


<p>Commissioners: Julie Kimble Michelle Kruzell Tammy McGehee Michelle Pribyl Karen Schaffhausen Erik Bjorum Emily Leutgeb</p>	<p style="text-align: center;">  Planning Commission Agenda Wednesday, March 2, 2022 6:30pm Members of the public who wish to speak during public comment or on an agenda item may do so in person during this meeting or virtually by registering at www.cityofroseville.com/attendmeeting. </p>	<p style="text-align: right;">Address: 2660 Civic Center Dr. Roseville, MN 55113</p> <p style="text-align: right;">Phone: 651-792-7080</p> <p style="text-align: right;">Website: www.cityofroseville.com/pc</p>
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1. Call To Order
2. Roll Call
3. Approval Of Agenda
4. Review Of Minutes

Documents:

[FEBRUARY 2, 2022 MINUTES.PDF](#)

5. Communications And Recognitions
 - 5.A. From The Public:
Public comment pertaining to general land use issues not on this agenda.
 - 5.B. From The Commission Or Staff:
Information about assorted business not already on this agenda.
6. Public Hearing
 - 6.A. Consider A Request By Driver Brands, LLC (Take 5 Car Wash) For A Conditional Use To Allow A Drive-Through At 1701 County Road C (PF21-020)

Documents:

[6A REPORT AND ATTACHMENTS.PDF](#)

7. Adjourn



**Planning Commission Regular Meeting
City Council Chambers, 2660 Civic Center Drive
Draft Minutes – Wednesday, February 2, 2022 – 6:30 p.m.**

Pursuant to Minn. Stat. 13.D.021, Planning Commission members, City Staff, and members of the public participated in this meeting electronically due to the COVID-19 pandemic.

1 **1. Call to Order**

2 Chair Kimble called to order the regular meeting of the Planning Commission meeting at
3 approximately 6:30 p.m. and reviewed the role and purpose of the Planning Commission.
4

5 **2. Roll Call**

6 At the request of Chair Kimble, City Planner Thomas Paschke called the Roll.
7

8 **Members Present:** Chair Julie Kimble, Vice-Chair Michell Pribyl, and
9 Commissioners Michelle Kruzel, Tammy McGehee, Karen
10 Schaffhausen, Erik Bjorum and Emily Leutgeb.
11

12 **Members Absent:** None
13

14 **Staff Present:** City Planner Thomas Paschke, Senior Planner Bryan Lloyd,
15 Community Development Director Janice Gundlach.
16

17 **3. Approve Agenda**

18 **MOTION**

19 **Member Pribyl moved, seconded by Member Schaffhausen, to approve the agenda**
20 **as presented.**
21

22 **Ayes: 7**

23 **Nays: 0**

24 **Motion carried.**
25
26

27 **4. Review of Minutes**

28 **a. January 5, 2022 Planning Commission Regular Meeting**

29 **MOTION**

30 **Member Schaffhausen moved, seconded by Member McGehee, to approve the**
31 **January 5, 2022 meeting minutes.**
32

33 **Ayes: 7**

34 **Nays: 0**

35 **Motion carried.**
36
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39 **5. Communications and Recognitions:**

- 40
41 **a. From the Public:** *Public comment pertaining to general land use issues not on this*
42 *agenda, including the 2040 Comprehensive Plan Update.*

43
44 None.

- 45
46 **b. From the Commission or Staff:** *Information about assorted business not already on*
47 *this agenda, including a brief update on the 2040 Comprehensive Plan Update*
48 *process.*

49
50 Mr. Paschke indicated the Take 5 Car Wash Drive-Through which was noticed in a
51 public hearing was pulled from the docket due to additional information needed. This
52 will be before the Planning Commission at the March meeting.

53
54 **6. Public Hearing**

- 55
56 **a. Consideration of a Request by Launch Properties for Preliminary Approval of a**
57 **Major Plat to Subdivide 8.01 Acres to be Known as Twin Lakes 4th Addition**
58 **Into Five Commercial Lots for Future Developments (PF21-020)**

59 Chair Kimble opened the public hearing for PF21-020 at approximately 6:40 p.m. and
60 reported on the purpose and process of a public hearing.

61
62 City Planner Paschke summarized the request as detailed in the staff report dated
63 February 2, 2022.

64
65 Member Schaffhausen asked when this plan was originally looked at was there a path
66 going from this location so people could walk easily to the park on the other side or to
67 the restaurants and retail area just beyond it.

68
69 Mr. Paschke did not believe the Planning Commission reviewed the minor plat but as
70 a part of the development project, City staff did look at and has been working with
71 the applicant on pedestrian access so there currently are sidewalks along County Road
72 C as well as converting a ditch to be a culvert with a trail across it that connects up to
73 or by Oasis Park and will actually connect into Oasis Park and then on either side of
74 the road going into the residential development there are currently sidewalks to get
75 there. There might also be a pathway that goes from the existing trail where the ditch
76 system was over into the site as well.

77
78 Member McGehee asked if was possible to work with the developer so the path that
79 is around the darker red development, the senior development, that has trees along the
80 back edge, there seems to be a similar arrangement on the other side but she
81 wondered if getting a path along that side between the fifth lot and County Road C to
82 make what seems to her a somewhat connection to the retail area rather than going all
83 the way down Hershel and making a big square. Then possibly having the owners
84 discussing coming across and catching the sidewalk that goes around the front of the
85 retail section.

86
87 Mr. Paschke believed staff could discuss this with the developer. He was not sure if
88 that was a plat item but staff could work with them on that.
89

90 Member Leutgeb asked if staff could talk about the irregular lot shapes that are
91 proposed in this plat, particularly lot 2 which is “L” shaped and narrow and she
92 wondered what the future development could become.
93

94 Mr. Paschke explained as it relates to lot design, commercial properties are not
95 treated like residential with perpendicular lines because their situations are much
96 different. In this case, for the most part there is shared parking throughout and shared
97 access throughout. He showed a different layout of the plans and explained the layout
98 to the Commission.
99

100 Mr. Dan Regan, Roseville Investment Partners, addressed the Commission.
101

102 Member McGehee thanked Mr. Regan for his work on this and appreciated their
103 efforts on getting some good things for the community.
104

105 **Public Comment**
106

107 **Ms. Kathy Peterson, 1751 Rose Place**

108 Ms. Peterson explained she lives behind this project and wondered how traffic will be
109 minimized because traffic in the area already affects their neighborhood. She asked if
110 there will be any plan for increased police presence because she anticipated more
111 accidents with the increased traffic.
112

113 Mr. Paschke explained there was an EAW done for this project and through that
114 process it put together mitigations to address those impacts. From a City standpoint
115 they were pretty concerned about the added traffic from this site as was Ramsey
116 County, however the study came back and did not indicate that there were any major
117 outstanding issues where there would need to be major improvements to the roadway.
118 The signal light going in on Hershel is actually an improvement and will assist in
119 helping the traffic flow better and slow traffic down as well as to allow for proper
120 movement of vehicles turning left onto or off of County Road C.
121

122 Chair Kimble closed the public hearing at 7:14 p.m.
123

124 **MOTION**

125 **Member Pribyl moved, seconded by Member Kruzel, to recommend to the City**
126 **Council approval of the proposed Twin Lakes 4th Addition Preliminary Plat,**
127 **based on the content of the RPCA, public input, and Planning Commission**
128 **deliberation with the two conditions listed in the report (PF22-020).**
129

130 **Ayes: 7**

131 **Nays: 0**

132 **Motion carried.**

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b. Consider a Request by Nelson Architecture and Interiors, on Behalf of Bank Of America, for a Conditional Use to Allow a Drive-Through at 1755 County Road C (PF21-020)

Chair Kimble opened the public hearing for PF21-020 at approximately 7:16 p.m. and reported on the purpose and process of a public hearing.

City Planner Paschke summarized the request as detailed in the staff report dated February 2, 2022.

The applicant was at the meeting but did not want to add anything to the presentation.

Public Comment

No one came forward to speak for or against this request.

Chair Kimble closed the public hearing at 7:28 p.m.

MOTION

Member Schaffhausen moved, seconded by Member McGehee, to recommend to the City Council approval of a Conditional Use for 1755 County Road C, allowing a drive-through facility based on the comments, findings, and condition listed in the report (PF22-020).

Ayes: 7

Nays: 0

Motion carried.

c. Consider a Request by BFI Real Estate Holdings, LLC, on Behalf of Border Foods (Taco Bell), for a Conditional Use to Allow a Drive-Through at 1743 County Road C (PF21-020)

Chair Kimble opened the public hearing for PF21-020 at approximately 7:32 p.m. and reported on the purpose and process of a public hearing.

City Planner Paschke summarized the request as detailed in the staff report dated February 2, 2022.

Member McGehee explained she liked the way that the private road goes into the development but the backside of the taco bell, where the windows are, is pretty stark. She wondered if that will be landscaped along there to soften it up.

Mr. Paschke explained the design of the building is not necessarily a component of the drive-through conditional use. Staff has completed a review of various aspects of the building and has sought some modifications of the design. That would be one of them to try to dress up that side of the building. Landscaping will be installed to augment and reduce the drive-through impact.

180 Mr. Zack Ellington, BFI Real Estate Holdings, addressed the Commission.
181

182 **Public Comment**
183

184 It was noted Mr. Kevin Neighbor, Fridley, Minnesota, submitted an online comment
185 expressing design comments.
186

187 No one came forward to speak for or against this request.
188

189 Chair Kimble closed the public hearing at 7:46 p.m.
190

191 **MOTION**

192 **Member Kruzel moved, seconded by Member Bjorum, to recommend to the**
193 **City Council approval of a Conditional Use for 1743 County Road C allowing a**
194 **drive-through on the subject property based on the comments, findings, and two**
195 **conditions listed in this report (PF22-020).**
196

197 **Ayes: 7**

198 **Nays: 0**

199 **Motion carried.**
200

201 **d. Request for Approval of an Interim Use Renewal Pursuant to §1009.03 of the**
202 **City Code to Permit Park-And-Ride Facilities at Nine Locations During the 12-**
203 **Day Minnesota State Fair (PF21-019)**

204 Chair Kimble opened the public hearing for PF21-019 at approximately 7:18 p.m. and
205 reported on the purpose and process of a public hearing.
206

207 Senior Planner Bryan Lloyd summarized the request as detailed in the staff report
208 dated February 2, 2022.
209

210 Member Leutgeb indicated there was one comment from the apartments at Pascal
211 about the site at Roseville Covenant, mentioning noise and increased traffic down
212 Pascal of the bus coming out and suggesting a turn-around in that parking lot. She
213 wondered if that has been studied and considered as a condition for that site.
214

215 Mr. Lloyd indicated there has not been additional discussion about that this year. He
216 reviewed the path of the buses going through Pascal Street and thought in general the
217 circulation of buses does try to use the simplest, navigational route. He noted it is a
218 complicated site there in the Centennial property and the shape of the buildings that
219 are there as well as the shape of the buildings in the shopping center to the South and
220 the Covenant site itself is fairly limited in the parking area as well. He thought it
221 would be difficult for the buses to turn around there and not go down Pascal because
222 of the limitations.
223

224 Member Pribyl indicated in the recommendation actions, Item C requires trash
225 containers at every site and she did not recall from the last time she used a park-and-

226 ride if there was also recycling provided. She thought it would be nice to have those
227 as well.

228
229 Mr. Lloyd indicated he thought that was a great idea and was in support of that. He
230 thought this particular condition is focused on trash because that is a potential
231 nuisance. He would not be surprised if there is also recycling but staff can ask the
232 State Fair if recycle containers could be included as well at each site.

233
234 Member Kruzal asked if there were any statistics on crime at any of the park-and-ride
235 locations, especially in the evening at the drop off at the end. With that she noticed
236 some of these drop off's do not have adequate lighting, especially the one at St. Rose.
237 She indicated she was worried about the safety in the evening.

238
239 Mr. Lloyd explained he did not know of any statistics. The proposals are reviewed by
240 the development review committee which includes people with the Police
241 Department. Between that and the condition that each site have staff monitoring it
242 through midnight, seems to have done fairly well. There are measures that have been
243 put in place to discourage crime.

244
245 Member McGehee thought the City over the years has made significant
246 improvements but she knew in 2021 there was difficulty in getting the buses to be
247 able to run very often on these sites. She wondered if there would be more regular
248 and more complete coverage of bus service to these sites this year.

249
250 Mr. Lloyd indicated he did not know the answer to that question and something the
251 applicant might be able to answer.

252
253 Mr. Mike Hagan, Minnesota State Fair, addressed the Commission and answered
254 questions previously asked questions regarding Pascal Street, recycling containers
255 and crime.

256
257 Member Kruzal thought there needed to be some type of lighting in the front parking
258 area at St. Rose. She also asked if there has ever been any consideration about in the
259 fair, along Midway Parkway when people are waiting to catch some of the buses to
260 have some sort of crowd control.

261
262 Mr. Hagan explained once people exit the fairground, the St. Paul Police Department
263 covers the area and are in control of the intersection at Midway Parkway and Snelling
264 Avenue and they would also be the ones to be around the extended distance on
265 Midway Parkway. They have in past years tried to stack up buses along there so
266 people are not waiting so long to transportation.

267
268 Chair Kimble complimented both the State Fair and staff for what they have done
269 over the years to improve this experience for everybody.

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Public Comment

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It was noted there were several comments through online and telephone in support of the park-and-ride locations.

No one came forward to speak for or against this request.

Chair Kimble closed the public hearing at 8:14 p.m.

MOTION

Member Bjorum moved, seconded by Member McGehee, to recommend to the City Council approval of the renewed approval of the nine specified Minnesota State Fair Park-and-ride lots as an interim use, based on the content of the RPCA, public input, and Planning Commission deliberations with the 12 conditions listed in the report (PF22-020).

**Ayes: 7
Nays: 0
Motion carried.**

7. Adjourn

MOTION

Member Pribyl, seconded by Member Leutgeb, to adjourn the meeting at 8:14 p.m.

**Ayes: 7
Nays: 0
Motion carried.**



REQUEST FOR PLANNING COMMISSION ACTION

Agenda Date: 03/02/22
Agenda Item: 6a

Prepared By

Agenda Section

Department Approval

Public Hearings

Janice Gundlach

Item Description: Consider a request by Driven Brands, LLC (Take 5 Car Wash), for a Conditional Use to allow a drive-through at 1701 County Road C (PF21-020)

1 **APPLICATION INFORMATION**

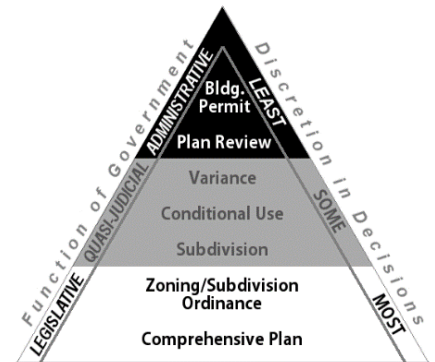
2 Applicant:	Driven Brands, LLC
3 Location:	1701 County Road C
4 Property Owner:	Launch Properties
5 Application Submission:	11/19/21; deemed incomplete due to required traffic study and open house meeting for plat
6	Deemed complete 02/18/22
7	
8 City Action Deadline:	April 19, 2022
9 Zoning:	Community Mixed-Use (MU-2B) District

10 **LEVEL OF DISCRETION IN DECISION MAKING:** Action taken on a
11 conditional use proposal is **quasi-judicial**; the City’s role is to
12 determine the facts associated with the request, and apply those facts
13 to the legal standards contained in State Statute and City Code.

14 **BACKGROUND**

15 Launch Properties, owners of the Twin Lakes Station development
16 have been working with representatives of Driven Brands (Take 5
17 Car Wash) on a drive-through car wash for the eastern portion of the
18 subject property. A car wash, as a principle drive-through type
19 facility, requires an approved conditional use that complies with City
20 Code requirements, including §1009.02.C and §1009.02.D.12.

21 Planning Division staff have attached a number of development documents, mostly for reference
22 purposes (Attachment C). The site plan is germane to the drive-through/conditional use
23 discussion as it details access, vehicle site circulation, and drive-through stacking. This report,
24 and the associated documents, only reviews the conditional use for the drive-through and
25 otherwise assumes the project can or will comply with the required City and Zoning Code
26 standards.



27 **PROJECT REVIEW**

28 The proposed Take 5 Car Wash at the Twin Lakes Station development is one of five total users,
29 two of which have already sought approval of a drive-through (Bank of America and Taco Bell).
30 The main site access to Twin Lakes Station is via the signalized intersection at County Road C
31 and Hershel Street (private road), with a secondary ingress located at the western-most point of
32 the development site, adjacent to the proposed Bank of America.

33 The Take 5 Car Wash is an express, self-serve car wash facility with two full-time employees on
34 site. When examining the overall site plan, the Take 5 Car Wash is located on the far right-hand
35 side of the site (easterly side) adjacent to the Rosedale Square Shopping Center, which includes
36 Lund's and Byerly's. Sole access to the car wash site is provided from the private roadway
37 along the northern portion of the commercial development site. The private roadway could
38 become a public street in the future. The ingress/egress to the site is designed so as to
39 accommodate the possibility of the private roadway becoming public, but only if/when a future
40 connection to the easterly property could be obtained.

41 Access to the car wash is designed as a one-way traveling north to south along the east side of
42 the site to the payment kiosks. From there, customers travel further south to the wash building.
43 Vehicles exit the wash building on the south, then turn right and head north to one of two vehicle
44 vacuum canopy areas comprising 15 vehicle stalls and/or directly to the site exit. The west side
45 of the site includes a two-way drive lane from the ingress/egress for employees and customers,
46 and 5 additional parking stalls.

47 Typically, a traffic study is required for drive-through facilities. However, this proposed
48 development was included in the 2020 Twin Lakes Station Environmental Assessment
49 Worksheet (EAW), which included an extensive traffic study, albeit with only a single drive-
50 through facility (Bank of America). Given the proposed car wash use and the two other drive-
51 through businesses proposed at Twin Lakes Station (Taco Bell and Bank of America), the
52 applicant was required to provide the City Engineer updated traffic information related to drive-
53 through facilities, including queuing and circulation within the site. This information was
54 provided by Kimley-Horn and reviewed by the City's traffic consultant, SRF Consulting Group
55 (Attachment D). It should be noted, in accordance with the EAW process, an extensive public
56 notification and comment period occurred whereby many governmental entities and the broader
57 public had the opportunity to comment on traffic and safety impacts from the proposed
58 development, including any drive-through facilities that may be proposed.

59 Zoning Code Sections §1009.02.C and §1009.02.D.12 establish general standards and criteria
60 applying to all conditional uses and specific standards and criteria applying to drive-through
61 facilities. The Planning Division's review of these criteria can be found below in the Conditional
62 Use Analysis section.

63 **CONDITIONAL USE ANALYSIS**

64 REVIEW OF GENERAL CONDITIONAL USE CRITERIA: Section 1009.02.C of the Zoning Code
65 establishes general standards and criteria for all conditional uses. When deciding on whether to
66 approve or deny a conditional use, the Planning Commission (and City Council) must review the
67 proposal and determine if compliance can be achieved with the stated findings.

68 The general code standards of §1009.02.C are as follows:

- 69 **a.** *The proposed use is not in conflict with the Comprehensive Plan.* While a drive-through
70 facility doesn't appreciably advance the goals of the Comprehensive Plan aside from
71 facilitating continued investment in a property, Planning Division staff believes it does not

72 conflict with the Comprehensive Plan either. More specifically, the General and Commercial
73 Area Goals and Policies sections of the Comprehensive Plan include a number of policies
74 related to reinvestment, redevelopment, quality development, and scale. The proposed drive-
75 through is one component of a larger investment at the Twin Lakes Station redevelopment
76 area, which would align with the related goals and policies of the Comprehensive Plan.

77 **b.** *The proposed use is not in conflict with a Regulating Map or other adopted plan.* The subject
78 property is located within the Twin Lakes Redevelopment Area, which area is required to
79 comply with specific building placement and site design standards under the MU-2B zoning
80 District and the applicable Regulating Plan. In the case of this proposed development, its
81 development standards are guided by the Flexible Frontage (1005.07.E.2) and/or the Design
82 Standards listed in §1005.02.A. Planning Division staff's review of the site plan concludes
83 the proposed building and drive-through lane comply with the placement standards of the
84 Regulating Plan and the City's Design Standards.

85 **c.** *The proposed use is not in conflict with any City Code requirements.* The proposed site plan
86 addresses all applicable requirements of the City Code as it pertains to the proposed drive-
87 through CU. Moreover, a CU approval can be rescinded if the approved use fails to comply
88 with all applicable Code requirements or any conditions of the approval. As part of the
89 building permit review process, Planning Division staff will conduct a more comprehensive
90 Code compliance analysis, including zoning standards such as landscaping, trash/recycling
91 enclosures, vehicle parking, materials, etc.

92 **d.** *The proposed use will not create an excessive burden on parks, streets, and other public*
93 *facilities.* Staff does not anticipate the proposal to intensify any practical impacts on parks,
94 streets, or public infrastructure. The City Engineer has determined there will be no
95 significant queueing or traffic issues associated with the proposed Take 5 Car Wash drive-
96 through. The drive-through will not create any significant increase in traffic on public streets,
97 nor cause any negative impacts to public infrastructure. Similarly, the Traffic Consultant
98 notes the site plan shows the car wash having approximately 480 feet of queueing space,
99 whereby the average maximum queue for car washes is 5 vehicles or 100 feet. However, the
100 85th percentile queue is 6 vehicles or 120 feet, which further suggests the queue lane for the
101 car wash is more than sufficient to accommodate the queueing of vehicles without overflow
102 onto the private road.

103 **e.** *The proposed use will not be injurious to the surrounding neighborhood, will not negatively*
104 *impact traffic or property values, and will not otherwise harm the public health, safety, and*
105 *general welfare.* The proposed car wash drive-through will not be injurious to the
106 surrounding neighborhood; negatively impact traffic or property values; and will not
107 otherwise harm the public health, safety, and general welfare. The City Engineer and
108 Planning Division staff anticipates this proposed drive-through will increase vehicle trips
109 within the Twin Lakes Station site and on the adjacent roadways, but, not significantly, for
110 extended periods of time, or in a manner that negatively impacts the site's overall function.
111 Based on the location of the proposed Take 5 Car Wash drive-through, movements through
112 the drive-through lane will not cause any negative impacts to adjacent sites or any traffic
113 entering Twin Lakes Station from the west. Further, based upon the proposed site plan, and
114 the broader redevelopment plans identified in the 2020 EAW, access to public facilities has
115 been improved through the signalized intersection and greater pedestrian connections both
116 internally and externally to/from the property.

117 REVIEW OF SPECIFIC CONDITIONAL USE CRITERIA: §1009.02.D.12 of the Zoning Code establishes
118 additional standards and criteria that are specific to drive-through facilities:

119 a. *Drive-through lanes and service windows shall be located to the side or rear of buildings*
120 *and shall not be located between the principal structure and a public street except when the*
121 *parcel and/or structure lies adjacent to more than one public street and the placement is*
122 *approved by the Community Development Department (Ord. 1443, 6-17-2013). The basis*
123 *for this criterion is to limit vehicular impacts on adjacent roadways and the pedestrian realm.*
124 *Although a small portion of the drive-through lane lies between the vehicle wash building*
125 *and County Road C, the payment kiosk is located in the side yard on the north side of the*
126 *wash building. This drive-through lane orientation on the site is acceptable to the Community*
127 *Development Department and functions well within the overall redevelopment area.*

128 b. *Points of vehicular ingress and egress shall be located at least 60 feet from the street right-*
129 *of-way lines of the nearest intersection. The nearest access to the Twin Lakes Station*
130 *development and the proposed Take 5 Car Wash site is the signalized intersection at County*
131 *Road C, which lies west of the development site approximately 680 feet from the access into*
132 *the car wash site.*

133 c. *The applicant shall submit a circulation plan that demonstrates that the use will not interfere*
134 *with or reduce the safety of pedestrian and bicyclist movements. Site design shall*
135 *accommodate a logical and safe vehicle and pedestrian circulation pattern. Queuing lane*
136 *space shall be provided, sufficient to accommodate demand, without interfering with primary*
137 *driving, entrance, exit, pedestrian walkways, or parking facilities on site. The circulation*
138 *plan shall be made a condition of approval and shall survive any and all users of the drive-*
139 *through and may need to be amended from time to time to ensure continued compliance with*
140 *this condition. Said amendments to the circulation plan will require an amendment to the*
141 *conditional use. The vehicle circulation plan provided for the Planning Commission's*
142 *consideration includes the north/south pathway connection requested by Commissioner*
143 *McGhee at the February meeting, connecting the trail along County Road C to the pathway*
144 *between the commercial development site and The Harbor residential complex. From that*
145 *north/south pathway there is a connection on the south allowing pedestrians to walk to the*
146 *car wash building. This pedestrian access does cross the exit lane from the wash building,*
147 *however its location will afford exiting vehicles to stop or slow enough to not impede*
148 *pedestrian safety. Further, the submitted Site Plan incorporates sufficient queuing space with*
149 *logical and safe circulation and adequate parking facilities.*

150 d. *Speaker box sounds from the drive-through lane shall not be loud enough to constitute a*
151 *nuisance on an abutting residentially zoned property or property in residential use.*
152 *Notwithstanding this requirement, such speaker boxes shall not be located less than 100 feet*
153 *from an existing residentially zoned property or property in residential use. Car wash drive-*
154 *throughs are similar to ATM drive-throughs in that they are typically automated at a very low*
155 *volume to guide the customer for payment or providing a wash number. Additionally, the*
156 *Take 5 payment kiosk lies approximately 240 feet from The Harbor apartment units. The*
157 *Take 5 Car Wash project included two options for the payment kiosk. The first includes up*
158 *to three employees occupying the payment kiosks to offer various washes and collect*
159 *payment. The second option is to install automated payment machines, much like an ATM.*
160 *Regardless of which option is implemented, with both instances the noise emitted from either*
161 *a human or an automated machine would not generate burdensome noise impacts to The*
162 *Harbor.*

- 163 e. *Drive-through canopies and other structures, where present, shall be constructed from the*
164 *same materials as the primary building and with a similar level of architectural quality and*
165 *detailing.* The Take 5 Car Wash proposal includes a number of canopies, however not all are
166 affiliated with the drive-through. One canopy is designed as a component of the payment
167 kiosk. This freestanding canopy covers three drive-through wash lanes and is of a modern
168 design. The second canopy type is provided on the building, both at the entrance and exit of
169 the wash bay. These are integrated into the building in a cantilever design similar to the
170 design of the kiosk canopy. The last canopy type are those that cover the 15 vehicle vacuum
171 stalls. There are 12 vacuum stalls on the west side of the property and 3 on the north, all of
172 which are accommodated with the same canopy design and materials similar to that of the
173 payment kiosk canopy. These proposed canopies are deemed by staff to satisfy this
174 requirement, however Planning Division staff will confirm Code compliance during the
175 building permit review and approval process.
- 176 f. *A 10-foot buffer area with screen planting and/or an opaque wall or fence between 6 and 8*
177 *feet in height shall be required between the drive-through lane and any property line*
178 *adjoining a public street or residentially zoned property or property in residential use and*
179 *approved by the Community Development Department (Ord. 1443, 6-17-2013).* The
180 Community Development Department has determined this drive-through lane requires screen
181 fencing at both of the curves of the exit lane from the wash building to properly screen
182 headlights from County Road C. The revised site plan includes fence screens in these
183 general areas, however, the Planning Division will work with the applicant on the exact
184 location, as well as landscaping to augment the fencing prior to the building permit being
185 issued.

186 **PLANNING DIVISION RECOMMENDATION**

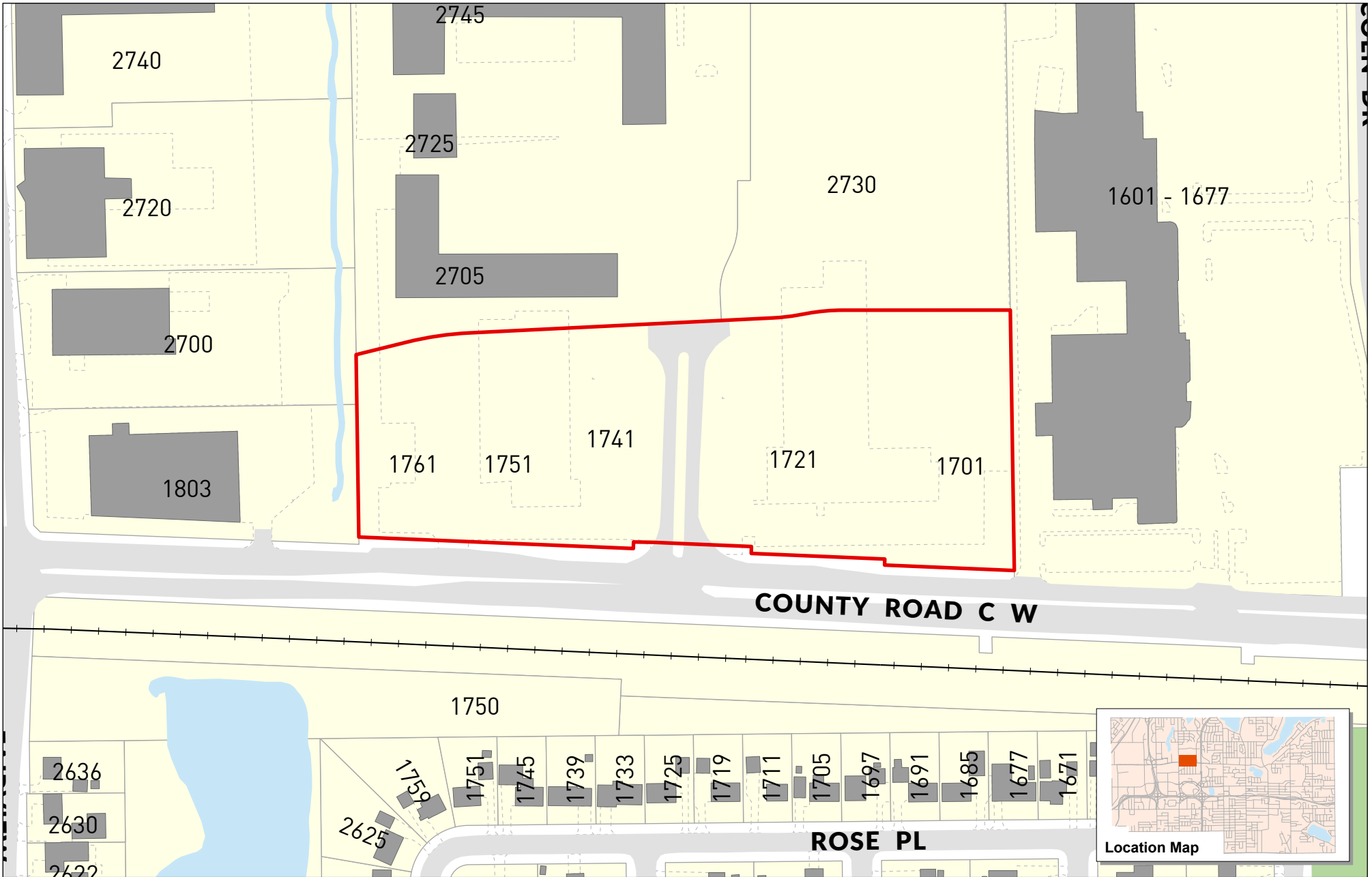
187 The Planning Division recommends approval of the CU to allow a drive-through for Take 5 Car
188 Wash at 1701 County Road C, based on the submitted site and development plans, subject to the
189 following conditions:

- 190 a. The site, building placement, and drive-through lane shall be constructed substantially
191 consistent with the plans submitted February 18, 2022, and provided as a component of the
192 report packet, unless otherwise revised to comply with Zoning Code standards.
- 193 b. The applicant and their landscape consultant shall work with Planning Division staff on a full
194 site landscaping and specifically an appropriate screen for the southern curves of the drive-
195 through lane to minimize or eliminate vehicle headlight impacts to County Road C.

196 **SUGGESTED PLANNING COMMISSION ACTION**

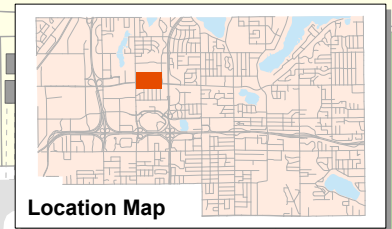
197 **By motion, recommend approval of a CONDITIONAL USE** for 1701 County Road C allowing a
198 drive-through on the subject property based on the comments, findings, and two conditions
199 stated in this report.

Attachment A: Planning File 21-020

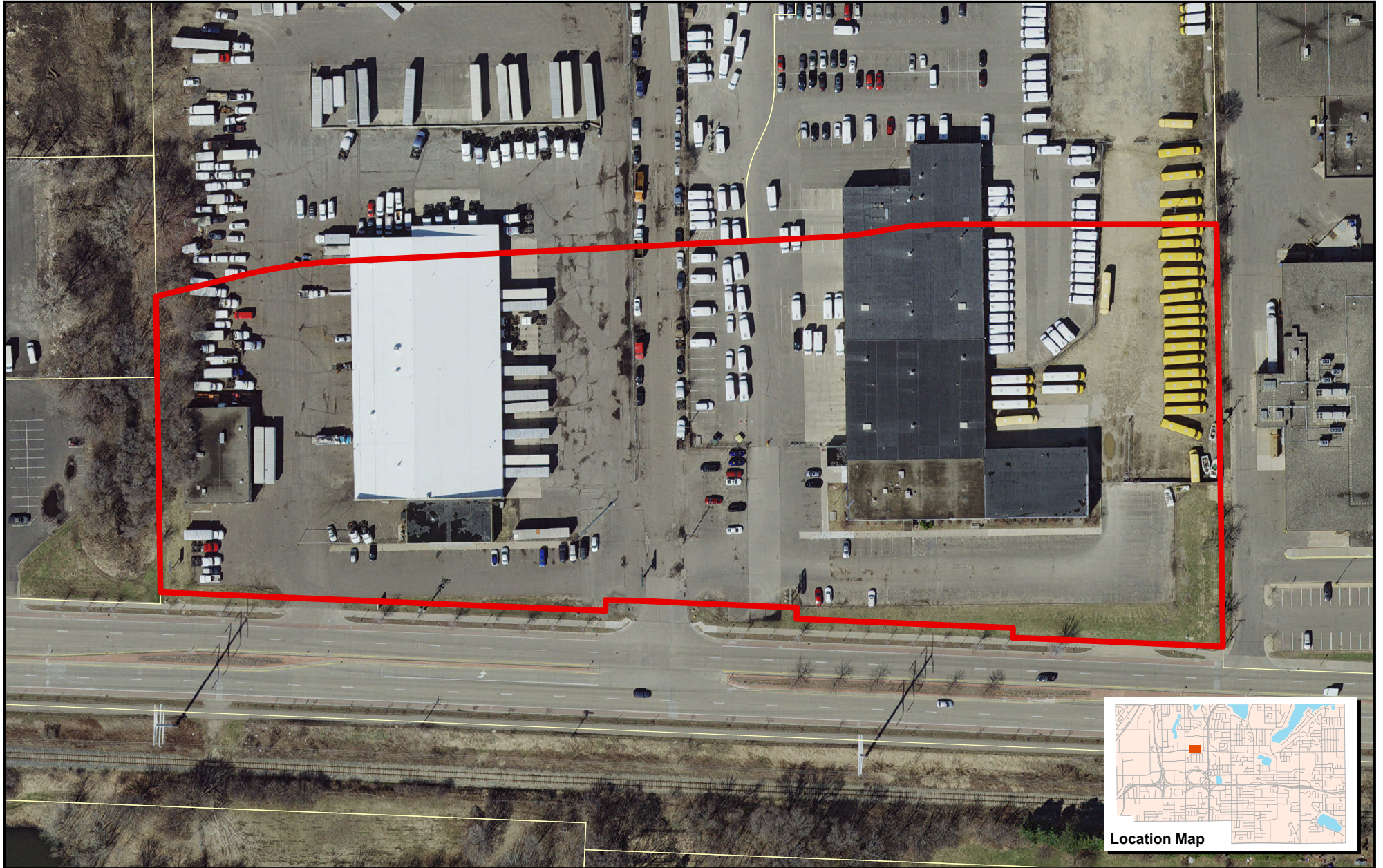


Data Sources
 * Ramsey County GIS Base Map (12/4/2021)
 For further information regarding the contents of this map contact:
 City of Roseville, Community Development Department,
 2660 Civic Center Drive, Roseville MN

Disclaimer
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Attachment B: Planning File 21-018



The logo for Driven Brands features the word "Driven" in a yellow, italicized sans-serif font and "Brands" in a bold black sans-serif font. A red swoosh underline is positioned beneath the text.

November 5, 2021

City of Roseville – Community Development Department
2660 Civic Center Dr
Roseville, MN 55113

Purpose: Project Narrative

Property Description: Take Five Express Car Wash to be located at 1743 County Road C, Roseville, MN 55113

ABOUT DRIVEN BRANDS

Driven Brands, Inc. is one of the largest automotive service companies in North America, providing over 4,100 locations and over 5,000 employees across the United States and Canada. Founded in 2006 with the merger of Meineke and Maaco, Driven Brands has been rapidly expanding for the past 15 years. By providing a range of consumer and commercial automotive needs including paint, body, collision, glass, repair, oil change, and car wash. Driven Brands services over 50 million vehicles annually across the portfolio of businesses.

PROJECT SUMMARY

This document shall serve as the project narrative to the City of Roseville for the Conditional Use Permit Application submitted by Kimley Horn acting as applicant on behalf of the current owner Driven Brands, Inc. The 1.0 acre site is part of the Twin Lakes Station re-development project that is ongoing on County Road C. The developer (Driven Brands) will develop an express self-service car wash with two full time employees on-site. This property is CMUD-4 where car wash use would require the CUP due to the drive-through feature related to the use. The proposed use is not in conflict with any existing regulating maps, adopted plans, or the Comprehensive Plan of Roseville for 2040. Driven Brands is pursuing this property as part of our overall market plan of high-density corridors in the Minneapolis-St. Paul market to provide high quality car wash services to customers.

GENERAL BUILDING AND SITE LAYOUT

The express car wash building area is 4,164 sf and will have 25 total spaces, (18 of which will be dedicated to self-service vacuums). The site will also include two ADA accessible spaces and an on-site dumpster enclosure. The parking counts have been accounted for the overall development to ensure this site will not be an encumbrance on the parking required for other uses in Twin Lakes Station. From the pay booth on the north, the customer's car will enter the building from the north into the car wash tunnel and exiting on the south side where they can go into one of the vacuum stalls from the exit drive lane. The express car wash will be utilizing stored water in tanks in the equipment room which will reduce tap size and capacity out of the

The logo for Driven Brands features the word "Driven" in a yellow, italicized sans-serif font and "Brands" in a bold black sans-serif font. A red swoosh underline is positioned beneath the text.

water main. This setup allows for the reclamation system to recycle approximately 80% of the used water.

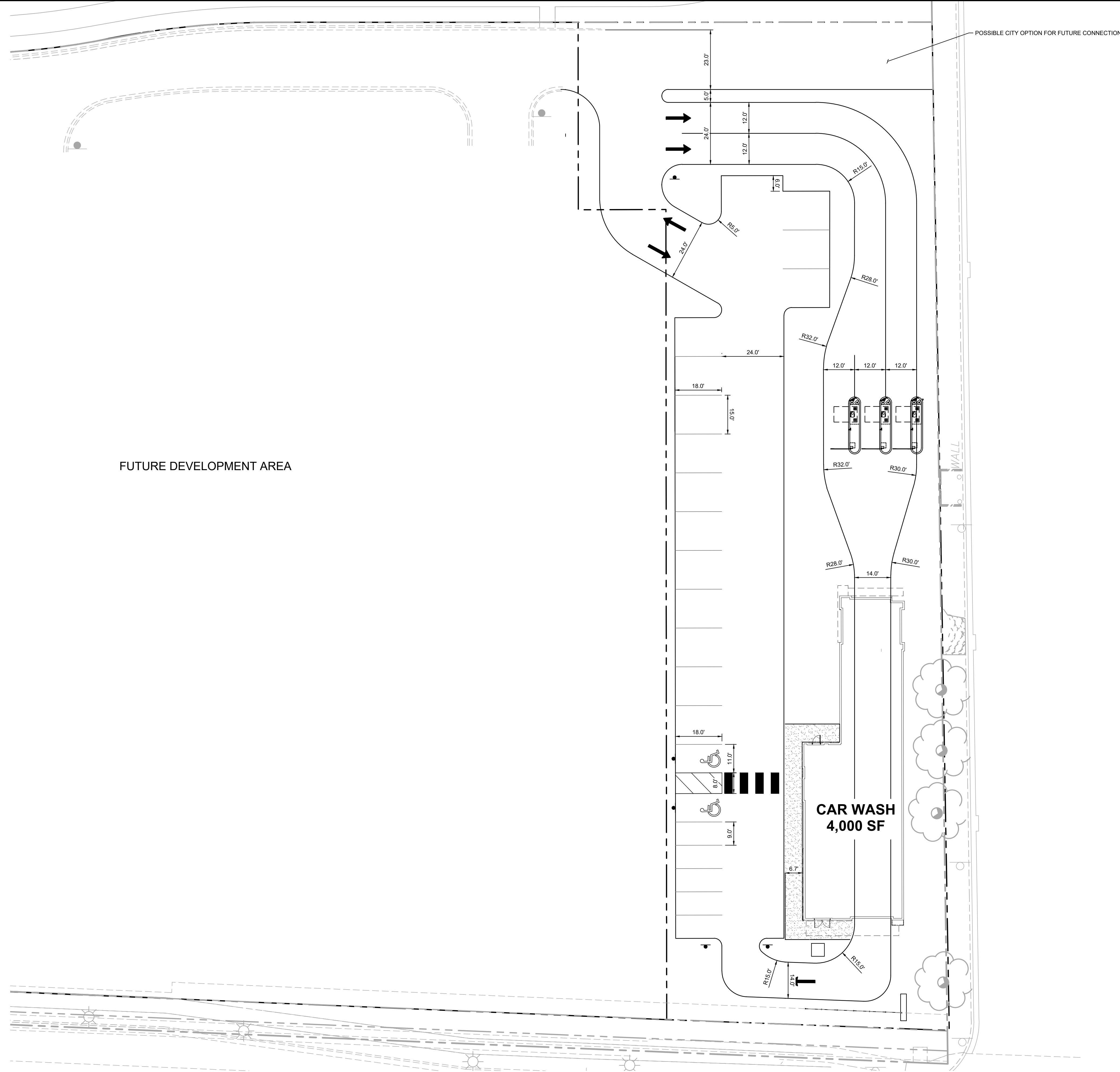
CONCLUSION

As described above, the proposed Take Five Express Car Wash appears to be consistent and in compliance with the City of Roseville land use plan and should not adversely affect the adjacent properties. The development of this site and additional landscaping should greatly improve the image of the site while continuing to offer the residents of the City of Roseville easy access to the services provided by Take Five Express Car Wash.

Sincerely,

A handwritten signature in black ink, appearing to read "Jerod Hanaman".

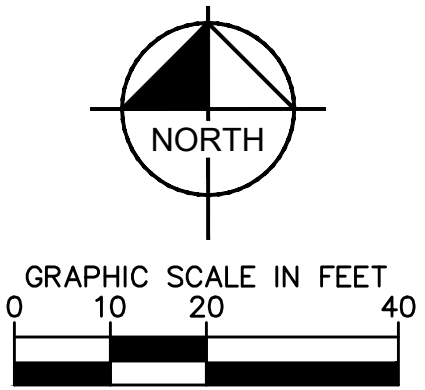
Jerod Hanaman
Real Estate Manager



FUTURE DEVELOPMENT AREA

CAR WASH
4,000 SF

PARKING BREAKDOWN	
PREVIOUS SITE PLAN	25 STALLS
CURRENT SITE PLAN	21 STALLS
DIFFERENCE	4 VACUUM STALLS



TWIN LAKES STATION 3RD
ADDITION
PREPARED FOR
LAUNCH
PROPERTIES
ROSEVILLE MN

SITE PLAN

KHA PROJECT	160727000.L533
DATE	11/17/2021
SCALE	AS SHOWN
DESIGNED BY	MTL
DRAWN BY	AJW
CHECKED BY	BMW

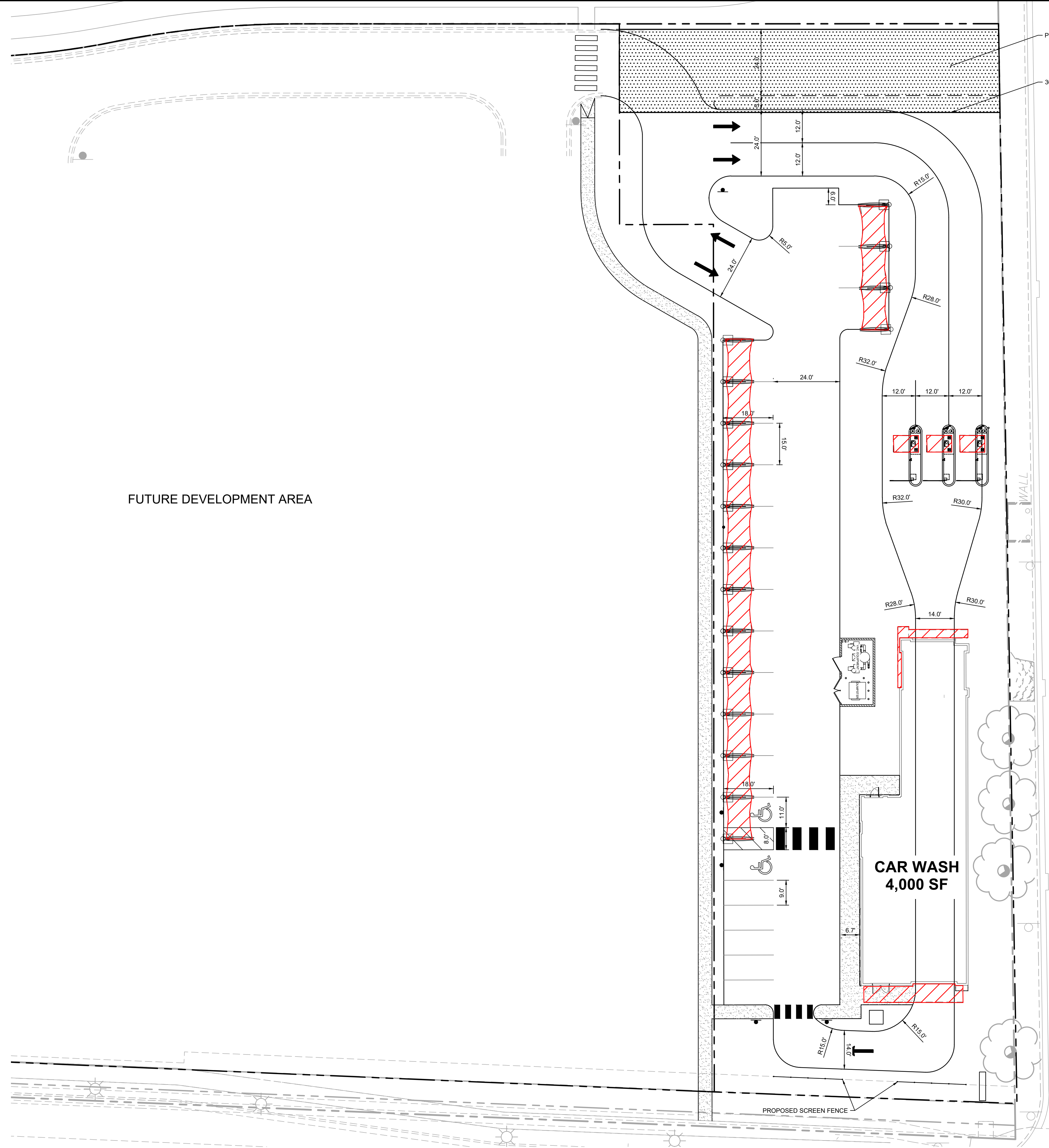
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

BRIAN M. WURDEMAN
MIN
DATE: 11/17/2021 LIC. NO. 5313

Kimley»Horn
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787 EUSTIS STREET, SUITE 100, ST. PAUL, MN 55114
PHONE: 651-454-1197
WWW.KIMLEY-HORN.COM

No.	REVISIONS	DATE	BY

SHEET NUMBER
C400



TWIN LAKES STATION 3RD
ADDITION
 PREPARED FOR
LAUNCH
PROPERTIES
 ROSEVILLE MN

SITE PLAN

KHA PROJECT	160727000.L533
DATE	11/17/2021
SCALE	AS SHOWN
DESIGNED BY	MTL
DRAWN BY	AJW
CHECKED BY	BMW

THESE CERTIFICATES, MAPS, PLANS, SPECIFICATIONS OR REPORTS WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

BRIAN M. WURDEMAN
 MN LIC. NO. 5313
 DATE: 11/17/2021

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No.	REVISIONS	DATE	BY

SHEET NUMBER
C400

EXTERIOR FINISH SCHEDULE

NO.	AREA	COLOR
1	TOWER	BY MANUF. MATCH SW 6169, SEDATE GRAY
2	METAL PANELS	BY MANUF. 1760 LIMESTONE
3	ACCENT CMU	MATCH SW COLOR #6169, SEDATE GRAY
4	DARK CMU	MATCH SW COLOR #7020, BLACK FOX

ELEVATION KEYNOTES

- 1 PRECAST CONC WATERTABLE
- 2 8 X 8 H X 16 SMOOTH FACE CMU W/ DRYBLOC ADDITIVE
- 3 4 X 8 H X 16 SPLIT FACE CMU VENEER W/ DRYBLOC ADDITIVE
- 4 STEEL TUBE FRAME AT OVERHEAD DOORS, PAINT TO MATCH SW 7020 BLACK FOX
- 5 TPO MEMBRANE ROOF SYSTEM, REFER TO ROOF PLAN A104
- 6 .050 X 8"W X 8"H PARAPET OPENING WITH PRE-FINISHED ALUM METAL COLLAR (2" W X 1" PROJ) BRONZE ANODIZED FINISH
- 7 CENTRIA PRE-FINISHED ALUMINUM CONCEALED FASTENER SIDING COMPLETE W/ DRIP FLASHING AND CLOSURES
- 8 PREFORMED PRE-FINISHED ALUM. COPING WITH CONT. HOLD DOWN CLIPS ON BOTH SIDES OF THE PARAPET; FINISH: DARK BRONZE; REFER TO SPECIFICATIONS 07 54 00 FOR MORE INFORMATION
- 9 PREFORMED PRE-FINISHED ALUM. GRAVEL STOP WITH BAKED ENAMEL FINISH AND CONT. HOLD DOWN CLIPS ON FRONT SIDE OF PARAPET
- 10 EXPOSED SIDEWALL AT TOWER TO RECEIVE JAMES HARDIE FIBER CEMENT ARTISAN V GROOVE SIDING, 8.25" X12" PRIMED, PAINT TO MATCH 4 X 8 H X 16 SPLIT FACE CMU VENEER W/ DRYBLOC ADDITIVE, DARK GRAY COLOR. SEALANT BETWEEN CMU AND SIDING
- 11 MITER CUT PRECAST CONC WATERTABLE TO FIT SLOPE
- 12 SIDEWALL GRAPHIC STRIP FURNISHED BY OWNER INSTALLED BY CONTR
- 13 ACM PANEL FINISH ON HDD BACKING SYSTEM; REFER TO SHEET A402 FOR ADDITIONAL DETAILS; FINISH BY MANUF.
- 14 NOT USED
- 15 6" DIA PVC DOWNSPOUT, RELEASE MIN 6" ABOVE GRADE
- 16 TOWER BEYOND

GENERAL NOTES

- A. CONTRACTOR TO COORDINATE WITH FIRE MARSHALL FOR REQUIREMENTS OF ADDRESS NUMBER.
- B. BUILDING SIGNAGE UNDER SEPARATE PERMIT. CONTRACTOR TO OBTAIN FINAL SIGNAGE DRAWINGS FOR CONFIRMATION OF ELECTRICAL ROUGH-IN PRIOR TO THE COMMENCEMENT OF WORK. SEE ELECTRICAL.
- C. REFER TO E-4 FOR SECURITY PLAN. COORDINATE FINAL LOCATIONS OF CAMERAS WITH THE OWNER PRIOR TO THE COMMENCEMENT OF WORK.
- G. UNLESS NOTED OTHERWISE, MASONRY CONTROL JOINTS SHALL BE LOCATED SUCH THAT NO STRAIGHT RUN OF MASONRY WALL EXCEEDS 24' - 0". REFER TO GENERAL STRUCTURAL NOTES ON S100.
- F. REFER TO DOOR AND WINDOW SCHEDULE ON A500 FOR DOOR AND FRAME FINISHES AND WINDOW GLASS TYPE AND FRAME FINISHES.



DATE	DESCRIPTION	DELTA

TAKE 5 CAR WASH
 1743 COUNTY RD C
 ROSEVILLE, MN 55113



ARCHITECT: **R**

ENGINEER: _____

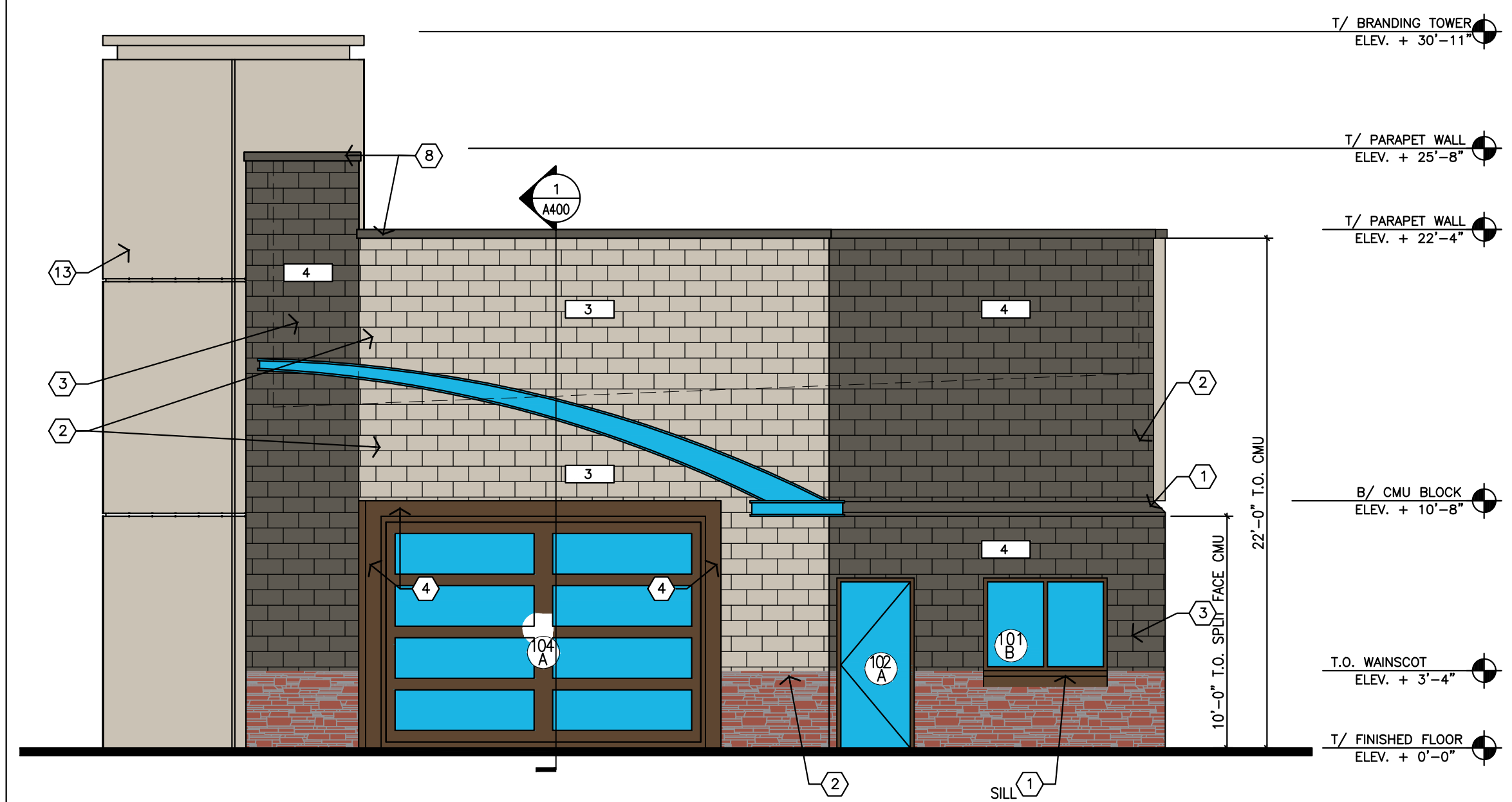
STAMPS _____

SHEET TITLE
COLOR ELEVATIONS

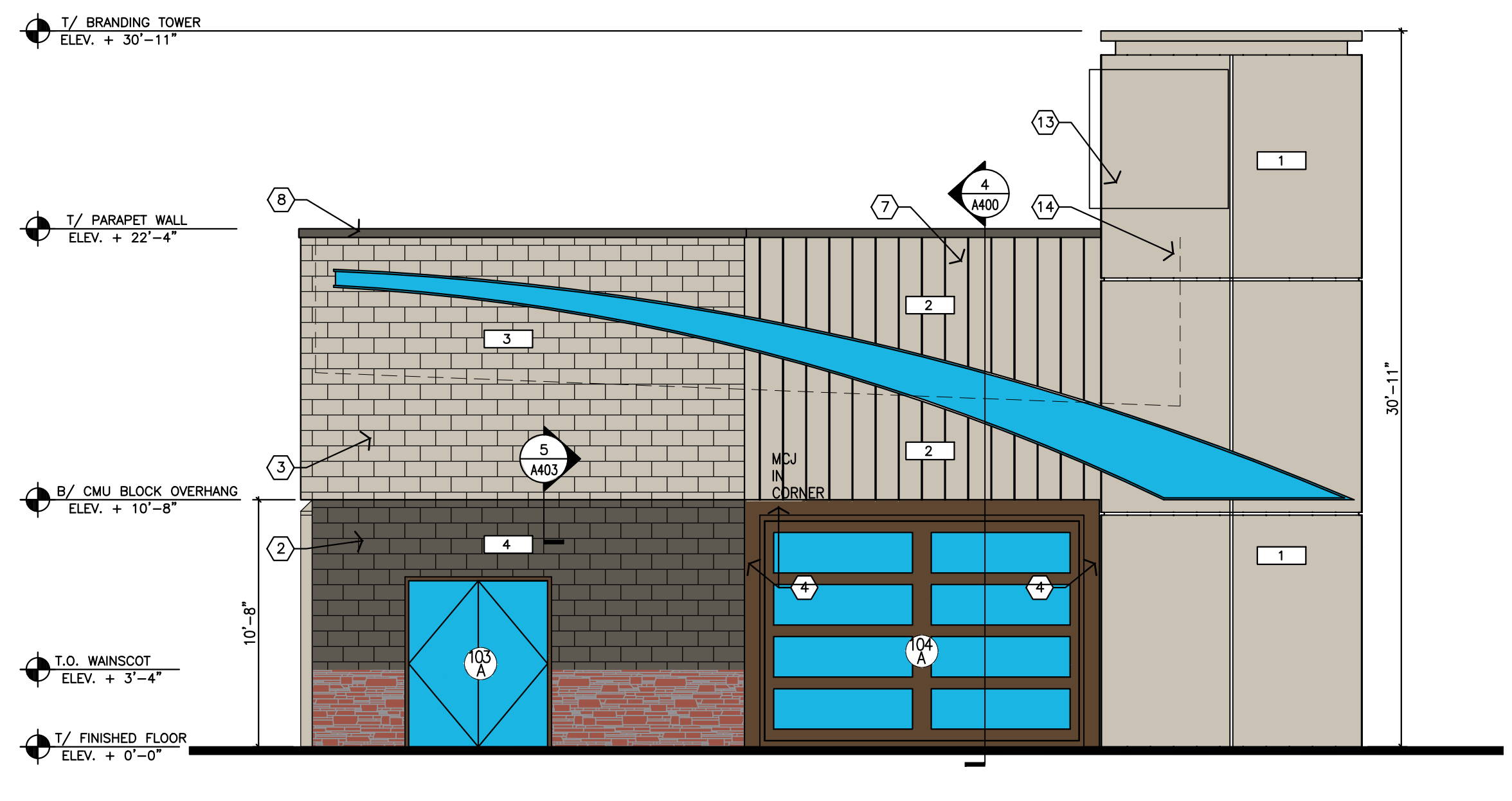
PROJECT NUMBER
21-1061

DATE: 12.16.2021
 SCALE: AS NOTED
 DRAWN: AN
 CHECKED: CA

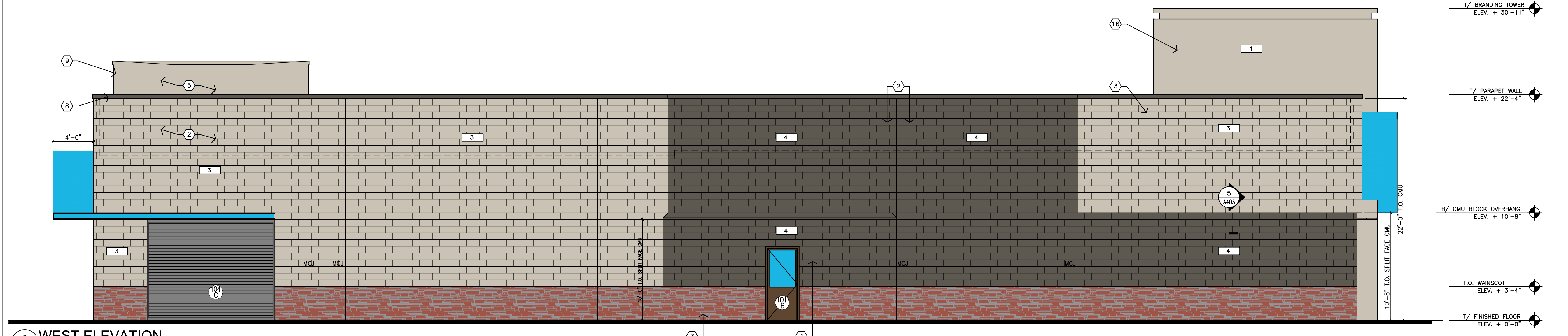
SHEET NO
A201



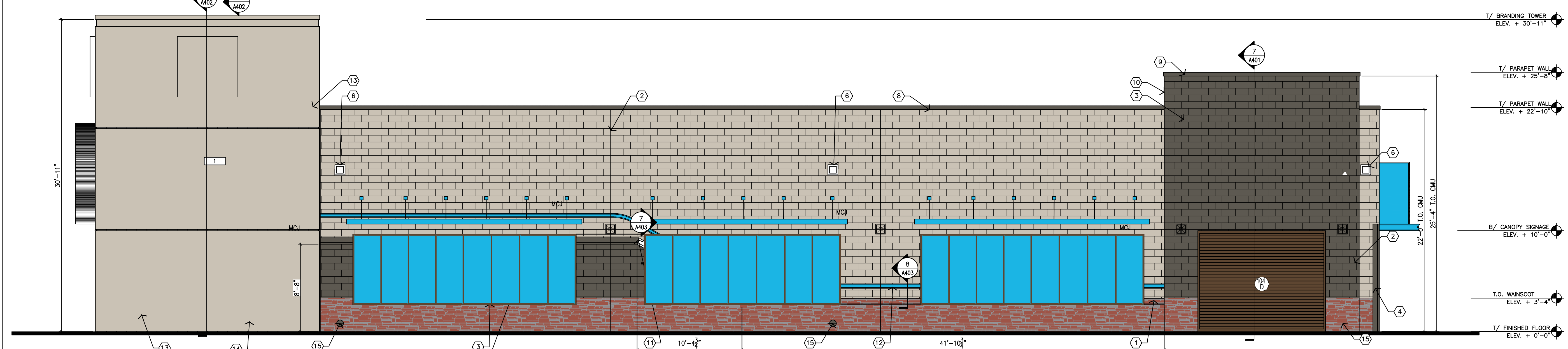
1 NORTH ELEVATION
 SCALE: 3/16" = 1'-0"



2 SOUTH ELEVATION
 SCALE: 3/16" = 1'-0"



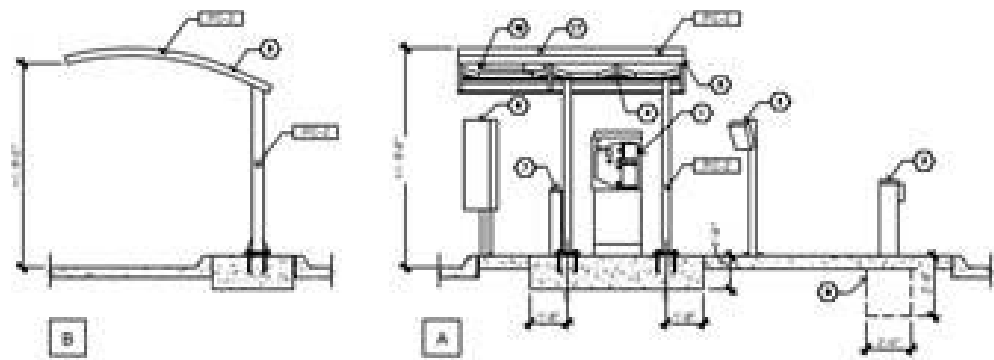
3 WEST ELEVATION
 SCALE: 3/16" = 1'-0"



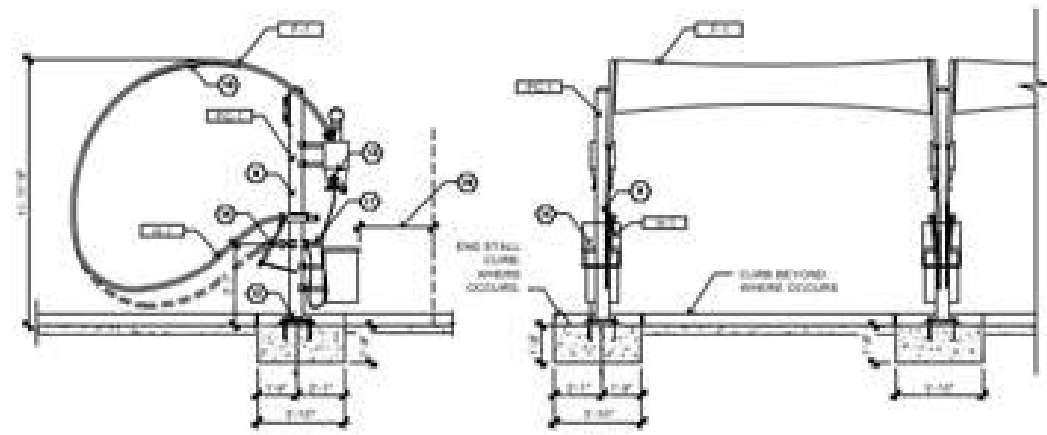
4 EAST ELEVATION
 SCALE: 3/16" = 1'-0"

1/4" = 1'-0"

1/4" = 1'-0"



PAY KIOSK ISLAND SECTION



TYPICAL VACUUM ARCH SECTION

KEYNOTES

- ① 8PT PAY KIOSK. REFER TO 4 - SPT FOR MANUFACTURER BASE FRAME. CONFIRM FOOTING AND BASE PLATE ANCHOR BOLT PLACEMENT WITH MANUFACTURER (SSE SYSTEMS) INSTRUCTIONS PRIOR TO INSTALLATION.
- ② FAST PASS READER. REFER TO 83 FOR ELECTRICAL REQUIREMENTS. NO FOOTING REQUIRED PER THE MANUFACTURER (SSE SYSTEMS).
- ③ VEHICULAR GATE. REFER TO 83 FOR ELECTRICAL REQUIREMENTS. REFER TO MANUFACTURER'S (SSE SYSTEMS) INSTALLATION REQUIREMENTS FOR THE REQUIRED FOOTING AND BASE PLATE ANCHOR BOLT PLACEMENT.
- ④ LED LIGHT KIT FURNISHED AND INSTALLED BY GENERAL CONTRACTOR.
- ⑤ PRE-MANUFACTURED CANOPY BY THE REFER TO 83 (SSE) MODEL. STENO - HEAVY DUTY DUAL POST CURVED PAY STATION CANOPY. SEE BOLT PATTERNS THIS SHEET. REFER TO STRUCTURAL FOR FOOTING.
- ⑥ MENU SIGN BY OWNER. REFER TO SHEET 818 FOR ELECTRICAL REQUIREMENTS.
- ⑦ BILLIARD REFER TO CIVIL.
- ⑧ LINE OF FOOTING. VERIFY FOOTING REQUIREMENTS WITH THE MANUFACTURER'S INSTALLATION MANUAL.
- ⑨ UMBRELLA ARCH - MANUFACTURER VACUUM. REFER TO BASE PLATE BOLT PATTERNS THIS SHEET. REFER TO STRUCTURAL FOR FOOTING.
- ⑩ 1" LIGHT FIXTURE. FURNISHED BY OWNER. INSTALLED BY GENERAL CONTRACTOR.
- ⑪ ACCESSIBLE VACUUM HOSE AT HANDICAP ACCESSIBLE STALL. REFER TO CIVIL FOR HANDICAP ACCESSIBLE VACUUM STALL.
- ⑫ WHEEL STOP (WHERE OCCURS) REFER TO CIVIL.
- ⑬ COMPRESSED AIR HOSE AND NOZZLE FURNISHED BY MANUFACTURER WITH VACUUM ARCH. GENERAL CONTRACTOR SHALL PROVIDE (1) UNDERGROUND SLEEVE FROM EQUIPMENT ROOM TO EACH ROW OF VACUUM STALLS. REFER TO SPT.
- ⑭ CAR BAY PACK FURNISHED BY MANUFACTURER.
- ⑮ MAINTAIN MINIMUM ACCESSIBLE ROUTE WIDTH. REFER TO CIVIL.
- ⑯ EXTERIOR INSECT HEATER. REFER TO MECHANICAL.
- ⑰ EXTENDED CANOPY OVERHANG BY TSA. REFER TO STRUCTURAL FOR FOOTING.

REFER TO SHEET FOR FINISH SCHEDULE

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SITE PLAN NOTES

- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY OF ROSEVILLE AND MNDOT REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
- CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL DISTURBED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL, SEED, MULCH AND WATER UNTIL A HEALTHY STAND OR GRASS IS ESTABLISHED
- ALL INNER CURBED RADII ARE TO BE 3' AND OUTER CURBED RADII ARE TO BE 10' UNLESS OTHERWISE NOTED. STRIPED RADII ARE TO BE 5'.
- ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- EXISTING STRUCTURES WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS) INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND PROJECT SITE WORK SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
- SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY A LAND SURVEYOR.
KIMLEY-HORN ASSUMES NO LIABILITY FOR ANY ERRORS, INACCURACIES, OR OMISSIONS CONTAINED THEREIN.
- TOTAL LAND AREA IS 3.67± ACRES.
- NO WETLANDS WERE PRESENT ON THIS SITE. DRAINAGE DITCH IS LOCATED ALONG THE WESTERLY PROPERTY LINE
- THE SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED "THE SITE SPECIFIC SPECIFICATIONS".
- CONTRACTOR SHALL REFERENCE ARCH / MEP PLANS FOR SITE LIGHTING AND ELECTRICAL PLAN.

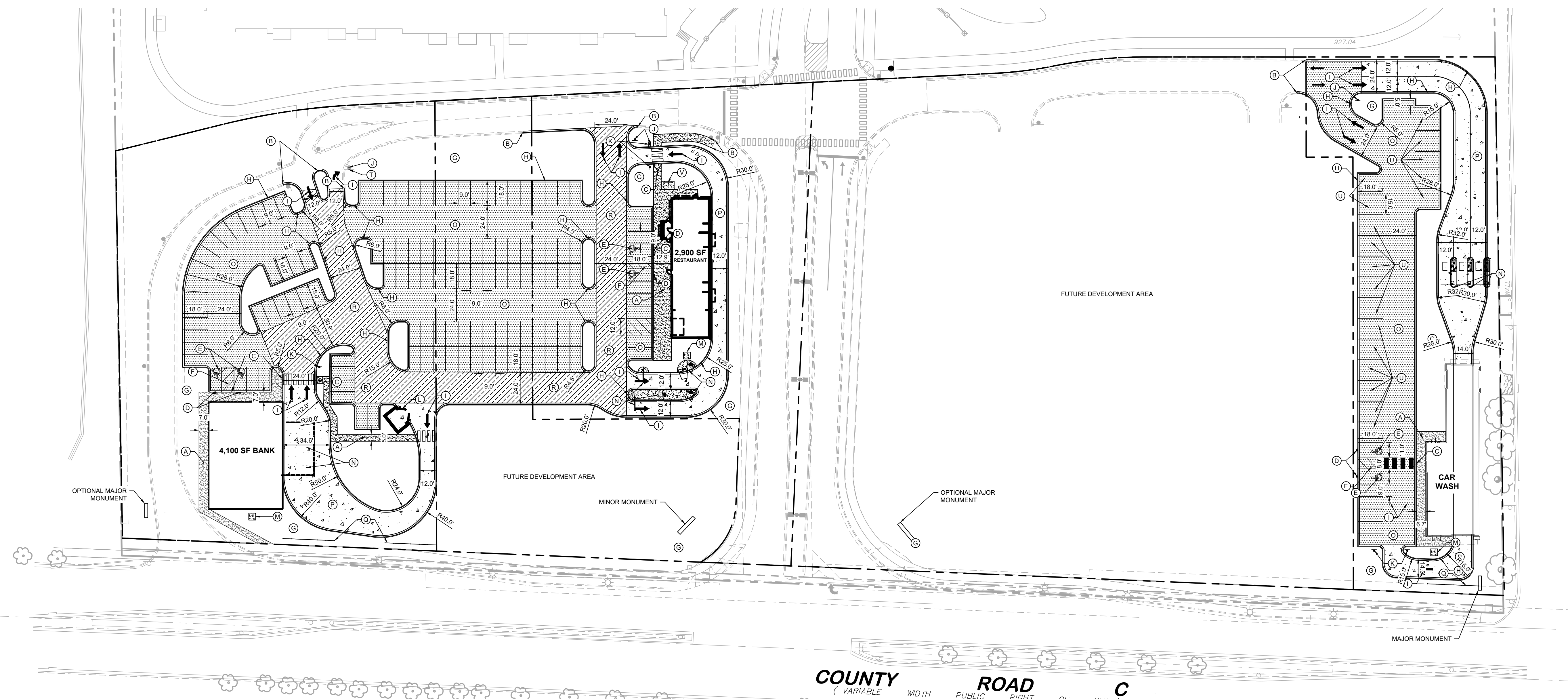
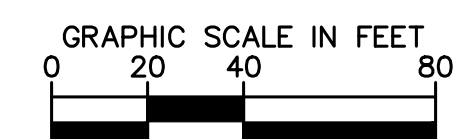
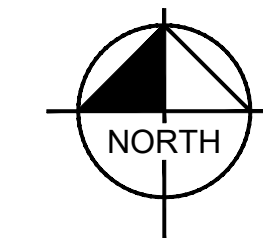
PROPERTY SUMMARY	
TWIN LAKES STATION 3RD ADDITION	
TOTAL PROPERTY AREA	4.67 AC
BANK PROPERTY AREA	1.62 AC
FUTURE DEVELOPMENT AREA	0.86 AC
RESTAURANT PROPERTY AREA	1.19 AC
CAR WASH PROPERTY AREA	1.00 AC
EXISTING IMPERVIOUS AREA	0.6 AC
PROPOSED IMPERVIOUS AREA	2.93 AC
EXISTING PERVIOUS AREA	4.07 AC
PROPOSED PERVIOUS AREA	1.74 AC
ZONING SUMMARY	
EXISTING ZONING	CMU4- COMMERCIAL MIXED USE-4
PROPOSED ZONING	CMU4- COMMERCIAL MIXED USE-4
PARKING SETBACKS	SIDE/REAR = 6' ROAD = 25'
BUILDING SETBACKS	FRONT = 0' SIDE = 0' REAR = 25' TO FRONT
PARKING SUMMARY	
PROPOSED PARKING (BANK)	54 STALLS
PROPOSED PARKING (FUTURE DEVELOPMENT)	41 STALLS
PROPOSED PARKING (RESTAURANT)	30 STALLS
PROPOSED PARKING (CAR WASH)	25 STALLS

KEYNOTE LEGEND

- (A) CONCRETE SIDEWALK
- (B) MATCH EXISTING EDGE OF PAVEMENT/ CURB & GUTTER
- (C) ACCESSIBLE CURB RAMP
- (D) ACCESSIBLE PARKING SIGN SET WITHIN BOLLARD
- (E) ACCESSIBLE PARKING STALL
- (F) AREA STRIPED WITH 4" SYSL @ 45° 2' O.C.
- (G) LANDSCAPE AREA
- (H) B612 CURB & GUTTER (TYP.)
- (I) DIRECTIONAL TRAFFIC ARROW. PER GREENBOOK STANDARDS
- (J) STOP SIGN
- (K) 'DO NOT ENTER' SIGN
- (L) TRASH ENCLOSURE (SEE ARCH. PLANS)
- (M) TRANSFORMER PAD (SEE ARCH./MEP PLANS)
- (N) DRIVE-THRU EQUIPMENT (TYP., SEE ARCH. PLANS)
- (O) ASPHALT PAVEMENT
- (P) CONCRETE PAVEMENT
- (Q) FENCE SCREENING (SEE LANDSCAPE DETAILS)
- (R) HEAVY DUTY ASPHALT PAVEMENT
- (T) RIGHT TURN ONLY SIGN
- (U) 15' WIDE VACUUM STALLS
- (V) BIKE RACK

LEGEND

- PROPERTY LINE
- - - SETBACK LINE
- ==== PROPOSED CURB AND GUTTER
- [Pattern] PROPOSED STANDARD DUTY ASPHALT
- [Pattern] PROPOSED CONCRETE PAVEMENT
- [Pattern] PROPOSED CONCRETE SIDEWALK
- [Pattern] PROPOSED HEAVY DUTY ASPHALT



COUNTY ROAD C
(VARIABLE WIDTH PUBLIC RIGHT OF WAY)

<p>TWIN LAKES STATION 3RD ADDITION PREPARED FOR LAUNCH PROPERTIES</p>	<p>SITE PLAN</p>	<p>ROSEVILLE MN</p>
		<p>SHEET NUMBER C400</p>
<p>KHA PROJECT 160727000.1.533 DATE 11/17/2021 SCALE AS SHOWN DESIGNED BY MTL DRAWN BY AJW CHECKED BY BMW</p>	<p>THIS DOCUMENT IS THE PROPERTY OF KIMLEY-HORN AND ASSOCIATES, INC. AND IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED IN THE TITLE BLOCK. ANY REUSE OR MODIFICATION OF THIS DOCUMENT WITHOUT THE WRITTEN AUTHORIZATION OF KIMLEY-HORN AND ASSOCIATES, INC. IS STRICTLY PROHIBITED.</p>	<p>BRIAN M. WURDEMAN MINN. LIC. NO. 53113</p>
<p>Kimley-Horn</p>		<p>© 2021 KIMLEY-HORN AND ASSOCIATES, INC. 787 EUSTIS STREET, SUITE 100, ST. PAUL, MN 55114 PHONE: 651-454-4197 WWW.KIMLEY-HORN.COM</p>
<p>REVISIONS</p>	<p>DATE</p>	<p>BY</p>

k:\TWC_LDE\launch properties\roseville retail - phase 2\3 Design\CAD\plansheets\BANK AND CAR WASH\C5-GRADING PLAN.dwg November 17, 2021 - 4:49pm

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GRADING PLAN NOTES

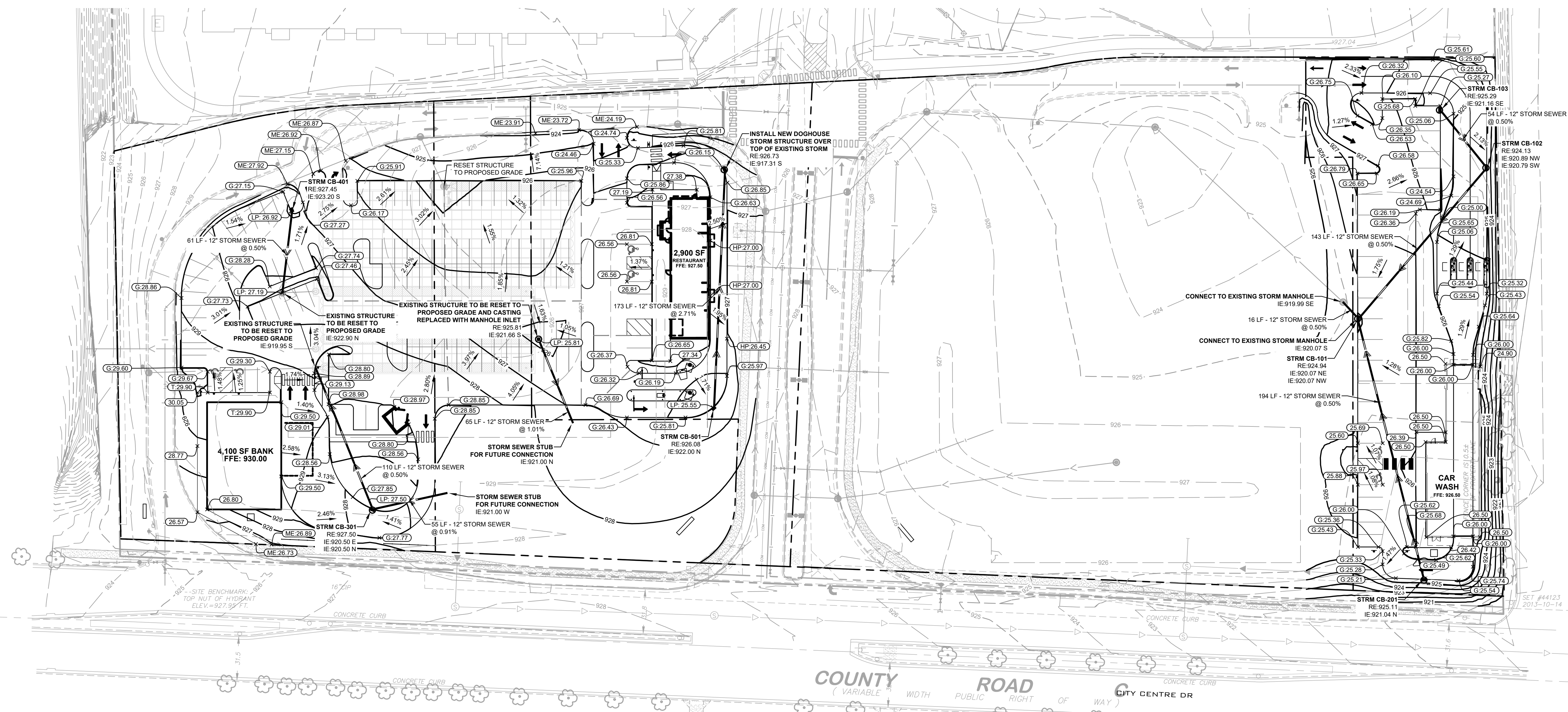
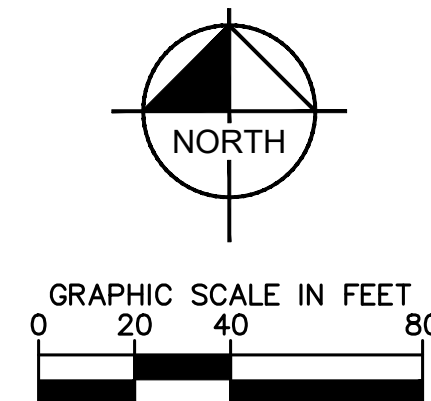
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF ROSEVILLE, SPECIFICATIONS AND BUILDING PERMIT REQUIREMENTS.
- CONTRACTOR TO CALL GOPHER STATE CALL ONE @ <1-800-252-1166> AT LEAST TWO WORKING DAYS PRIOR TO EXCAVATION/CONSTRUCTION FOR UTILITY LOCATIONS.
- STORM SEWER PIPE SHALL BE AS FOLLOWS:
RCP PER ASTM C-76
HDPE: 0' - 10' PER AASHTO M-252
HDPE: 12' OR GREATER PER ASTM F-2306
PVC SCH. 40 PER ASTM D-1785
WHEN STORM SEWER CROSSES ABOVE WATERMAIN: STORM SEWER (18" AND SMALLER) SHALL BE PVC SCH 40 PER ASTM D-1785, PROVIDE 18" MINIMUM VERTICAL CLEARANCE OUTER EDGE TO OUTER EDGE.
STORM SEWER FITTINGS SHALL BE AS FOLLOWS:
RCP PER ASTM C-76, JOINTS PER ASTM C-361, C-990, AND C-443
HDPE PER ASTM 3212
PVC PER ASTM D-3034, JOINTS PER ASTM D-3212
WHEN STORM SEWER CROSSES ABOVE WATERMAIN: STORM SEWER (18" AND SMALLER) SHALL BE PVC SCH 40 PER ASTM D-1785, PROVIDE 18" MINIMUM VERTICAL CLEARANCE OUTER EDGE TO OUTER EDGE.
- CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OR EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE START OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER OF ANY DISCREPANCIES OR VARIATIONS.
- SUBGRADE EXCAVATION SHALL BE BACKFILLED IMMEDIATELY AFTER EXCAVATION TO HELP OFFSET ANY STABILITY PROBLEMS DUE TO WATER SEEPAGE OR STEEP SLOPES. WHEN PLACING NEW SURFACE MATERIAL ADJACENT TO EXISTING PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING OF EXISTING PAVEMENT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HORIZONTAL AND VERTICAL CONTROL.
- CONTRACTOR SHALL EXCAVATE DRAINAGE TRENCHES TO FOLLOW PROPOSED STORM SEWER ALIGNMENTS.
- GRADES SHOWN ARE FINISHED GRADES. CONTRACTOR SHALL ROUGH GRADE TO SUBGRADE ELEVATION AND LEAVE STREET READY FOR SUBBASE.
- ALL EXCESS MATERIAL, BITUMINOUS SURFACING, CONCRETE ITEMS, ANY ABANDONED UTILITY ITEMS, AND OTHER UNSTABLE MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF THE CONSTRUCTION SITE.
- REFER TO THE UTILITY PLAN FOR SANITARY SEWER MAIN, WATER MAIN SERVICE LAYOUT AND ELEVATIONS AND CASTING / STRUCTURE NOTATION.
- CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION OF PAVEMENTS AND CURB AND GUTTER WITH SMOOTH UNIFORM SLOPES TO PROVIDE POSITIVE DRAINAGE.
- INSTALL A MINIMUM OF 4" CLASS 5+ AGGREGATE BASE UNDER CURB AND GUTTER AND CONCRETE SIDEWALKS.
- UPON COMPLETION OF EXCAVATION AND FILLING, CONTRACTOR SHALL RESTORE ALL STREETS AND DISTURBED AREAS ON SITE. ALL DISTURBED AREAS SHALL BE RE-VEGETATED WITH A MINIMUM OF 4" OF TOPSOIL.
- ALL SPOT ELEVATIONS/CONTOURS ARE TO GUTTER / FLOW LINE UNLESS OTHERWISE NOTED.
- GRADING FOR ALL SIDEWALKS AND ACCESSIBLE ROUTES INCLUDING CROSSING DRIVEWAYS SHALL CONFORM TO CURRENT ADA STATE/NATIONAL STANDARDS. IN NO CASE SHALL ACCESSIBLE RAMP SLOPES EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPES EXCEED 2%. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPES EXCEED 5%. IN NO CASE SHALL ACCESSIBLE PARKING STALLS OR AISLES EXCEED 2% (1.5% TARGET) IN ALL DIRECTIONS. SIDEWALK ACCESS TO EXTERNAL BUILDING DOORS AND GATES SHALL BE ADA COMPLIANT. CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY IF ADA CRITERIA CANNOT BE MET IN ANY LOCATION PRIOR TO PAVING. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR A.D.A COMPLIANCE ISSUES.
- MAINTAIN A MINIMUM OF 0.5% GUTTER SLOPE TOWARDS LOW POINTS.
- CONTRACTOR TO PROVIDE 3" INSULATION BY 5' WIDE CENTERED ON STORM PIPE IF LESS THAN 4" OF COVER IN PAVEMENT AREAS AND LESS THAN 3' OF COVER IN LANDSCAPE AREAS.
- ROOF DRAIN INVERT CONNECTIONS AT THE BUILDING SHALL BE AT ELEVATION <XXX.XX> OR LOWER UNLESS NOTED OTHERWISE. REFERENCE MEP PLANS FOR ROOF DRAIN CONNECTION.
- ALL STORM SEWER CONNECTIONS SHALL BE GASKETED AND WATER TIGHT INCLUDING MANHOLE CONNECTIONS.
- ALL STORM SEWER PIPE SHALL BE AIR TESTED IN ACCORDANCE WITH THE CURRENT PLUMBING CODE.
- MAINTAIN A MINIMUM OF 1.25% SLOPE IN BITUMINOUS PAVEMENT AREAS, 0.5% SLOPE IN CONCRETE PAVEMENT AREAS.
- CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "INFALL CURB" WHERE PAVEMENT DRAINS TOWARD GUTTER, AND "OUTFALL" CURB WHERE PAVEMENT DRAINS AWAY FROM GUTTER. CONTRACTOR TO COORDINATE WITH TRAFFIC CONTROL WITH CITY OF ROSEVILLE, RAMSEY COUNTY, AND MNDOT FOR CONSTRUCTION DISTURBANCE WITHIN HIAWATHA AVENUE.

LEGEND

- PROPERTY LINE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED STORM MANHOLE (SOLID CASTING)
- PROPOSED STORM MANHOLE (ROUND INLET CASTING)
- PROPOSED STORM MANHOLE/ CATCH BASIN (CURB INLET CASTING)
- PROPOSED STORM SEWER CLEANOUT
- PROPOSED FLARED END SECTION
- PROPOSED RIPRAP
- PROPOSED STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED SPOT ELEVATION
- PROPOSED HIGH POINT ELEVATION
- PROPOSED LOW POINT ELEVATION
- PROPOSED GUTTER ELEVATION
- PROPOSED TOP OF CURB ELEVATION
- PROPOSED FLUSH PAVEMENT ELEVATION
- MATCH EXISTING ELEVATION
- PROPOSED EMERGENCY OVERFLOW
- PROPOSED DRAINAGE DIRECTION
- PROPOSED ADA SLOPE
- PROPOSED RIDGELINE

CITY OF ROSEVILLE NOTES:

CONTRACTOR TO NOTIFY CITY OF ROSEVILLE ENGINEERING DEPARTMENT AT 651-792-7004, AT LEAST 24 HOURS PRIOR TO THE CONSTRUCTION OF THE STORMWATER BMPS.



Kimley >>> Horn

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DATE: 11/17/2021
LIC. NO. 5313

DESIGNED BY: MTL
DRAWN BY: AJW
CHECKED BY: BMW

KHA PROJECT: 160727000-1.533
DATE: 11/17/2021
SCALE: AS SHOWN
DESIGNED BY: MTL
DRAWN BY: AJW
CHECKED BY: BMW

DATE: 11/17/2021
LIC. NO. 5313

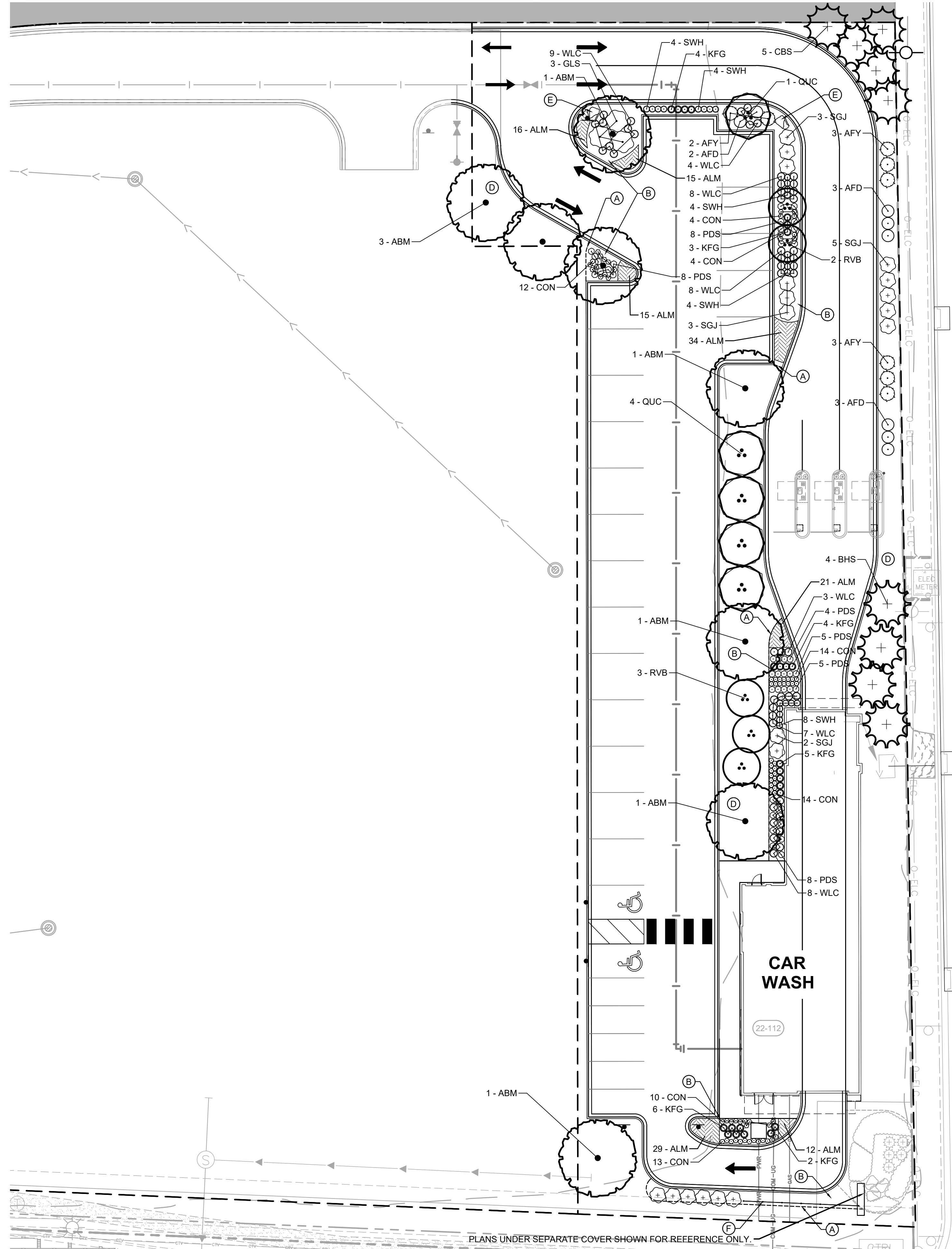
TWIN LAKES STATION 3RD ADDITION

PREPARED FOR **LAUNCH PROPERTIES**

ROSEVILLE, MN

GRADING PLAN

SHEET NUMBER **C500**



PLANT SCHEDULE CAR WASH

CONFIFEROUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL/SIZE
	BHS	4	PICEA GLAUCA 'DENSATA'	BLACK HILLS SPRUCE	B & B	6' HT.
	CBS	5	PICEA PUNGENS	COLORADO SPRUCE	B & B	6' HT.
DECIDUOUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL/SIZE
	ABM	8	ACER X FREEMANII 'AUTUMN BLAZE'	AUTUMN BLAZE MAPLE	B & B	2.5" CAL.
ORNAMENTAL TREE	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL/SIZE
	QUC	5	POPULUS TREMULOIDES	QUAKING ASPEN	B & B, CLUMP	6' HT.
	RVB	5	BETULA NIGRA	RIVER BIRCH MULTI-TRUNK	B & B, CLUMP	6' HT.
CONFIFEROUS SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	SGJ	19	JUNIPERUS CHINENSIS 'SEA GREEN'	SEA GREEN JUNIPER	#5 CONT.	
DECIDUOUS SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	AFD	8	CORNUS STOLONIFERA 'FARROW'	ARCTIC FIRE RED TWIG DOGWOOD	#5 CONT.	
	AFY	8	CORNUS STOLONIFERA 'SMNCSD' TM	ARCTIC FIRE YELLOW DOGWOOD	#5 CONT.	
	GLS	3	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	#5 CONT.	
GRASSES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	KFG	24	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	KARL FOERSTER FEATHER REED GRASS	#5 CONT.	
	PDS	38	SPOROBOLUS HETEROLEPIS	PRAIRIE DROPSEED	#5 CONT.	
	SWH	24	PANICUM VIRGATUM 'HEAVY METAL'	HEAVY METAL SWITCH GRASS	#5 CONT.	
PERENNIALS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	CON	71	ECHINACEA X 'TNECHKY'	KISMET YELLOW CONEFLOWER	#5 CONT.	
	WLC	47	NEPETA X 'WALKER'S LOW'	WALKER'S LOW CATMINT	#5 CONT.	
GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING
	ALM	142	ALLIUM TANGUTICUM 'SUMMER BEAUTY'	SUMMER BEAUTY GLOBE LILY	18" o.c.	

LANDSCAPE KEYNOTES

- (A) EDGER (TYP.)
- (B) DOUBLE SHREDDED HARDWOOD MULCH (TYP.)
- (C) ROCK MULCH (TYP.)
- (D) SOD (TYP.)
- (E) 2'-3' TRAP ROCK BOULDER (TYP.)
- (F) FENCE (SEE DETAIL) (TYP.)

SEEDING KEYNOTES

- (22-112) SEED WITH MNDOT 22-112: FIVE-YEAR STABILIZATION SEED MIX (TYP.)

LANDSCAPE SUMMARY

ZONED: RB - REGIONAL BUSINESS

MINIMUM LANDSCAPE REQUIREMENTS

REQUIRED TREES: 4 CANOPY/ EVERGREEN TREES = 1* 3,796 GROSS BUILDING FLOOR AREA/ 1,000 OR (WHICHEVER IS GREATER)
22 CANOPY/ EVERGREEN TREES = 1* 1,055 L.F. SITE PERIMETER/ 50

PROVIDED TREES: 22 TREES = 9 EVERGREEN TREES + 8 CANOPY TREES + (10 ORNAMENTAL TREES / 2)

*UP TO 25% OF THE REQUIRED NUMBER OF CANOPY OR EVERGREEN TREES MAY BE SUBSTITUTED WITH ORNAMENTAL TREES AT A RATIO OF 2 ORNAMENTAL TREES TO 1 CANOPY OR EVERGREEN TREE.
THE COMPLEMENT OF TREES REQUIRED SHALL BE AT LEAST 25% DECIDUOUS AND AT LEAST 25% CONFIFEROUS. NOT MORE THAN 30% OF THE REQUIRED NUMBER OF TREES SHALL BE COMPOSED OF A SINGLE SPECIES.

REQUIRED SHRUBS: 24 SHRUBS= 6*(3,796 GROSS BUILDING FLOOR AREA/ 1,000) OR (WHICHEVER IS GREATER)
132 SHRUBS= 6*(1,055 L.F. SITE PERIMETER/ 50)

PROVIDED SHRUBS: 140 SHRUBS* = 38 SHRUBS + (102 PERENNIALS/2)

*PERENNIALS SUBSTITUTED AT A RATIO OF 2 PERENNIALS TO 1 SHRUB

PARKING LOT LANDSCAPE

ISLAND PLANTING REQUIREMENTS: 5 CANOPY TREES / MIN. 1 CANOPY TREE IN EACH ISLAND, IN ADDITION TO ASSORTED SHRUBS, PERENNIALS, AND ORNAMENTAL GRASS.

PROVIDED: 5 CANOPY TREES AND SHRUBS, PERENNIALS AND ORNAMENTAL GRASS.

*UNABLE TO PLANT TREES IN SOME OF THE ISLANDS DUE TO UNDERGROUND FILTRATION BASIN & SIDEWALK SYSTEM. TREES PLANTED ELSEWHERE ON SITE.

LANDSCAPE LEGEND

- EXISTING DECIDUOUS TREE (TYP.)
- EXISTING CONFIFEROUS TREE (TYP.)
- EXISTING SHRUB (TYP.)
- EDGER (TYP.)
- APPROXIMATE LIMITS OF SOD / IRRIGATION, SOD ALL DISTURBED AREAS (TYP.)
- SEED/ SOD EDGE (TYP.)
- SOD
- FENCE (SEE DETAIL) (TYP.)

NO.	REVISIONS	DATE	BY

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KHA PROJECT: 160727000-1.533
 DATE: 11/17/2021
 SCALE: AS SHOWN
 DESIGNED BY: BPS
 DRAWN BY: BPS
 CHECKED BY: RAH

MIN. LIC. NO. 53828
 DATE: 11/17/2021
 RAH

LANDSCAPE PLAN

TWIN LAKES STATION
 3RD ADDITION
 PREPARED FOR
 LAUNCH
 PROPERTIES

ROSEVILLE MN

SHEET NUMBER
L103



Draft Review Summary

SRF No. 15234

To: Jesse Freihammer, PE, City Engineer and Assistant Public Works Director
City of Roseville

From: Tom Sachi, PE, Associate
Mark Powers, PE, PTOE, Senior Engineer

Date: January 17, 2022

Subject: Twin Lakes Station Traffic Impact Study Review, Roseville, MN

Introduction

As requested, SRF has completed a review of the *Twin Lakes Traffic Impact Study* document, dated November 19, 2021 completed by Kimley-Horn. The proposed development is located north of County Road C on Herschel Street in the City of Roseville and includes multifamily housing, senior housing, multipurpose recreation facility and drinking place, medical-dental office building, drive-in bank, fast food restaurant with drive-through window, and an automated car wash. A review of the memorandum identified the following questions and comments for consideration.

Review Summary

Based on our review, the following items were identified:

Background Information

1. The data collected at the study intersections was taken in May 2019 before the impacts from COVID-19, which is reasonable. The intersections collected are appropriate for the study. Data was collected during the weekday a.m. and p.m. peak hours, which is appropriate.

Volume Development

2. The peak hour trip generation using the *Institute of Transportation Engineers Trip Generation Manual, 11th Edition* is accurate for the proposed land uses. Without knowing exactly what the multipurpose recreation facility/drinking place entails, the numbers for that facility cannot be verified.
3. The pass-by percentages are accurate for the drive-up bank are accurate. A pass by percentage was not applied to the fast-food restaurant with a drive thru.
4. The internal capture in the a.m. peak is reasonable. However, the p.m. peak internal capture of 33% could be high. This results in the a.m. peak generating more new trips than the p.m. peak, which seems unlikely given the land uses. If a tool or spreadsheet was used to arrive at the p.m. internal capture percentage, it could be included.
5. The entering/exiting trips presented in Exhibit 1 do not add up to the volumes presented within the trip generation table. However, the differences are negligible and could potentially be attributed to a rounding error.

Existing Intersection Capacity Analysis

6. Traffic analysis was completed for the a.m. and p.m. peak hours. This is appropriate for the given land uses.
7. The delays and queues identified in the analysis results are provided from a HCM report and appear to be accurate.

Alternative Traffic Control

8. The justification for installation of a signal at Herschel is adequate.

Full Build Intersection Capacity Analysis

9. The report states that the Fairview Avenue intersection is LOS E in Opening Year analysis, but this not reflected in the table or HCM report. They may have meant to say Snelling Avenue.
10. The delays and queues identified in the analysis results are provided from a HCM report and appear to be accurate.

Conclusion and Site Review

11. A site plan review was not completed at part of the study to identify any changes with the site plan.
12. The site plan shows the bank having approximately 290 feet of queueing space. A study by Spack Consulting, using empirical data, shows that the average maximum queue for banks is 6 vehicles (120 feet), and the 85th percentile queue is 8 vehicles (160 feet). The proposed driveway for the bank should provide sufficient space for queueing.
13. The site plan shows the car wash having approximately 480 feet of queueing space. A study by Spack Consulting, using empirical data, shows that the average maximum queue for car washes is 5 vehicles (100 feet), and the 85th percentile queue is 6 vehicles (120 feet). The proposed driveway for the car wash should provide sufficient space for queueing.
14. The site plan shows the restaurant having approximately 220 feet of queueing space. A study by Spack Consulting, using empirical data, shows that the average maximum queue for fast food restaurants is 8.5 vehicles (170 feet), and the 85th percentile queue is 12 vehicles (240 feet). The proposed driveway for the restaurant should provide sufficient space for queueing at most times, but at its absolute peaks, queues may spillover into the parking area.