFIER REPLACEMENTS OR ADDITIONS, BREAKER DISTRIBUTION CHANGES, BATTERY ADDITIONS, BATTERY REPLACEMENTS AND INSTALLATIONS OR CHANGES TO DC CONVERTER SYSTEMS IT SHALL BE REQUIRED THAT SERVICE CONTRACTORS VERIFY ALL DC POWER SYSTEMS ARE EQUIPPED WITH A MASTER DC SYSTEM RETURN GROUND CONDUCTOR FROM THE DC POWER SYSTEM COMMON RETURN BUS DIRECTLY CONNECTED TO THE CELL SITE REFERENCE GROUND BAR
- (P) TOWER TOP COLLECTOR BUSS BAR IS TO BE MECHANICALLY BONDED TO TOWER STEEL.

REFER TO DISH Wireless L.L.C. GROUNDING NOTES.

wireless.

5701 SOUTH SANTA FE DRIVE LITTLETON, CO 80120



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Signature:
Typed or Printed Name: STEVEN P. SCHAUB Date: 08/23/2021 License Number: 50426

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LYN CJM	
LIN	I MRL

RFDS REV #:

CONSTRUCTION **DOCUMENTS**

	SUBMITTALS					
REV	DATE	DESCRIPTION				
A	07/08/2021	ISSUED FOR REVIEW				
0	08/23/2021	ISSUED FOR REVIEW				
	A&E F	PROJECT NUMBER				
	202172	A 75 8AA376 01				

2021/24./5.8443/6.01

DISH Wireless L.L.C. PROJECT INFORMATION

MNMSP00115A 2420 WEST COUNTY ROAD (ROSEVILLE, MN 55113

> SHEET TITLE GROUNDING PLANS AND NOTES

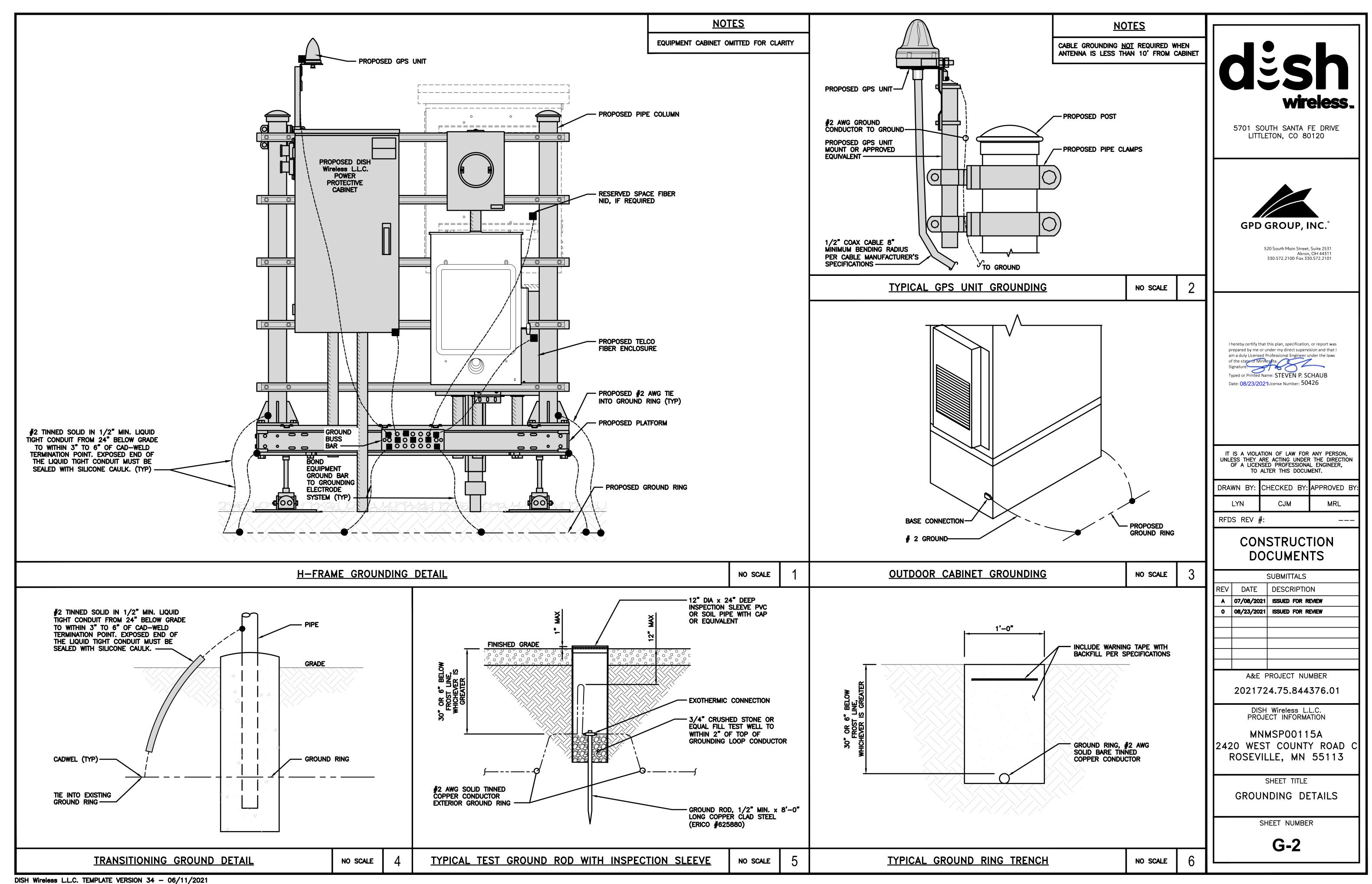
> > SHEET NUMBER

G-1

NO SCALE

GROUNDING KEY NOTES

NO SCALE



REQUEST FOR PLANNING COMMISSION ACTION

11/03/2021 Date:

Item No.: 7a

Department Approval

Agenda Section **Other Business**

Janue Gundrach

2022 Variance Board & Planning Commission Meeting Calendar Item Description:

BACKGROUND

Every year the City Council adopts a meeting calendar. For 2022, the following dates have been 2

- identified for Variance Board and/or Planning Commission meetings as needed. As is customary, 3
 - these dates consist of the first Wednesday of every month except when these dates fall on a holiday.
- In the event of a holiday, the date is adjusted appropriately. The 2022 meeting dates are as follows:

January 5, 2022 7

February 2, 2022 8

March 2, 2022

April 6, 2022 10

May 4, 2022

June 1, 2022

July 6, 2022

August 3, 2022 14 15

September 7, 2022

October 5, 2022

November 2, 2022

December 7, 2022

19 20

21

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12

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16

17

18

While it is recognized conflicts arise, if possible, please let staff know if you will be unable to attend any of these meeting dates.

STAFF RECOMMENDATION

No formal action is necessary. 23

> Prepared by: Janice Gundlach, Community Development Director